

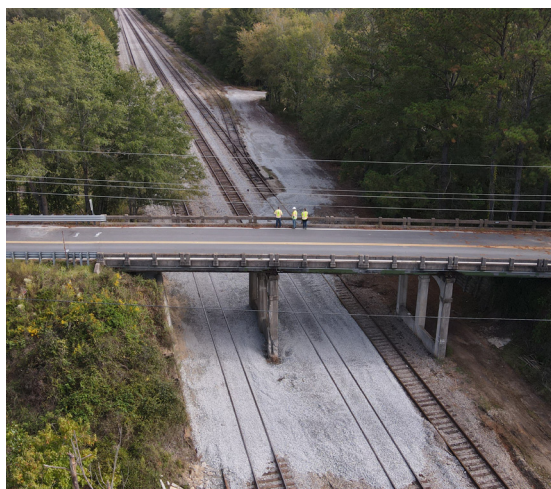
Technical Proposal



US 17A/21 over CSX Emergency Bridge Replacement Design-Build Project

Project ID P042942

Beaufort and Hampton Counties, SC



December 18, 2023

4.1 Technical Proposal - Narrative



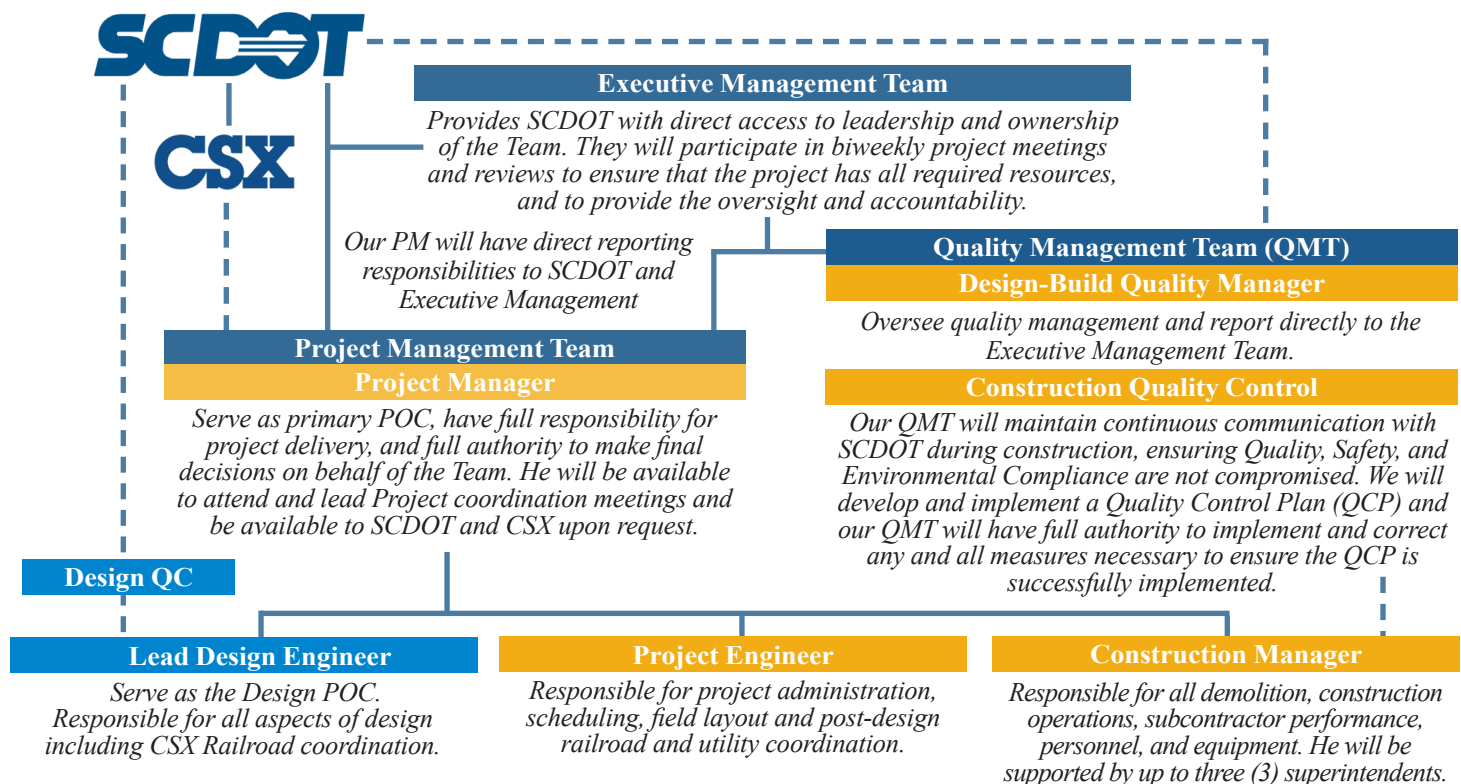
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Technical Proposal Narrative

1. | Project Delivery and Approach | Our Team immediately recognized the need for heightened communication and accountability during execution, due to the accelerated timeline for this Project. Communication is critical on all projects; however, an undertaking of this magnitude magnifies the importance. Reeves and RK&K have developed highly successful communication and collaboration protocols through our many ventures over the last seven years, including successfully completing 24 bridges on our recent CLRB D-B projects (2020-1 and 2021-1). The Reeves-RK&K D-B Team is dedicated to seamless communication and partnering with all stakeholders. We will further enhance our protocols and create a plan to fit this Project. *The exhibit below, briefly illustrates our proposed managerial and operations structure, including reporting responsibilities and clear lines of communication.*



Our Team's approach is to efficiently and immediately attack any portion of available work at the earliest opportunity. Significant approach roadway work that includes necessary surcharge embankments and bridge construction access (BCA) can be constructed, outside the railroad (R/R) right of way (ROW) and be effectively completed while the R/R right of entry (ROE) is executed. This will require expedited designs, submittals, and approvals to create early work availability. Upon execution of the R/R ROE, we will complete the BCA and begin work on the bridge structure over the R/R.

These early steps of preparation will allow our Team to begin execution of our plan to construct the Project. In addition to a carefully thought-out demolition plan detailed below, our Team has revised the girders from 54" FIBs to 63"



MBTs, to reduce the critical lifts load weight within CSX ROW and foul zone, providing more options for crane resources to meet CSX’s submittal requirements and project schedule. During operations within CSX ROW, effective management and implementation of our communication plan will be critical to maximizing efficiency of balancing construction and R/R flagging operations.

Our Team will assign a “Last Planner” for each operation requiring a R/R flagger to provide assurance that operations are ready to move forward when scheduled. The Last Planner will be responsible and held accountable to confirm all labor, equipment, materials are available, and all submittals and certifications have been acquired to perform the task.

Bridge Demolition | Reeves will prepare and submit the demo plan to SCDOT and CSX for review and approval. Upon receipt of the

approved demo plan, Reeves will mobilize required equipment, including cranes, excavators, hoe rams, and saws for demolition. Our plan is based on setting up the crane under the bridge for demo operations, if a crane is needed on the roadway approach it will be setup with sufficient offset back from the crib walls. Our team will begin operations with a coordination meeting attended by our team and R/R Flagger to review the demo plan and ensure compliance. We will install ballast and rail protection, and all picks will be rigged and secure prior to completing the saw cut and section removal. Next will be removal of girders by crane picks and cutting girders free at the bearings. Upper portions of interior piers will be saw cut and picked off in sections. Lower portion of interior and end bents near ground level will be demolished using hoe rams. Further details are included on our preliminary demo plan in [Appendix A.3](#).

Ability to Complete the Project within the Required Timeframe | Immediately upon invitation, Reeves contacted our trusted design partner, RK&K, to form a proven D-B Team to assist SCDOT and CSX with this critical project. As a Team, our recent delivery of 24 D-B bridges on the District 2 and 4 CLRB Packages demonstrates our ability to seamlessly deliver accelerated D-B projects. Coordination and collaboration within our Team are seamless at all levels, as each entity is extremely familiar with each other and anticipate each other’s needs.

Reeves will incorporate recent experience working with CSX on the S-90 (Boundary St.) bridge over CSX R/R bid-build project in Newberry, SC. Reeves’ knowledge provides current experience and insight to details sought by CSX and

Proposed Delivery Approach
• Immediately and efficiently attack any portion of available work at the earliest opportunity.
• Effectively complete approach roadway work and bridge construction access (BCA) while the railroad right of entry is executed.
• Expedite designs, submittals, and approvals to create early work availability.
• Upon execution of the railroad right of entry, we will complete the BCA and begin work within the railroad right of way.
• Steps above allow our Team to execute our plan to demolish the existing structure and expeditiously construct the new structure.
• Our Team revised the girders from 54” FIBs to 63” MBTs, to reduce the critical lifts load weight within CSX right of way and foul zone, providing more options for crane resources to meet CSX’s submittal requirements and project schedule.
• Assign a “Last Planner” for each operation requiring a railroad flagger. They will be responsible and held accountable to confirm all labor, equipment, materials are available, and all submittals and certifications have been acquired to perform the task.
• Substantially Complete on June 28, 2024



their agents in submittal packages and a thorough understanding of CSX's areas of concern. Reeves also has experience constructing bridges over many other R/R corridors. Specifically, Reeves constructed dual bridges over CSX R/R in Leland, NC, which included 155 ft drilled shafts and structural steel girder erection. This project required multi-crane lift plans to pick rebar cages for drilled shafts and structural steel girder erection, with in-air splices and temporary bent supports.

RK&K maintains extensive experience coordinating with various rail and transit owners, including CSX, Norfolk Southern, NCRR, Amtrak, and others. Their recent R/R experience on NC D-B projects, includes CSX - Monroe Bypass, NSRR/NCRR - I-40 Widening, NSRR - Macy Grove Road, and NSRR/NCRR - NC 43 Connector.

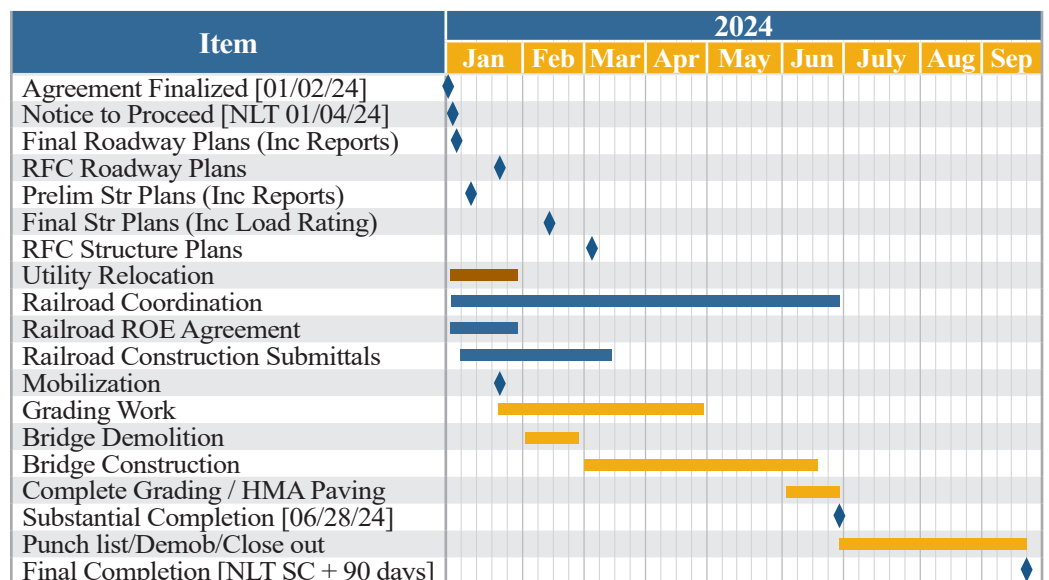
Immediately Available Resources | Our Team is committed to mobilizing resources to

ensure timely project completion. Reeves has multiple structure crews available to work 24/7, if needed, when coordinated R/R flagmen are available for operations. Reeves' structure crews readily available includes two crews in Savannah, GA on a D-B Interchange project and two crews from District 4 CLRB D-B project, as they all will be available by the end of 2023. Reeves maintains grading and HMA paving crews to mobilize from our Piedmont and Structures Regions in SC, and our Coastal Region in Savannah, GA.

Subcontracting / DBE Utilization | Reeves has solicited local subcontractors on our extensive list of contacts within the region for grading, asphalt, and specialty scopes. We will evaluate and select vendors based upon past performance, as well as price. Every effort is being made to achieve the 5% DBE contract goal, which included changing from 54-FIB to 63-MBT

to provide an opportunity for a DBE manufacturer to participate on this Project.

General Schedule | We have developed a proposed schedule that is aggressive, yet realistic to successfully deliver this bridge within the contractual time frame as outlined within the RFP.



Appendix A.1 - Bridge Plans



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a **HYPERLINK** and will take you to more detailed information.

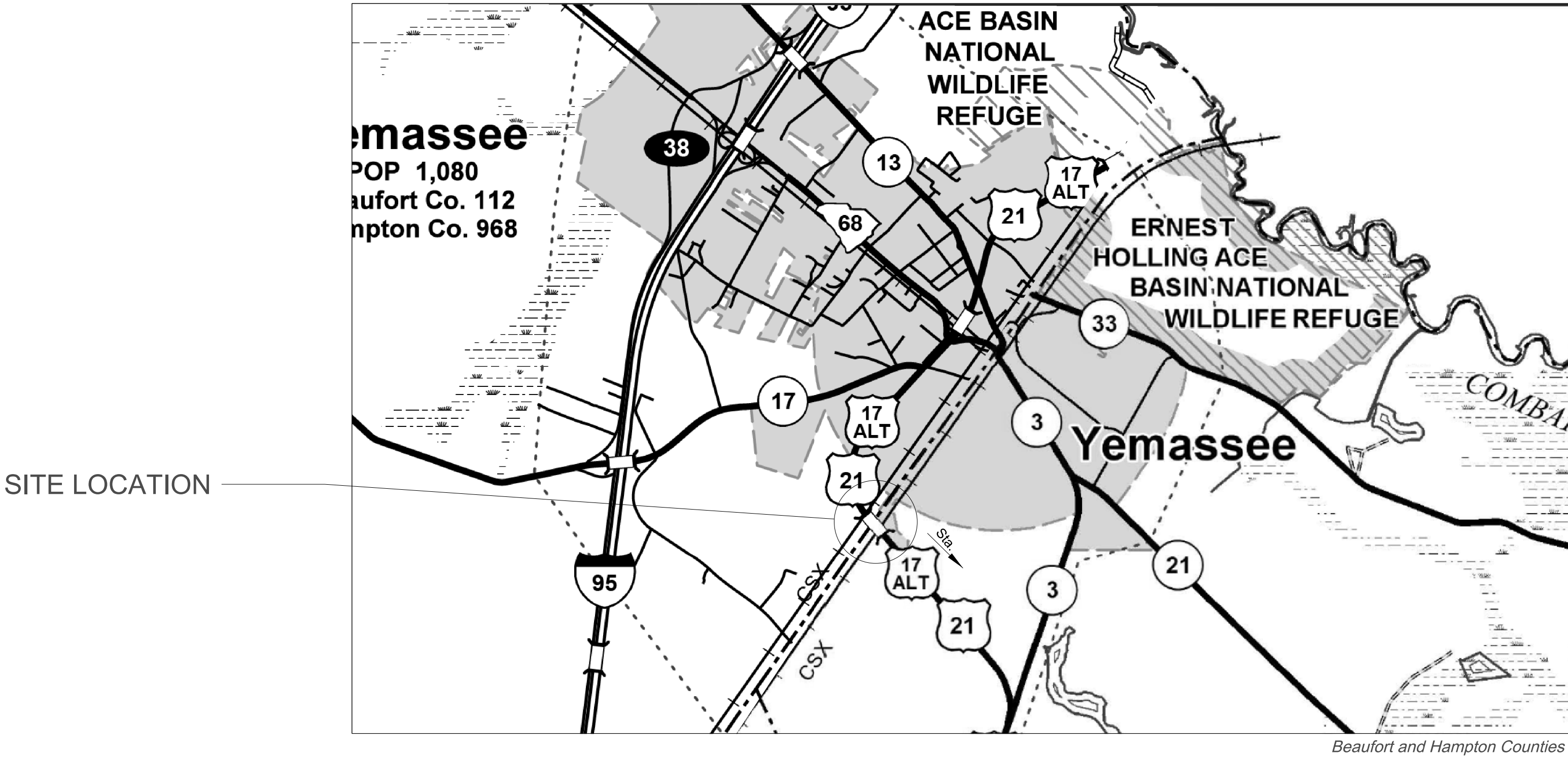
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INDEX OF SHEETS

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- 2. Bridge Plan and Profile
- 3. Interior Bent
- 4. Superstructure Typical Section



CONCEPTUAL BRIDGE PLANS
FOR
BEAUFORT AND HAMPTON COUNTIES
PROJECT ID P042942
U.S.ROUTE 17-ALT & U.S.ROUTE 21 (FRAMPTON ROAD)
REPLACE BRIDGE OVER CSX RAILROAD



Approximate Location of Bridge is
Latitude 32° - 40' - 26" N
Longitude 80° - 51' - 31" W

LAYOUT

NET LENGTH OF ROADWAY	0.000	MILES
NET LENGTH OF BRIDGES	0.046	MILES
NET LENGTH OF PROJECT	0.046	MILES
LENGTH OF EXCEPTIONS	0.000	MILES
GROSS LENGTH OF PROJECT	0.046	MILES

NOTE: EXCEPT AS MAY OTHERWISE BE SPECIFIED ON THE PLANS OR IN THE SPECIAL PROVISIONS, ALL MATERIALS AND WORKMANSHIP ON THIS PROJECT SHALL CONFORM TO THE SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (2007 EDITION) AND THE STANDARD DRAWINGS FOR ROAD CONSTRUCTION IN EFFECT AT THE TIME OF FINAL RFP.



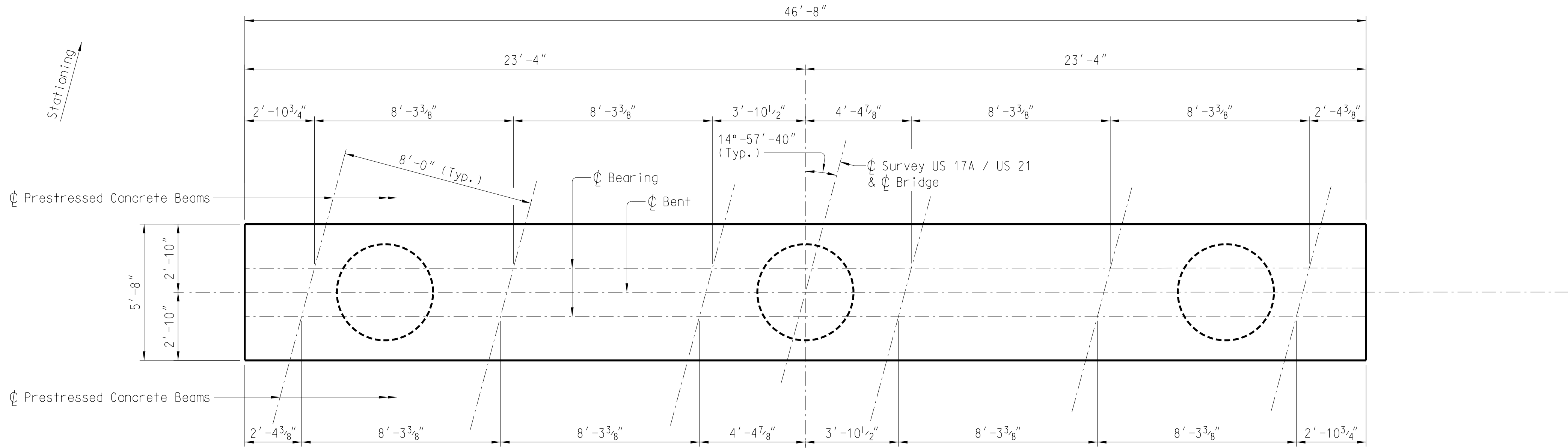
3 DAYS BEFORE DIGGING IN
SOUTH CAROLINA
CALL 811
SOUTH CAROLINA 811 (SC811)
WWW.SC811.COM
ALL UTILITIES MAY NOT BE A MEMBER OF SC811

ASSET ID 10890

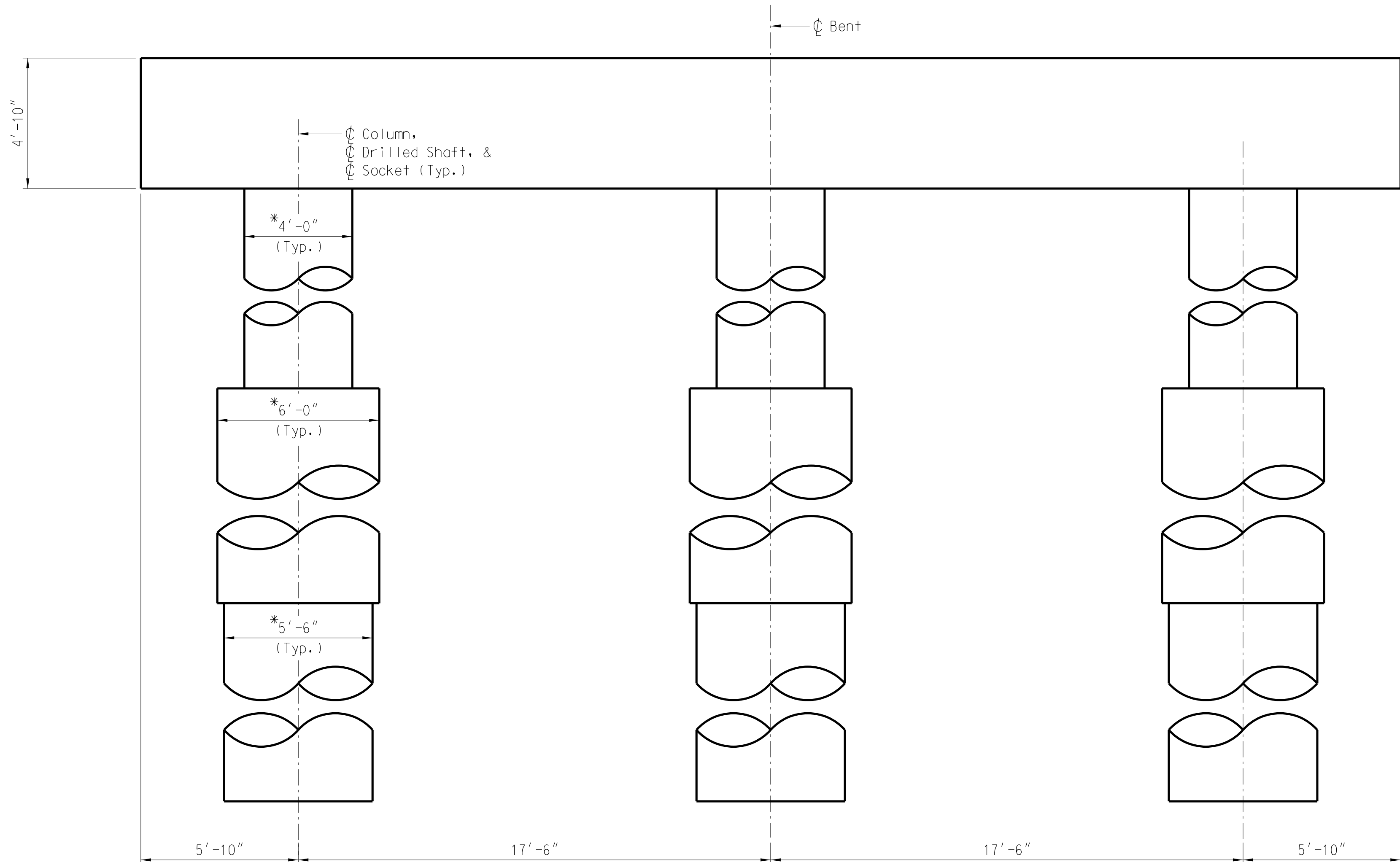
TRAFFIC DATA

2025 ADT 2,600 V.P.D.
2045 ADT 4,300 V.P.D.
TRUCKS 14 %

REVIEWED	DR.	LTW	KSH	9-23	DATE
		BY	CHK		



PLAN



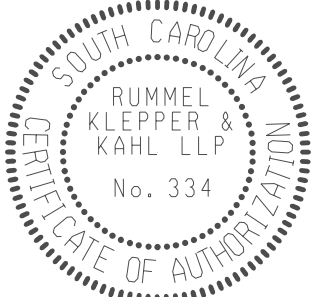
SHAFT NO. ➡

1



Drilled shafts are numbered from left to right looking in direction of stationing.

ELEVATION
(Looking in the direction of stationing)

*Foundation sizes dependent upon Final Geotechnical Report



REV.			
REV.			
REV.			
REVIEWED			
QUAN.			
DR.	KSH	PLD	10-23
DES.			
BY	CHK.	DATE	



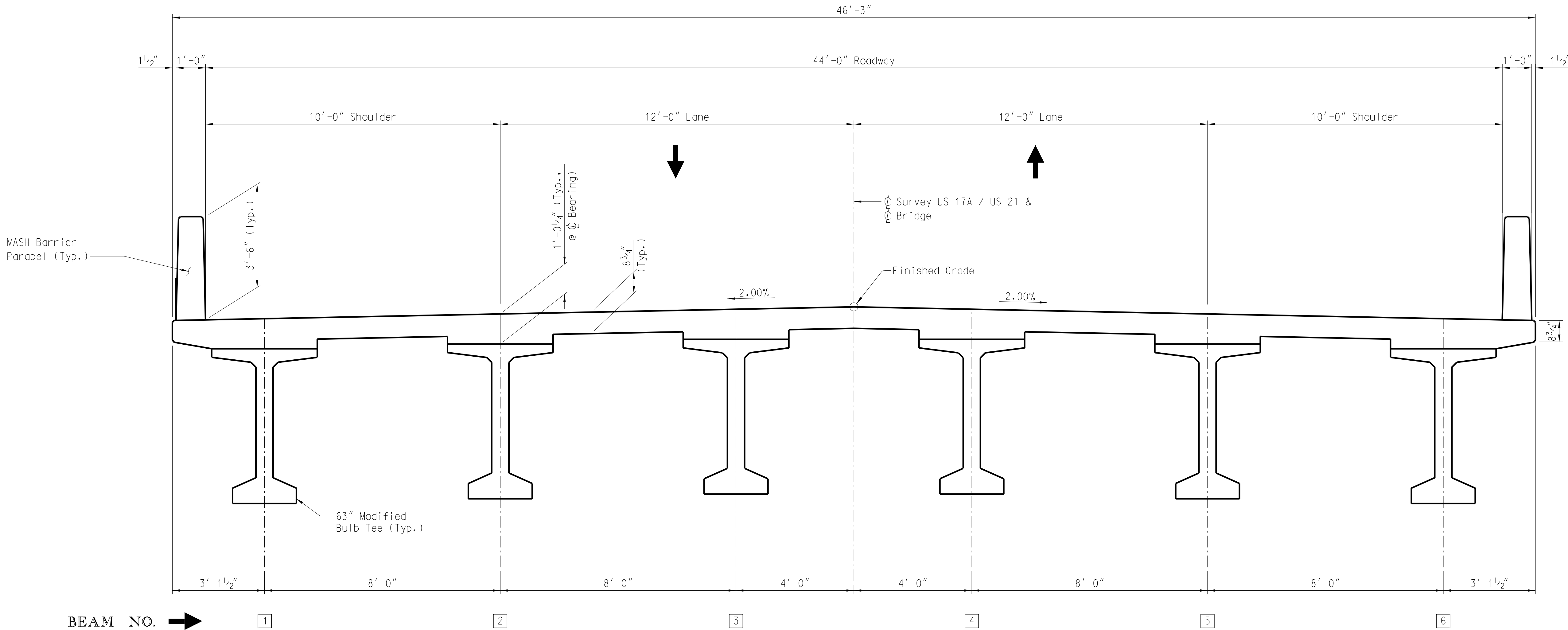
REEVES
A COLAS COMPANY

RK&K
100 years

SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION

INTERIOR BENTS 2 & 3

COUNTY	BEAUFORT / HAMPTON	ROUTE	US 17A / US 21
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SECTION THRU SUPERSTRUCTURE
(Looking in direction of stationing)

Note: Required CSX Protective Fencing not shown.



REV.			
REV.			
REV.			
REVIEWED			
QUAN.			
DR.	KSH	PLD	10-23
DES.			
BY	CHK.	DATE	



REEVES
A COLAS COMPANY

RK&K
100 years

SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION

SECTION THRU
SUPERSTRUCTURE

COUNTY	ROUTE
BEAUFORT / HAMPTON	US17A / US 21

Appendix A.2 - Roadway Plans

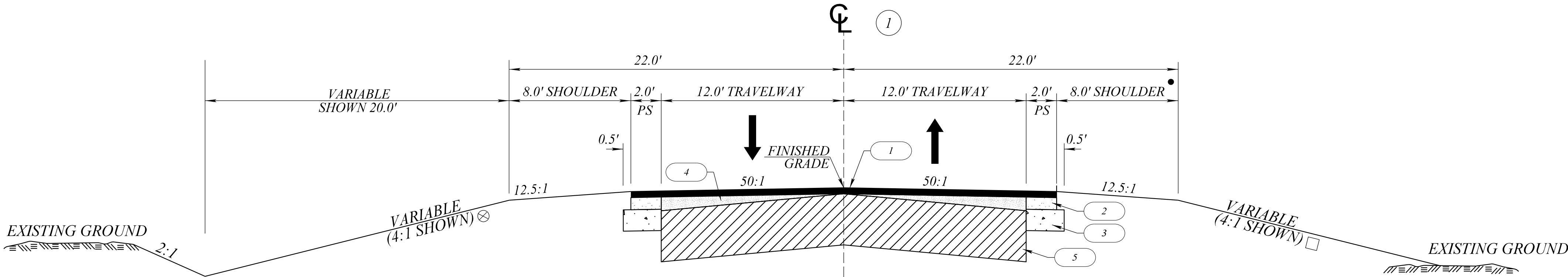


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FED. RD. DIST. NO.	STATE	COUNTY	PROJECT ID	ROUTE NO.	SHEET NO.
3	S.C.	HAMPTON/BEAUFORT	P042942	US 17A/21	2

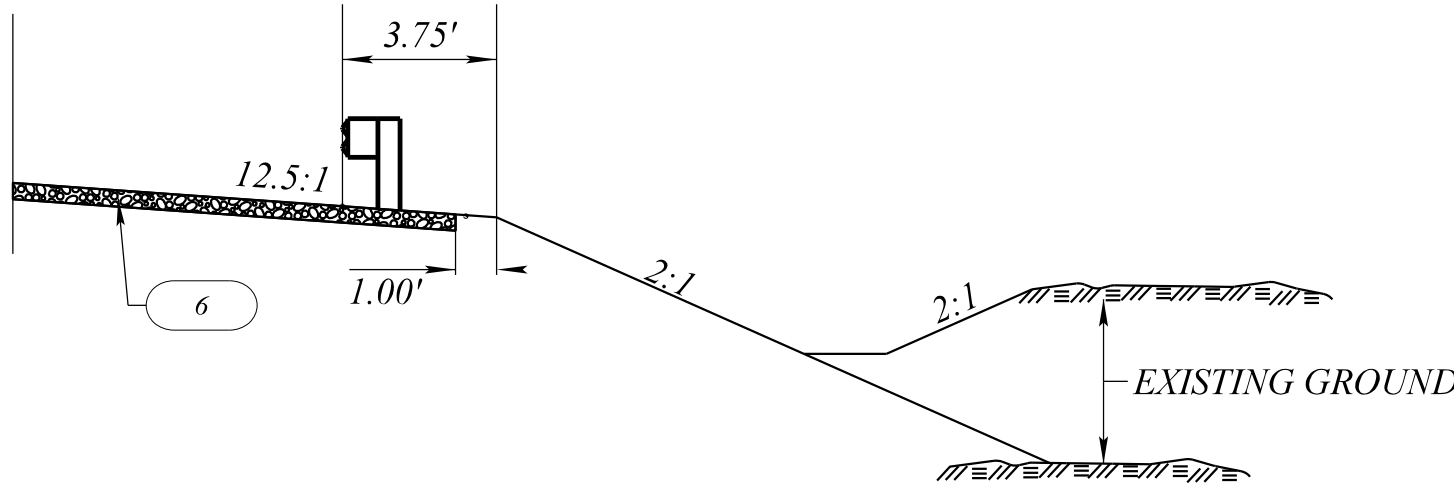
US 17A/21 (FRAMPTON ROAD)
OVER CSX RAILROAD



USE THIS SECTION ON:

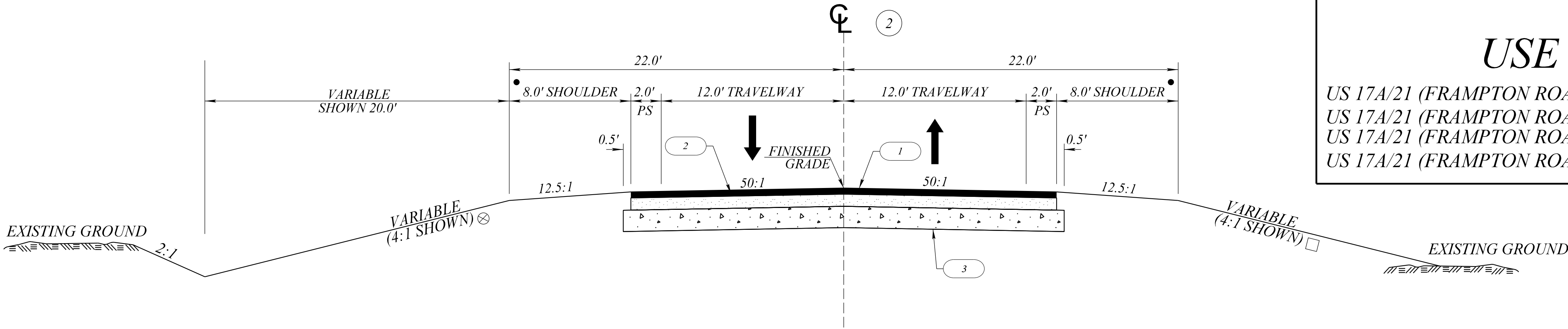
US 17A/21 (FRAMPTON ROAD) FROM STA. 1093+00.00 TO STA. 1095+00.00
US 17A/21 (FRAMPTON ROAD) FROM STA. 1116+75.00 TO STA. 1119+43.72

INSET 1



USE THIS INSET ON:

US 17A/21 (FRAMPTON ROAD) FROM STA. 1095+31.54 TO STA. 1104+89.32 (LT)
US 17A/21 (FRAMPTON ROAD) FROM STA. 1107+51.32 TO STA. 1116+44.00 (LT)
US 17A/21 (FRAMPTON ROAD) FROM STA. 1094+05.76 TO STA. 1105+01.28 (RT)
US 17A/21 (FRAMPTON ROAD) FROM STA. 1107+63.69 TO STA. 1115+52.00 (RT)



USE THIS SECTION ON:

US 17A/21 (FRAMPTON ROAD) FROM STA. 1095+00.00 TO STA. 1105+03.38 (BEGIN BRIDGE)
US 17A/21 (FRAMPTON ROAD) FROM STA. 1107+49.38 (END BRIDGE) TO 1116+75.00

□ FILL SLOPES

4:1 ----- IF FILL HEIGHT IS UNDER 5'

2:1 ----- IF FILL HEIGHT IS OVER 5'

⊗ NOTE:

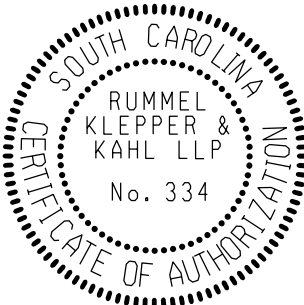
THIS SLOPE MAY BE VARIED WHEN A DEEPER DITCH IS NECESSARY FOR DRAINAGE PURPOSES,
USING A MINIMUM SLOPE OF 6:1 AND A MAXIMUM SLOPE OF 2:1.

- ADDITIONAL 3.75' WHERE GUARDRAIL IS USED.

LEGEND

1		HOT MIX ASPHALT SURFACE COURSE TYPE C (175 LBS/SY)
2		HOT MIX ASPHALT INTERMEDIATE COURSE TYPE C (200 LBS/SY)
3		HOT MIX ASPHALT BASE COURSE TYPE B (450 LBS/SY)
4		HOT MIX ASPHALT SURFACE TYPE E FOR BUILDUP AND LEVELING 0" TO 1.5" ** ** HOT MIX ASPHALT INTERMEDIATE TYPE B FOR BUILDUP AND LEVELING FOR GREATER THICKNESSES
5		EXISTING PAVEMENT - RETAIN
6		4" HOT MIX ASPHALT SURFACE COURSE TYPE C (GUARDRAIL NON-MOW STRIP)

FUNCTIONAL CLASS	DESIGN SPEED	FROM STA.	TO STA.
US 17A/21 - RURAL - MINOR ARTERIAL	55	1093+00.00	1119+43.72



CONTRACTOR:
REEVES
A COLAS COMPANY

PREPARED BY:
RK&K

PREPARED FOR:
SCDOT
CONCEPTUAL PLANS

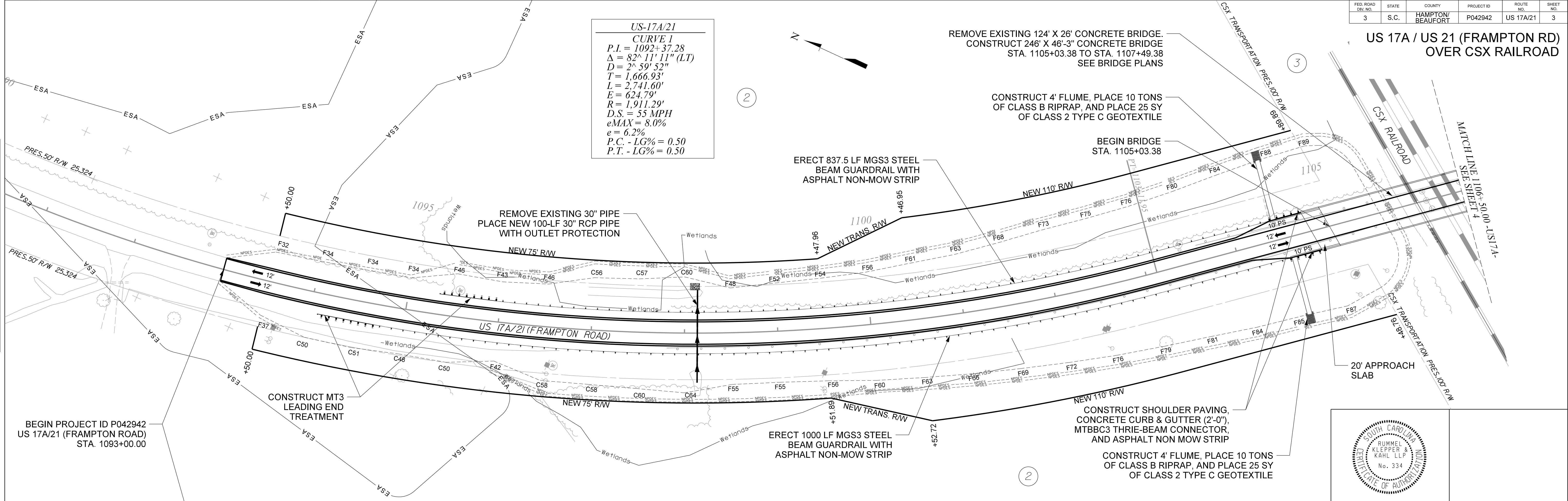
TYPICAL SECTION

SCALE: NTS

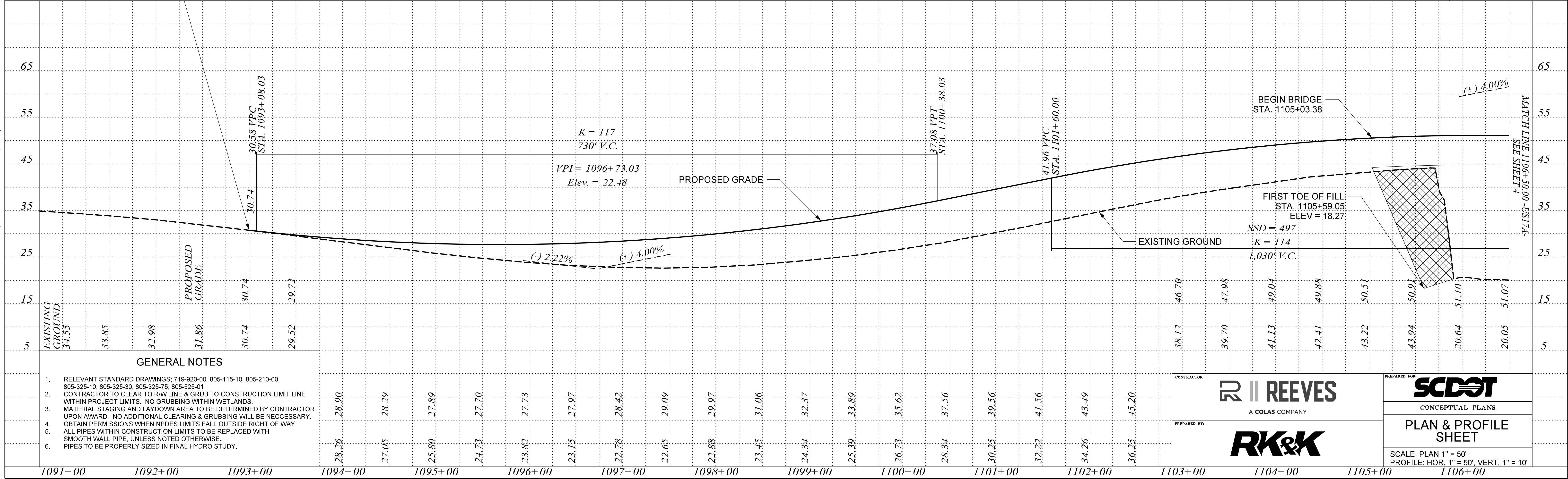
FED. ROAD DIST. NO.	STATE	COUNTY	PROJECT ID	ROUTE NO.	SHEET NO.
3	S.C.	HAMPTON/BEAUFORT	P042942	US 17A/21	3

US 17A / US 21 (FRAMPTON RD)
OVER CSX RAILROAD

PLAN	DATE	BY	DATE	BY	DATE
PLAN					
NOTE BOOK					
No.					



PLAN	DATE	BY	DATE	BY	DATE
PLAN					
NOTE BOOK					
No.					



GENERAL NOTES

- RELEVANT STANDARD DRAWINGS: 719-920-00, 805-115-10, 805-210-00, 805-325-10, 805-325-30, 805-325-75, 805-525-01
- CONTRACTOR TO CLEAR TO R/W LINE & GRUB TO CONSTRUCTION LIMIT LINE WITHIN PROJECT LIMITS. NO GRUBBING WITHIN WETLANDS.
- MATERIAL STAGING AND LAYDOWN AREA TO BE DETERMINED BY CONTRACTOR UPON AWARD. NO ADDITIONAL CLEARING & GRUBBING WILL BE NECESSARY.
- OBTAIN PERMISSIONS WHEN NPDES LIMITS FALL OUTSIDE RIGHT OF WAY
- ALL PIPES WITHIN CONSTRUCTION LIMITS TO BE REPLACED WITH SMOOTH WALL PIPE, UNLESS NOTED OTHERWISE.
- PIPES TO BE PROPERLY SIZED IN FINAL HYDRO STUDY.

CONTRACTOR:

REEVES

A COLAS COMPANY

PREPARED BY:

RK&K

PREPARED FOR:

SCDOT

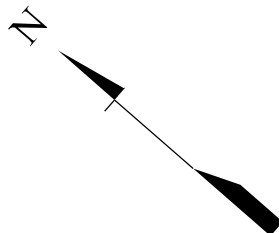
CONCEPTUAL PLANS

PLAN & PROFILE SHEET

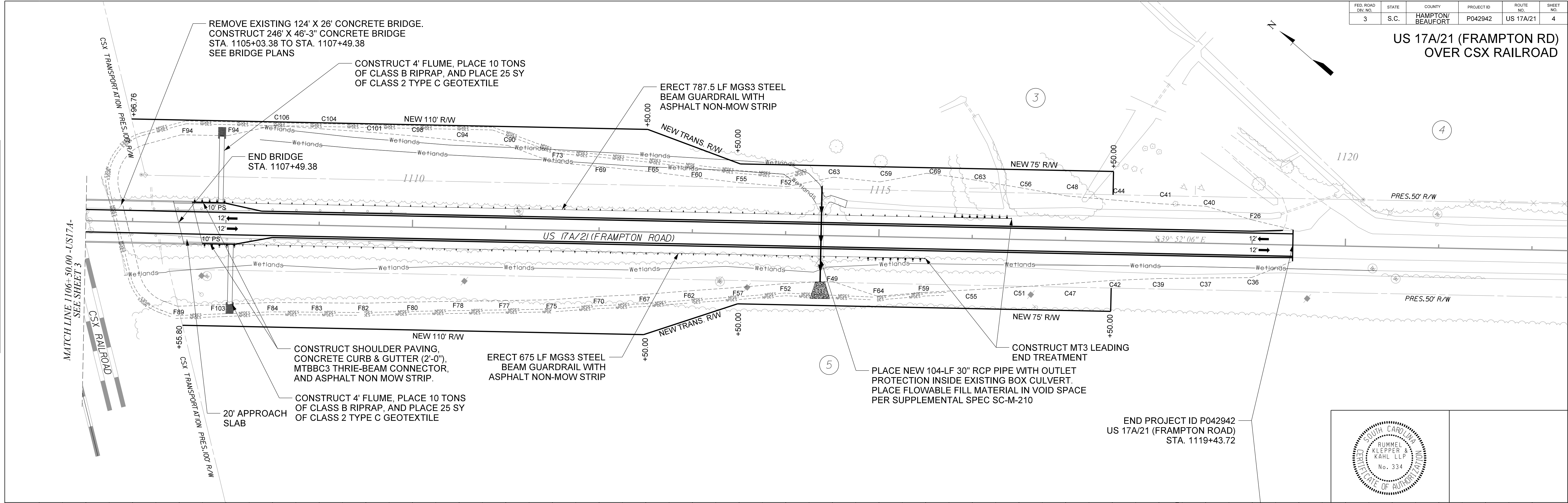
SCALE: PLAN 1" = 50'
PROFILE: HOR. 1" = 50', VERT. 1" = 10'

FED. ROAD DIST. NO.	STATE	COUNTY	PROJECT ID	ROUTE NO.	SHEET NO.
3	S.C.	HAMPTON/BEAUFORT	P042942	US 17A/21	4

US 17A/21 (FRAMPTON RD)
OVER CSX RAILROAD

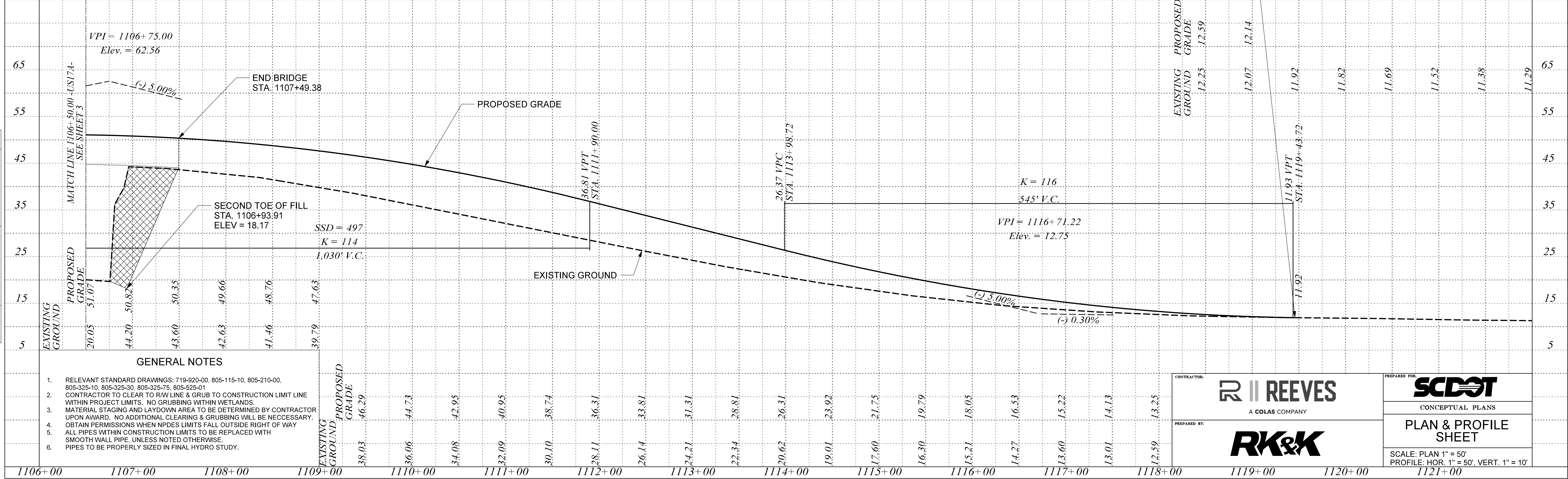


PLAN	DATE	BY	DATE	BY
PLAN				
NOTE BOOK				
RT. OF WAY CHECKED				
RT. OF WAY CHECKED				
No.				



END PROJECT ID P042942
US 17A/21 (FRAMPTON ROAD)
STA. 1119+43.72

PLAN	DATE	BY	DATE	BY
PLAN				
NOTE BOOK				
RT. OF WAY CHECKED				
RT. OF WAY CHECKED				
No.				



CONTRACTOR: **REEVES**
A COLAS COMPANY

PREPARED BY: **RK&K**

PREPARED FOR: **SCDOT**

CONCEPTUAL PLANS
PLAN & PROFILE SHEET

SCALE: PLAN 1" = 50'
PROFILE: HOR. 1" = 50', VERT. 1" = 10'

Appendix A.3 - Bridge Demolition Plans



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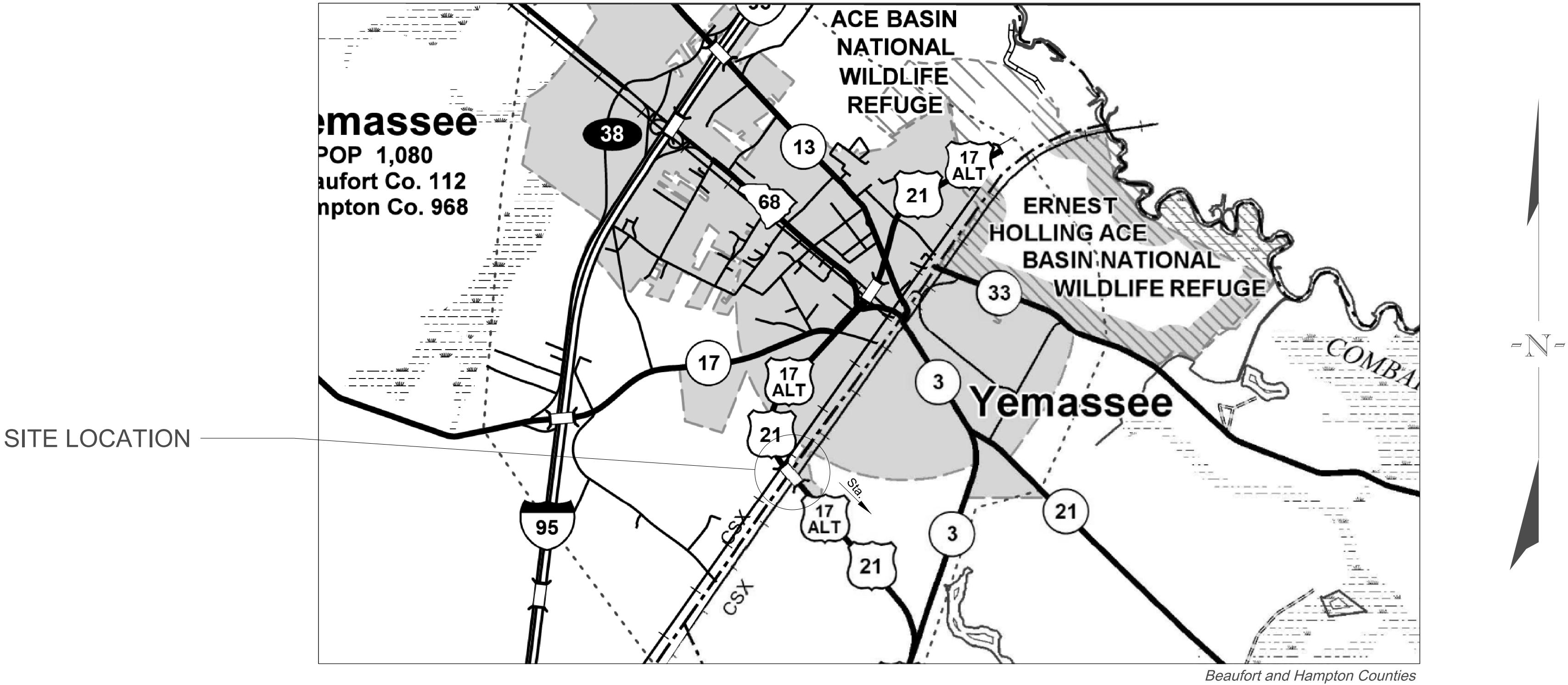
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3. Bridge Demolition Notes



PRELIMINARY BRIDGE DEMOLITION PLAN
FOR
BEAUFORT AND HAMPTON COUNTIES
PROJECT ID P042942
U.S.ROUTE 17-ALT & U.S.ROUTE 21 (FRAMPTON ROAD)



Approximate Location of Bridge is

Latitude	32° - 40' - 26" N
Longitude	80° - 51' - 31" W

3 DAYS BEFORE DIGGING IN
SOUTH CAROLINA
CALL 811
SOUTH CAROLINA 811 (SC811)
WWW.SC811.COM
ALL UTILITIES MAY NOT BE A MEMBER OF SC811

ASSET ID 834

TRAFFIC DATA				
2025	ADT	2,600	V.P.D.	
2045	ADT	4,300	V.P.D.	
TRUCKS		14	%	

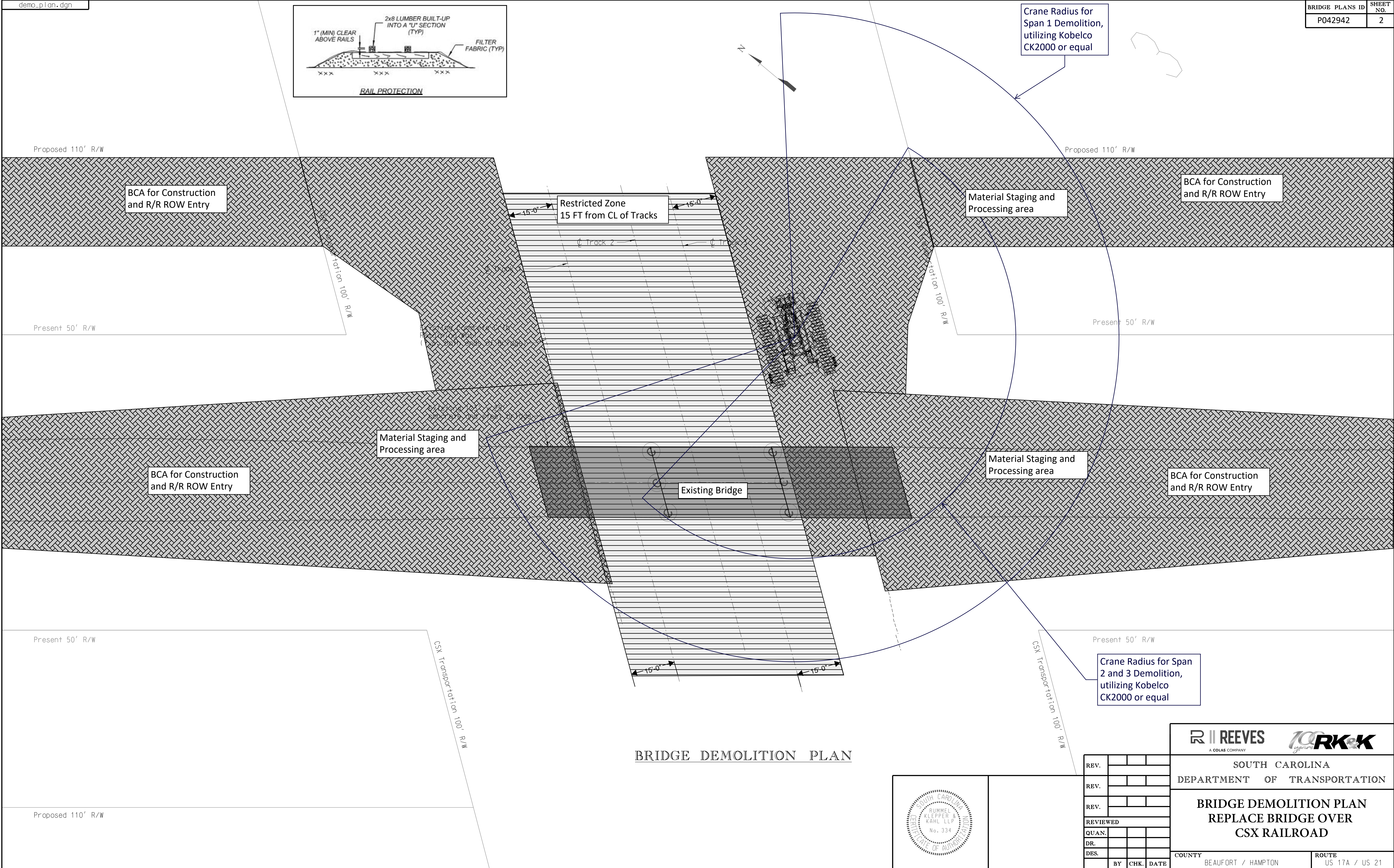
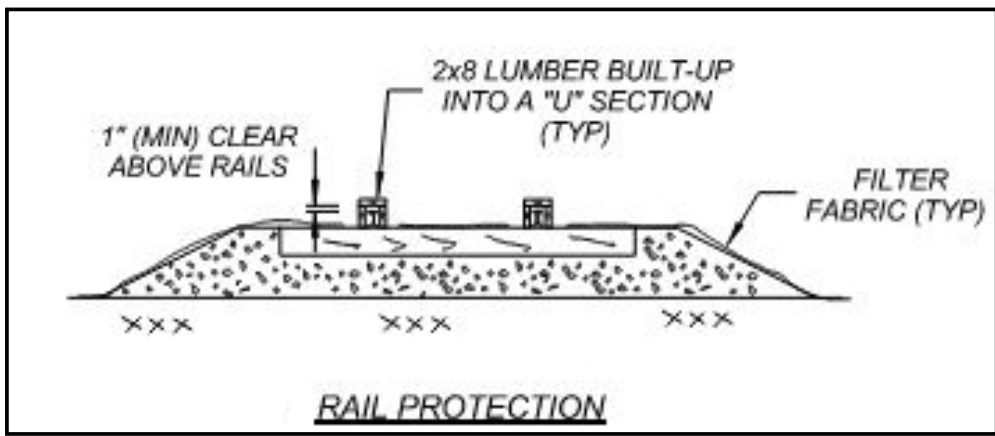
NET LENGTH OF ROADWAY	0.000	MILES
NET LENGTH OF BRIDGES	0.046	MILES
NET LENGTH OF PROJECT	0.046	MILES
LENGTH OF EXCEPTIONS	0.000	MILES
GROSS LENGTH OF PROJECT	0.046	MILES



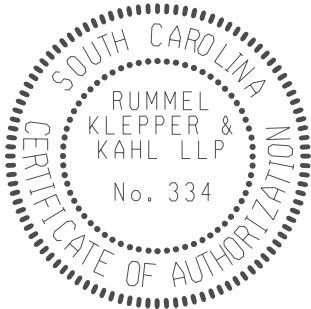
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REVIEWED	DR.	LTW	KSH	9-23	DATE
		BY	CHK		



12/14/2023 2:34:02 PM



BRIDGE DEMOLITION PLAN



REV.		
REV.		
REV.		
REVIEWED		
QUAN.		
DR.		
DES.		
BY	CHK.	DATE



A COLAS COMPANY

SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION

**BRIDGE DEMOLITION PLAN
REPLACE BRIDGE OVER
CSX RAILROAD**

COUNTY BEAUFORT / HAMPTON

ROUTE US 17A / US 21

BRIDGE DEMOLITION SEQUENCE

(**Preliminary: Sequence and/or Plan details may need to be revised as our Team finalizes the demo plan, per the CSX project manual we will need to verify current bridge condition and field verify actual dimensions of bridge elements by on-site investigation)

General

- Hold daily pre-shift coordination meeting attended by Contractor’s team and R/R personnel (Flagger) to review the demo operations for that shift. Make sure everyone on-site knows all restriction(s) and anticipated train schedule for that shift.
- Work shall be performed with R/R personnel present.
- Contractor will coordinate with R/R company to review demolition schedule and determine best window (largest time window) to remove girders from bridge structure over tracks.
 - Contractor will coordinate with R/R company to schedule these critical operations in advance to optimize schedule for demolition crew(s) and R/R personnel (Flaggers).
- Contractor shall provide a geotextile fabric ballast protection system extending 25’ beyond work limits to prevent fouling ballast.
- Prepare and stage rail protection, and install over rail, when operation requires. Operations may include core drilling over tracks, picking slabs and/or girders over tracks.
- Due to Bridge Damage, equipment to be placed on bridge deck will be limited to concrete saws (any additional equipment to be determined by demo plan evaluation with PE)
- Crane(s) shall be placed on timber crane mats (or other adequate dunnage) and placed on well compacted, level surface.
- Crane booms shall be rotated away from the tracks and the boom in the lowered position prior to passing trains.
- Dynamic hoisting operations are prohibited when carrying a load with the potential to foul the track. Crane(s) and other lifting equipment shall remain stationary during hoisting operations.
- Contractor and SCDOT to coordinate removal of all utilities prior to demo operations.
- Plunge cuts will be a combination of hand sawing, flat sawing, and/or wire sawing.
- Contractor to keep pressure washer on standby during saw cutting operations to keep rail free of slurry.
- Demolished elements of the bridge structure placed into material staging and processing areas, will be further processed by hoe ram(s), muncher(s), and excavator(s). Processed debris will be incorporated into roadway fill per SCDOT specifications or disposed of at approved location(s).
- Upon completion of demolition in each area, the Contractor may begin construction of the new bridge structure elements.

** Span 1 demo by slabbing out bridge deck / rail, and then picking girders off separately.

** Span 2 and 3 demo by removing barrier rail, and then cutting deck along middle of bay, and picking bridge deck and girders off as “T” unit.

Barrier rail and deck removal – Span 1

(Note: All dimensions for sections per final demo plan design calculations)

- Layout all saw cuts for bridge rail and deck removal.
- Core rigging holes thru concrete bridge deck for rigging holes. (must complete before slab picks as track time allows, not critical for barrier removal)
- Make plunge cuts through barrier and deck OH up to exterior girder flange.
- Connect rigging from crane to section to remove and ensure secure.
- Apply tension to barrier rail prior to making longitudinal saw cuts.
- Make longitudinal saw cut until barrier rail / deck OH is free.
- Remove section and set in material staging area for further processing.
- Repeat steps 4 thru 7, until all barrier / deck OH is removed from span.
- Make transverse saw cuts on concrete deck per lengths for deck slab sections.
- Make first longitudinal saw cuts for deck slab sections to pull (make sure saw cuts are over the top flange of the girders)
- Connect rigging from crane to deck slab section to remove and ensure secure.
- Apply tension to deck slab section prior to final saw cut.
- Make final longitudinal saw cut to free up the deck slab section.
- Remove deck slab section and set in material staging area for further processing.
- Repeat steps 11 thru 14, until all bridge deck slab sections are removed from span.

Girder removal – Span 1

(Note: Girder removal per final demo plan design calculations)

- Connect rigging from crane to girder(s) to remove.
- Cut girder(s) free at the bearings.
- Remove girder(s) and set in material staging area for further processing.
- Repeat steps 1 thru 3, until all girder(s) are removed from span.

Barrier rail, deck, and girder removal – Span 2 and 3

(Note: All dimensions for sections per final demo plan design calculations)

- Layout all saw cuts for bridge rail and deck/girder “T” unit removal.
- Core rigging holes thru concrete bridge deck for rigging holes. (along center of bays, must complete before slab picks as track time allows, not critical for barrier removal)
- Make plunge cuts through barrier to beyond the curb line.
- Connect rigging from crane to section to remove and ensure secure.
- Apply tension to barrier rail prior to making longitudinal saw cuts.
- Make longitudinal saw cut along curb line until barrier rail is free.
- Remove section and set in material staging area for further processing.
- Repeat steps 4 thru 7, until all barrier rail is removed from span.
- Make first pass on longitudinal saw cut down center of bay.
- Connect rigging from crane to support deck slab/girder “T” to remove and ensure secure.
- Apply tension to “T” unit prior to final saw cut.
- Make final saw cut to free up the deck slab section.
- Remove deck slab section and set in material staging area for further processing.
- Repeat steps 10 thru 13, until all “T” units are removed from span.
 - Note: prior to cutting and removing next to last “T” unit on each span, bracing of the last “T” unit deck slab/girder is required to support last “T” unit while next to last unit is being removed.

Interior bent removal – sequence for one bent (Typ) for each bent

(Note: Bent removal per final demo plan design calculations for section sizes)

- Saw cut bent Cap into section length.
- Connect rigging from crane to bent cap section to remove.
- Cut bent cap free from the column(s).
- Remove bent cap section and set in material staging area for further processing.
- Repeat steps 1 thru 4, until the bent cap is removed entirely.
- Connect rigging from crane to bent column section(s) to remove.
- Cut bent column free.
- Remove bent column section(s) and set in material staging area for further processing.
- Repeat steps 6 thru 8, until all column section(s) to saw cut are removed.
- The remaining column section(s) and foundation to be removed utilizing temporary shoring, excavator, and hoe ram(s) to remove the foundation to 3 ft below natural ground line, per the RFP and CSX project manual.



End bent removal – sequence for one bent (Typ) for each bent

(Note: Bent removal per final demo plan design calculations for section sizes)

- End bent cap and wing walls will be removed utilizing a hoe ram and excavator.
- Excavate earth material from structure excavation zone.
- Remove the concrete crib retaining wall utilizing hoe ram, muncher as needed as excavation is progressed.
- Complete excavation for full zone, until back to the new slope line and end bent location.



REV.			
REV.			
REV.			
REVIEWED			
QUAN.			
DR.			
DES.			
	BY	CHK.	DATE



SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION

BRIDGE DEMOLITION NOTES


COUNTY
BEAUFORT / HAMPTON

ROUTE
US 17A / US 21

Appendix B - Required Forms, and Confidential and Proprietary Information Page List



This document is bookmarked for your convenience.
Green and underlined text within this document indicates
a **HYPERLINK** and will take you to more detailed information.

To return to your previous location, simply type  + 



12. EQUAL EMPLOYMENT OPPORTUNITY CERTIFICATION

(COMPLETE THIS SECTION FOR FEDERAL PROJECTS ONLY) EQUAL EMPLOYMENT OPPORTUNITY PERFORMANCE

Select the Certification that applies to the PROPOSER:

Certification (1) ☒

or

Certification (2) ☐

Select the appropriate responses in the applicable Certification:

Certification (1): Pursuant to 41 C.F.R. §60-1.7(b)(1), Previous Equal Employment Opportunity Performance Certification, as the Prospective Prime Contractor, I HEREBY CERTIFY THAT I:

(a) (HAVE) / HAVE NOT developed and filed an Affirmative Action Program pursuant to 41 C.F.R. §60-2 and/or 60-4;

(b) (HAVE) / HAVE NOT participated in a previous contract or subcontract subject to the equal opportunity clause;

(c) (HAVE) / HAVE NOT filed with the Joint Reporting Committee, the Director of Office of Federal Contract Compliance, or the Equal Employment Opportunity Commission, all reports due under the applicable filing requirements,

OR

Certification (2): I, HEREBY CERTIFY that as the Prospective Prime Contractor submitting this Proposal, **(CLAIM / DO NOT CLAIM)** exemption from the submission of the Standard Form 100 (EEO-1) due to the fact that it employs a total of less than fifty (50) employees under C.F.R. §60-1.7, or qualifies for an exempted status under 41 C.F.R. §60-1.5.

I FURTHER CERTIFY that the above Certification will be made part of any Subcontract Agreement involved with this project.

Executed on 12/11, 2023.

Signed: [Signature]
(Officer/PROPOSER)

Title: Vice President

Company: Reeves Construction Company

Address: 248 Plummer Rd
Durham, SC 29374

Note: The above certification is required by the Equal Employment Opportunity Regulations of the Secretary of Labor (41 CFR 60-1.7(b)(1)), and must be submitted by PROPOSERS only in connection with contracts which are subject to the equal opportunity clause. Contracts that are exempt from the equal opportunity clause are set forth in 41 CFR 60-1.5. (Generally, only contracts of \$10,000 or under are exempt.)

Currently, Standard Form 100 (EEO-1) is the only report required by Executive Orders or their implementing regulations.

Proposers, Primary Members, or proposed Contractors and Consultants who have participated in a previous contract subject to the Executive Orders and have not filed the required reports shall note that 41 CFR 60-1.7(b)(1) prevents the award of contracts and subcontracts unless such contractor submits a report covering the delinquent period or such other period specified by the Federal Highway Administration or by the Director, Office of Federal Contract Compliance, U.S. Department of Labor.



11. NON-COLLUSION CERTIFICATION

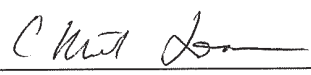
NON-COLLUSION CERTIFICATION

Project ID: P042942

IN ACCORDANCE WITH THE PROVISIONS OF S.C. CODE ANN. §§ 39-3-10 ET. SEQ., 39-5-10 ET. SEQ., 15 U.S.C. §45; 23 C.F.R. §635.112(F); AND 28 U.S.C. §1746, I HEREBY ACKNOWLEDGE THAT I AM AN OFFICER OF THE PROPOSER FIRM AND, UNDER PENALTY OF PERJURY UNDER THE LAWS OF THE UNITED STATES AND SOUTH CAROLINA, DECLARE, BY MY CERTIFICATION BELOW, THAT THE FOLLOWING IS TRUE AND CORRECT, AND FURTHER, THAT THIS FIRM, ASSOCIATION OR CORPORATION HAS NOT, EITHER DIRECTLY OR INDIRECTLY, ENTERED INTO ANY AGREEMENT, PARTICIPATED IN ANY COLLUSION, OR OTHERWISE TAKEN ANY ACTION IN RESTRAINT OF FREE COMPETITIVE BIDDING IN CONNECTION WITH THE SUBMISSION OF A BID PROPOSAL ON THE ABOVE REFERENCED PROJECT.

BY CHECKING THIS BOX ☒, I CERTIFY THAT I HAVE READ, UNDERSTAND, ACCEPT, AND ACKNOWLEDGE ALL OF THE ABOVE STATEMENTS.

Executed on 12-11-23
(Date)

Signed: 
(Officer/Proposer)

Vice President
(Title)

Reeves Construction Company
(Address)

248 Plemons Rd.
Duncan, SC 29334



South Carolina
Department of Transportation

NOTICE TO PROPOSERS

US 17A/21 over CSX Emergency Bridge Replacement Design-Build Project – Project ID P042942 Beaufort and Hampton Counties

December 1, 2023

NOTICE TO PROPOSERS - Enclosed is **Addendum 1** to the Request for Proposals (RFP) for the US 17A/21 over CSX Emergency Bridge Replacement design-build project. The information provided in this notice and the addendum shall be made part of the contract documents.

The **yellow** highlights identify the revisions associated with Addendum 1.

This addendum is being issued in order to provide clarification and additional information for the project. The following sections of the RFP contain revisions:

- Agreement
- Instructions
- Exhibit 3
- Exhibit 4a
- Exhibit 4b
- Exhibit 4d – Part 1
- Exhibit 4f
- Attachment B
- Project Information Package



Post Office Box 191
Columbia, South Carolina 29202-0191

Phone: (803) 737-2314
TTY: (803) 737-3870

AN EQUAL OPPORTUNITY
AFFIRMATIVE ACTION EMPLOYER



South Carolina
Department of Transportation

NOTICE TO PROPOSERS

US 17A/21 over CSX Emergency Bridge Replacement Design-Build Project – Project ID P042942 Beaufort and Hampton Counties

Addendum 1

The information in this addendum shall be made part of the contract documents. PROPOSERS are instructed to incorporate the information into the previously provided RFP documents.

PROPOSERS are required to sign this document and enclose it with their Technical Proposal. Receipt of this signed document by The South Carolina Department of Transportation serves as confirmation that the PROPOSER has received and incorporated this Addendum into the contract documents.

Confirmation Statement:

I, the PROPOSER confirm that I have received this addendum package and have incorporated the information provided in the addendum into the contract documents.

PROPOSER's Signature

12-11-23

Date

C Robert Loo

Printed Name

For:
Design-Build Team Name





South Carolina
Department of Transportation

NOTICE TO PROPOSERS

US 17A/21 over CSX Emergency Bridge Replacement Design-Build Project – Project ID P042942 Beaufort and Hampton Counties

December 8, 2023

NOTICE TO PROPOSERS - Enclosed is **Addendum 2** to the Request for Proposals (RFP) for the US 17A/21 over CSX Emergency Bridge Replacement design-build project. The information provided in this notice and the addendum shall be made part of the contract documents.

The yellow highlights identify the revisions associated with Addendum 1. The green highlights identify the revisions associated with Addendum 2.

This addendum is being issued in order to provide clarification and additional information for the project. The following sections of the RFP contain revisions:

- Agreement
- Instructions
- Exhibit 4c
- Exhibit 4z
- Exhibit 6
- Exhibit 8
- Attachment B



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AFFIRMATIVE ACTION EMPLOYER



South Carolina
Department of Transportation

NOTICE TO PROPOSERS

US 17A/21 over CSX Emergency Bridge Replacement Design-Build Project – Project ID P042942 Beaufort and Hampton Counties

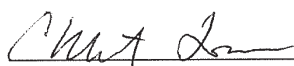
Addendum 2

The information in this addendum shall be made part of the contract documents. PROPOSERS are instructed to incorporate the information into the previously provided RFP documents.

PROPOSERS are required to sign this document and enclose it with their Technical Proposal. Receipt of this signed document by The South Carolina Department of Transportation serves as confirmation that the PROPOSER has received and incorporated this Addendum into the contract documents.

Confirmation Statement:

I, the PROPOSER confirm that I have received this addendum package and have incorporated the information provided in the addendum into the contract documents.



PROPOSER's Signature

12-14-23

Date

C Robert Loar

Printed Name

For: Reeves Construction Company

Design-Build Team Name





South Carolina
Department of Transportation

NOTICE TO PROPOSERS

US 17A/21 over CSX Emergency Bridge Replacement Design-Build Project – Project ID P042942 Beaufort and Hampton Counties

December 13, 2023

NOTICE TO PROPOSERS - Enclosed is **Addendum 3** to the Request for Proposals (RFP) for the US 17A/21 over CSX Emergency Bridge Replacement design-build project. The information provided in this notice and the addendum shall be made part of the contract documents.

The yellow highlights identify the revisions associated with Addendum 1. The green highlights identify the revisions associated with Addendum 2. The blue highlights identify the revisions associated with Addendum 3.

This addendum is being issued in order to provide clarification and additional information for the project. The following sections of the RFP contain revisions:

- Agreement
- Instructions
- Exhibit 3
- Exhibit 4b
- Exhibit 5
- Exhibit 8
- Attachment B
- Project Information Package



Post Office Box 191
Columbia, South Carolina 29202-0191

Phone: (803) 737-2314
TTY: (803) 737-3870

AN EQUAL OPPORTUNITY
AFFIRMATIVE ACTION EMPLOYER



South Carolina
Department of Transportation

NOTICE TO PROPOSERS

US 17A/21 over CSX Emergency Bridge Replacement Design-Build Project – Project ID P042942 Beaufort and Hampton Counties


Addendum 3

The information in this addendum shall be made part of the contract documents. PROPOSERS are instructed to incorporate the information into the previously provided RFP documents.

PROPOSERS are required to sign this document and enclose it with their Technical Proposal. Receipt of this signed document by The South Carolina Department of Transportation serves as confirmation that the PROPOSER has received and incorporated this Addendum into the contract documents.

Confirmation Statement:

I, the PROPOSER confirm that I have received this addendum package and have incorporated the information provided in the addendum into the contract documents.


PROPOSER's Signature

12-14-23
Date

C Robert Loar
Printed Name

For: Reeves Construction Company
Design-Build Team Name





DISCLOSURE OF POTENTIAL CONFLICT OF INTEREST CERTIFICATION

PROPOSER hereby indicates that it has, to the best of its knowledge and belief has:

☒ Determined that no potential organizational conflict of interest exists.

Determined a potential organizational conflict of interest as follows:

Attach additional sheets as necessary.

1. Describe nature of the potential conflict(s):

N/A

2. Describe measures proposed to mitigate the potential conflict(s):

$$N/A$$


Signature

12-11-23
Date

C Robert Low
Print Name

Reeves Construction Company
Company

If a potential conflict has been identified, please provide name and phone number for a contact person authorized to discuss this disclosure certification with Department of Transportation contract personnel.

Name _____

Phone

Company



DISCLOSURE OF POTENTIAL CONFLICT OF INTEREST CERTIFICATION

X Determined that no potential organizational conflict of interest exists.

Determined a potential organizational conflict of interest as follows:

1. Describe nature of the potential conflict(s):

2. Describe measures proposed to mitigate the potential conflict(s):

Quinn K. Shinn

Signature

December 7, 2023

Date _____

Brian K. Skinner, PE

Print Name

Rummel, Klepper & Kahl, LLP (RK&K)

Company

If a potential conflict has been identified, please provide name and phone number for a contact person authorized to discuss this disclosure certification with Department of Transportation contract personnel.

Name

Phone

Company



Confidential and Proprietary Information Page List

As required by the Request for Proposals, our Team shall include a list of page numbers that contain confidential and/or proprietary information. We understand that failure to include this list in the Technical Proposal appendix waives the confidentiality protection and subjects the information to disclosure in accordance with the law.

Reeves Construction Company (Contractor) and Rummel, Klepper & Kahl, LLP (RK&K) (Lead Designer) have identified no items that we consider confidential and proprietary information.



 250 Plemmons Road, Duncan, South Carolina 29334
 864.416.0200  <https://reevescc.com>