

Technical Proposal



**SCDOT | Design-Build Project**

**S-2-105 (Old Vacluse Road) over Interstate 20 Emergency**

**Bridge Replacement**

**Project ID P044879**

**Aiken County, South Carolina**



*Submitted by:*

**UNITED**  
INFRASTRUCTURE GROUP

**UNITED**  
ENGINEERING & CONSULTING  
A UNITED INFRASTRUCTURE GROUP COMPANY





# TECHNICAL NARRATIVE



## INTRODUCTION

**Contracting Entity:** United Infrastructure Group, Inc. (UIG), a South Carolina Corporation, will be the Contracting Entity for the S-2-105 (Old Vacluse Road) over Interstate 20 Emergency Bridge Replacement.

**Proposer's Point of Contact for Procurement:**

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 Great Falls, SC 29045

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**United Infrastructure Group, Inc.**  
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 5562 Pendergrass Boulevard  
 Great Falls, SC 29045

**G. Rett Triplett, PE**  
**United Engineering & Consulting, LLC**  
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 5562 Pendergrass Boulevard  
 Great Falls, SC 29045

**Full Legal Name of Lead Contractor and Lead Designer:** United Infrastructure Group, Inc. will be Lead Contractor and United Engineering & Consulting, LLC (UEC) will be Lead Designer.

**Commitment Statement:** The Design and Construction Teams have ample available resources and are committed to meeting SCDOT's quality and schedule expectations.

## PROJECT DELIVERY AND APPROACH

**Project Approach, Capacity, & Resources**

The successful delivery of this emergency design-build project requires focused execution across three primary categories: Design & Preconstruction, Construction, and Quality Control & Assurance. These tasks are summarized in the table below. Our team possesses the expertise, resources, and integrated workflows to efficiently and effectively carry out each task, minimizing schedule risk while maintaining quality and compliance with SCDOT requirements.

Major Tasks Involved for the Successful Completion of the Project		
Design & Preconstruction	Construction	Quality Control & Assurance
<ul style="list-style-type: none"> <li>• Design Management</li> <li>• Structures Design &amp; Final Plans</li> <li>• Roadway Design &amp; Final Plans</li> <li>• Geotechnical Exploration &amp; Design</li> <li>• Erosion and Sediment Control Plans</li> <li>• Hydraulic / Hydrologic Design</li> <li>• Seismic Design</li> <li>• Utility Coordination</li> <li>• TMP (Traffic Control/MOT/PR)</li> <li>• QC/QA prior to Design Submittals</li> <li>• Design &amp; Schedule Submittals</li> <li>• Construction Engineering</li> <li>• Shop Drawings</li> <li>• Scope and Activity Planning</li> </ul>	<ul style="list-style-type: none"> <li>• Construction Management</li> <li>• Utility Coordination</li> <li>• Construction Signs</li> <li>• Traffic Control &amp; Maintenance</li> <li>• Clearing &amp; Erosion Control</li> <li>• Structures &amp; Demolition</li> <li>• Earthwork &amp; Drainage</li> <li>• NPDES Compliance &amp; Inspections</li> <li>• Subgrade &amp; Base Preparation</li> <li>• Asphalt Paving</li> <li>• Guardrail, Cable Barrier</li> <li>• Concrete Barrier Wall</li> <li>• Marking and Signing</li> <li>• As Built Plan Preparation/Reviews</li> </ul>	<ul style="list-style-type: none"> <li>• Development, Approval &amp; Implementation of QC Plan</li> <li>• Foundation Testing</li> <li>• Erosion &amp; Sediment Control Monitoring</li> <li>• Permit and NEPA Compliance</li> <li>• QC Inspection &amp; Construction Oversight</li> <li>• Materials Sampling &amp; Testing</li> <li>• Materials Certification/Document Control</li> <li>• Quality Acceptance and independent assurance testing (by SCDOT)</li> </ul>

United Engineering & Consulting (UEC) is the Lead Designer and, although a newer firm, is composed of seasoned staff with extensive SCDOT bridge and project design experience. Our engineers have a strong track record of SCDOT bridge and design-build delivery, including successful fast-track and emergency projects, who maintain a strong and integrated working relationship with United Infrastructure Group (UIG), ensuring seamless coordination between design and construction.



In addition to permanent bridge and roadway design, **UEC will also provide Construction Engineering Services (CES)**, including formwork and temporary works design. Because UEC is continuously integrated with UIG's estimating, preconstruction, and construction teams, we deliver constructible, field-informed design solutions and minimize lag time in communication between disciplines. This streamlined approach improves both the quality and speed of execution for permanent and temporary works. To avoid any conflict of interest, all Construction Engineering Services (CES) provided by UEC—including formwork and temporary works design—will be developed by personnel independent from those responsible for the sealed permanent bridge design. Internal QA/QC procedures will ensure a clear separation of responsibilities, and all CES will be peer-reviewed and sealed by a qualified Professional Engineer who did not participate in the design of the permanent structure. This approach maintains design integrity and aligns with SCDOT and FHWA expectations for professional independence and constructability assurance.

### Preconstruction & Design

Our team's design approach for the emergency replacement of the S-2-105 bridge is driven by a singular priority: rapid, constructible delivery of a permanent solution that meets SCDOT's goals for safety, cost-effectiveness, and long-term adaptability. The proposed replacement bridge will be constructed on existing alignment and consist of two continuous for live load 90'-0" spans utilizing 36" FIB prestressed concrete girders. The superstructure is supported by reinforced concrete integral end bents founded on steel piles with pile sleeves, and a reinforced concrete interior bent supported by two columns and individual footings also founded on steel piles. The end bents are retained with mechanically stabilized earth (MSE) walls, which offer rapid installation and long-term performance. This structure was deliberately configured to minimize bridge superstructure depth and length, thereby **reducing roadway tie-in lengths and excavation**, which directly contributes to faster construction and lower costs. At the same time, the bridge is future-ready, providing **72 feet of horizontal clearance** in both directions of I-20 to fully accommodate two additional lanes in each direction during future widening. The structure also satisfies current SCDOT vertical clearance requirements of 17 feet and has been **strategically designed to accommodate a future lowering of I-20 by up to 7 feet**, with sufficient MSE wall and substructure depth to handle such grade modifications without retrofitting. The proposed bridge and roadway tie-ins have been designed to **remain entirely within the existing right-of-way**, eliminating the need for ROW acquisition and supporting SCDOT's goal of minimizing impacts to adjacent properties and access points. This approach also accelerates the design approval process and avoids delays associated with ROW coordination. In order to compress the project delivery timeline, the design team has **voluntarily progressed the plans beyond typical technical proposal requirements**—currently approaching 60% plan development. Post-award, our team will **bypass the preliminary and right-of-way plan submittals** and instead move directly into final plan development. We are fully committed to submitting the Final Plans **by the end of September**, an aggressive but realistic timeline enabled by our internal QA/QC workflows and the high level of preparedness demonstrated in our early design development.



## Construction

United Infrastructure Group (UIG) will self-perform all major scopes of work, leveraging our extensive emergency response experience, in-house personnel, heavy equipment fleet, and long-established field leadership. This includes bridge substructure and superstructure construction, MSE wall installation, grading, and drainage. By self-performing these elements, we maintain **maximum control over safety, quality, and schedule**, eliminating potential delays associated with subcontractor availability or mobilization.

**Only minor scopes of work will be subcontracted:** the most significant of these are asphalt paving, permanent concrete barrier wall, and traffic control (which will be managed by UIG). All major scope items will be executed by UIG's internal resources, ensuring schedule certainty and cost control.

All **major equipment is company-owned**, removing dependence on rental timelines or third-party operators. Our team will mobilize immediately upon NTP and approval of required plans, beginning with **installation of temporary concrete barrier adjacent to I-20**, followed by excavation for the end bent MSE walls within the existing embankments, installation of end bent H-pile, and installation of drilled shaft median bent foundations.

Utility coordination will be proactively managed by UIG's **dedicated Utility Coordinator**, who will engage SEGRA and any other impacted utility owners immediately after award. Early identification and conflict resolution will prevent delays during construction.

## Assurances and Ability to Complete Project within the Required Timeframe

Our team is committed to delivering this emergency replacement bridge ahead of schedule. Specific assurances include:

- ✓ A **proven track record** of executing emergency bridge replacements for decades, and across four generations.
- ✓ Deep **design-build experience with SCDOT**, including projects of similar complexity and urgency.
- ✓ A **design already advanced** beyond the proposal stage, allowing accelerated final design submittal and early procurement of long-lead materials.
- ✓ Exceptional **familiarity with the project site**, due to prior involvement in a proposed interchange at this location and through United Demolition's emergency bridge removal.
- ✓ **Detailed understanding of the RFP** and all referenced design criteria, allowing for design decisions that meet both the letter and intent of SCDOT's expectations.
- ✓ **Geotechnical familiarity**, through F&ME's completion of preliminary borings for SCDOT, eliminating delays related to subsurface uncertainty.
- ✓ Access to **over eighty fully equipped construction crews**, with three expected to work on this project.
- ✓ A **seasoned Project Manager, Lead Engineer, and Superintendent team** with long-standing internal relationships and established lines of communication, enabling rapid decision-making.

**We are fully prepared to mobilize all necessary design and construction resources to ensure this project is delivered ahead of schedule, with no compromise to quality or safety.**

## CONCEPTUAL BRIDGE AND ROADWAY PLANS

The Conceptual Bridge and Roadway Plans are provided in **APPENDIX A**.

## REQUIRED FORMS

**APPENDIX B** includes all Required Forms, Confidential and Proprietary Information Page List.





# APPENDIX A:

## *Conceptual Plans*



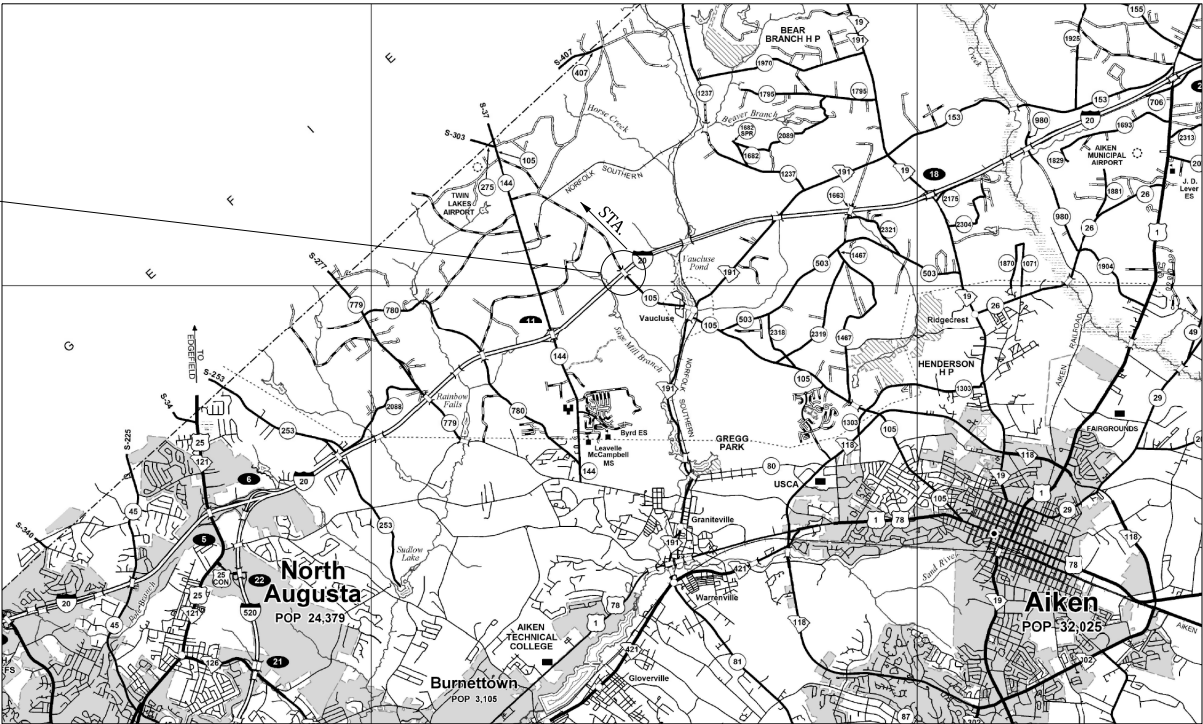
INDEX OF SHEETS

- 1. TITLE SHEET
- 2. BRIDGE PLAN AND PROFILE
- 3. END BENT 1 & 3
- 4. INTERIOR BENT 2
- 5. SUPERSTRUCTURE TYPICAL SECTION
- 6. MSE WALL 1 PLAN AND PROFILE
- 7. MSE WALL 2 PLAN AND PROFILE



PROPOSED PLANS  
FOR  
AIKEN COUNTY  
PROJECT ID P044879  
S-2-150 (OLD VAUCLUSE ROAD) OVER I-20  
BRIDGE REPLACEMENT

SITE LOCATION



LAYOUT

APPROXIMATE LOCATION OF BRIDGE IS  
LATITUDE 33°- 37' - 15.3" N  
LONGITUDE 81°- 49' - 26.7" W

-N-

3 DAYS BEFORE DIGGING IN  
SOUTH CAROLINA  
CALL 811  
SOUTH CAROLINA 811 (SC811)  
WWW.SC811.COM  
ALL UTILITIES MAY NOT BE A MEMBER OF SC811

ASSET ID 5198

TRAFFIC DATA

2025 ADT 1,700 V.P.D.  
2045 ADT 5,100 V.P.D.  
TRUCKS 3 %

NET LENGTH OF ROADWAY	0.161	MILES
NET LENGTH OF BRIDGES	0.034	MILES
NET LENGTH OF PROJECT	0.195	MILES
LENGTH OF EXCEPTIONS	0.000	MILES
GROSS LENGTH OF PROJECT	0.195	MILES

NOTE: EXCEPT AS MAY OTHERWISE BE SPECIFIED ON THE PLANS OR IN THE SPECIAL PROVISIONS, ALL MATERIALS AND WORKMANSHIP ON THIS PROJECT SHALL CONFORM TO THE SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION AND THE STANDARD DRAWINGS FOR ROAD CONSTRUCTION IN EFFECT AT THE TIME OF LETTING.

REVIEWED DLK  
DR. JMV OAA 7-25  
BY CHK. DATE  
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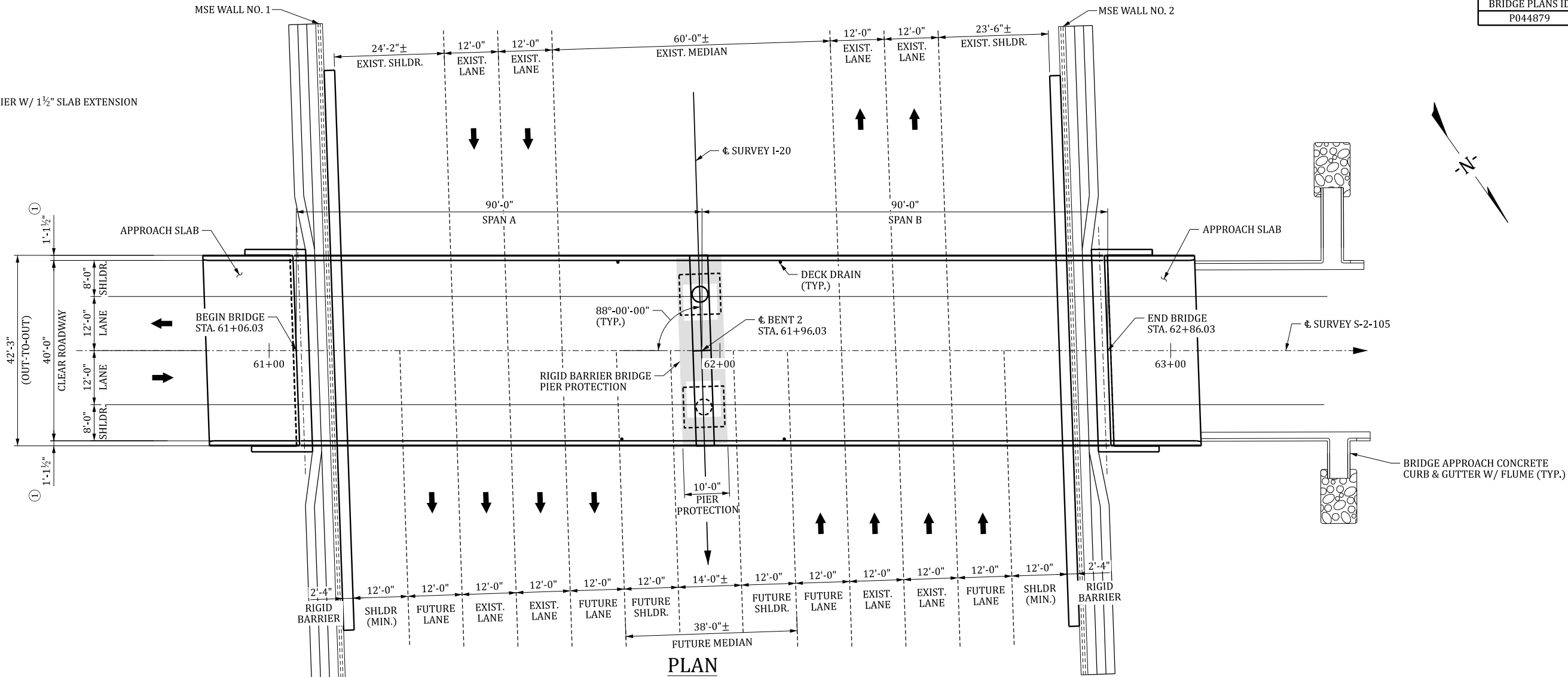


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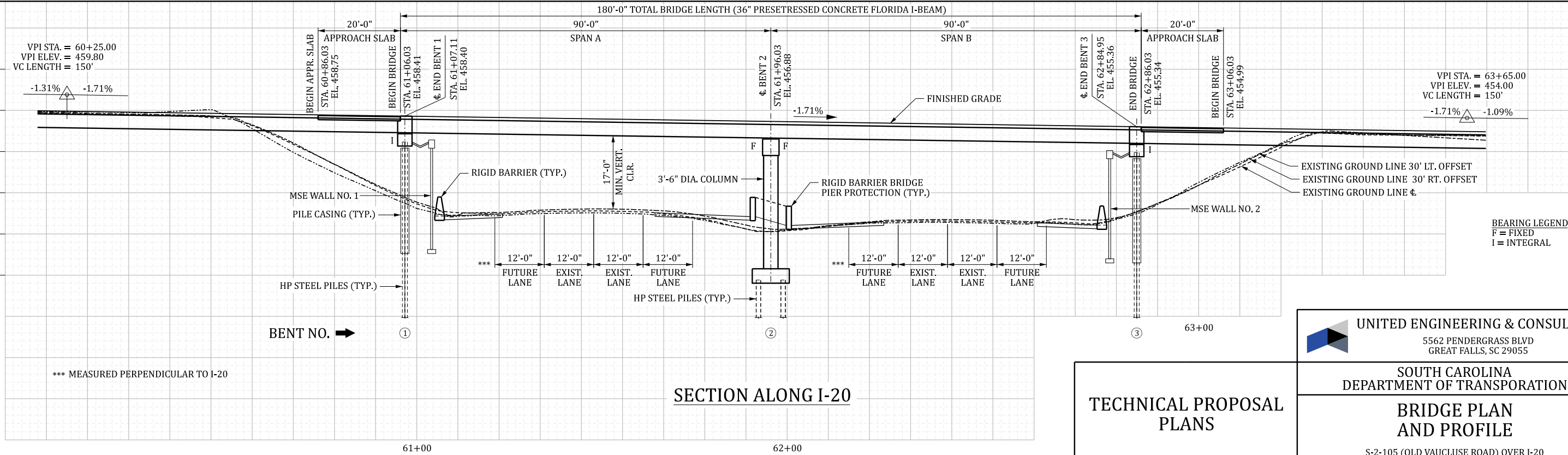
REVIEWED DLK			
QUAN.	DR.	AAA	OAA
DES.	BY	CHK.	DATE

REV.	BY	CHK.	DATE	DESCRIPTION OF REVISION

① 1'-0" BARRIER W/ 1½" SLAB EXTENSION




PLAN



SECTION ALONG I-20

TECHNICAL PROPOSAL  
PLANS



UNITED ENGINEERING & CONSULTING  
5562 PENDERGRASS BLVD  
GREAT FALLS, SC 29055

SOUTH CAROLINA  
DEPARTMENT OF TRANSPORTATION

**BRIDGE PLAN  
AND PROFILE**

S-2-105 (OLD VAUCLUSE ROAD) OVER I-20

COUNTY: AIKEN

ROUTE: S-2-105

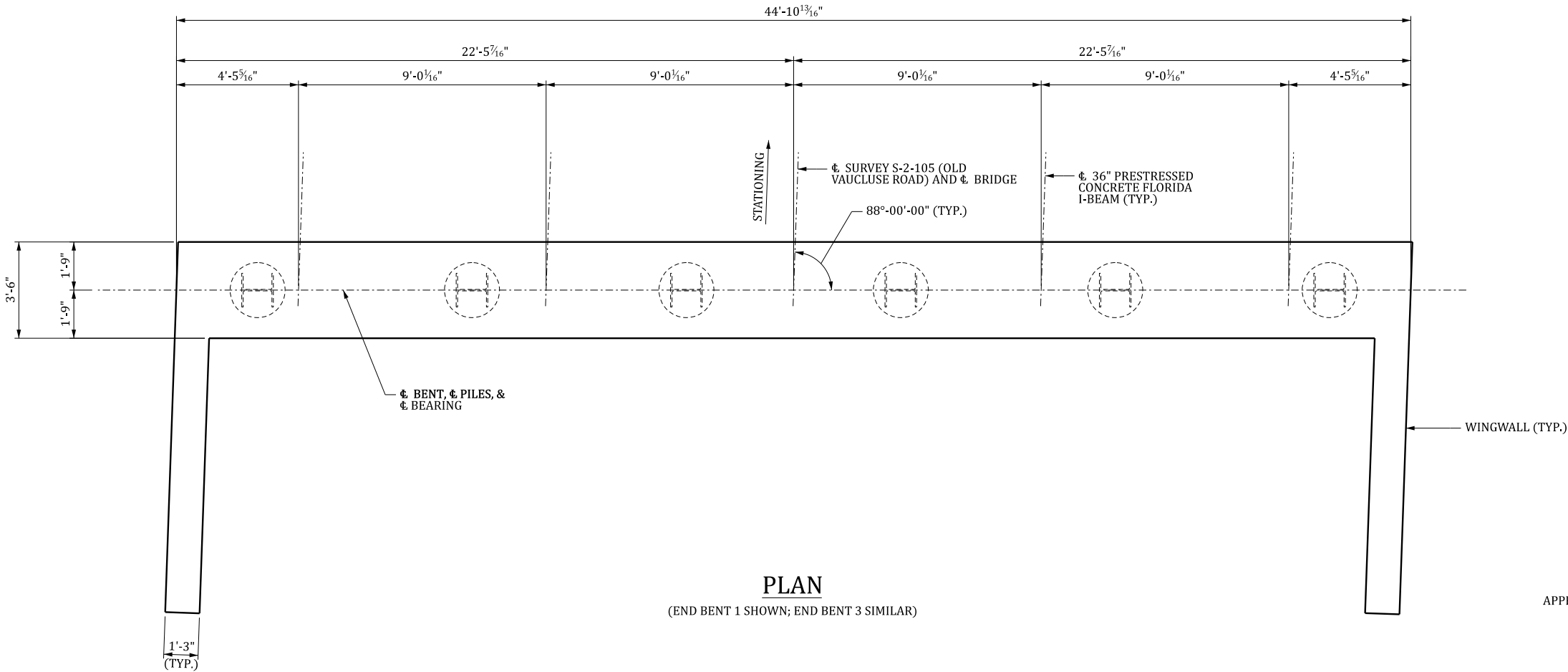


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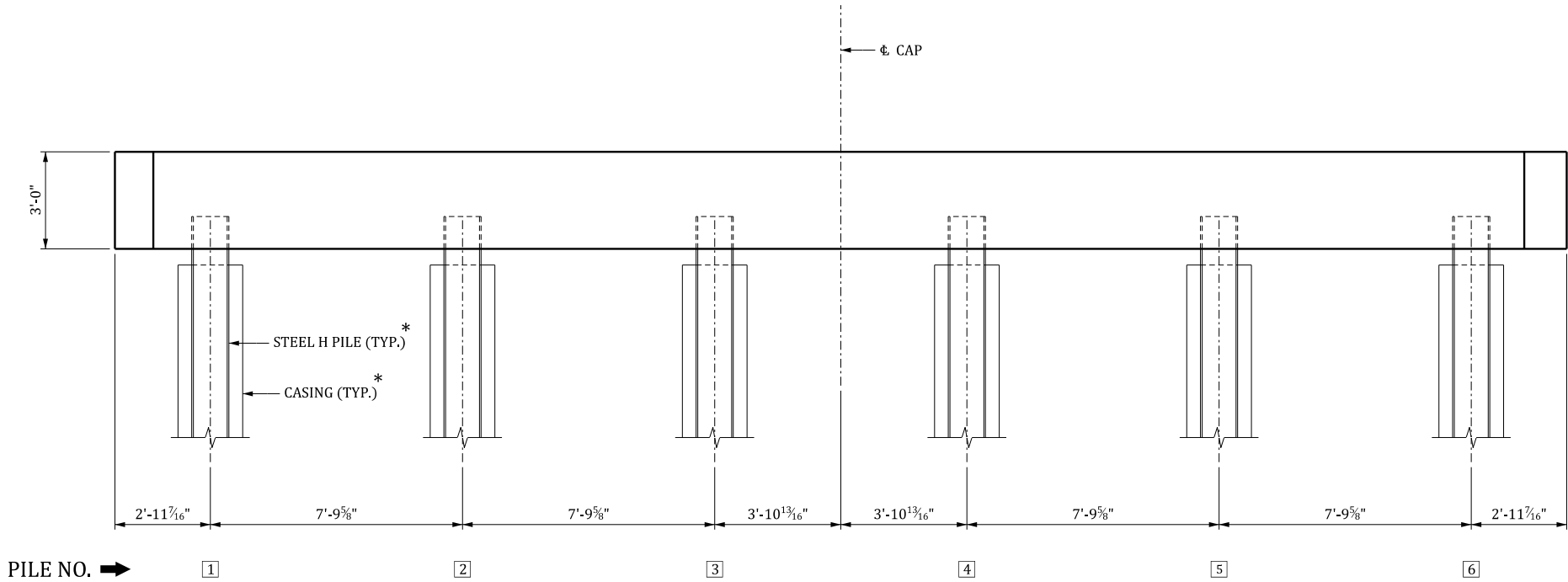
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QUAN.	DR.	DES.	BY	CHK.	DATE

BRIDGE PLANS ID	SHEET
P044879	3

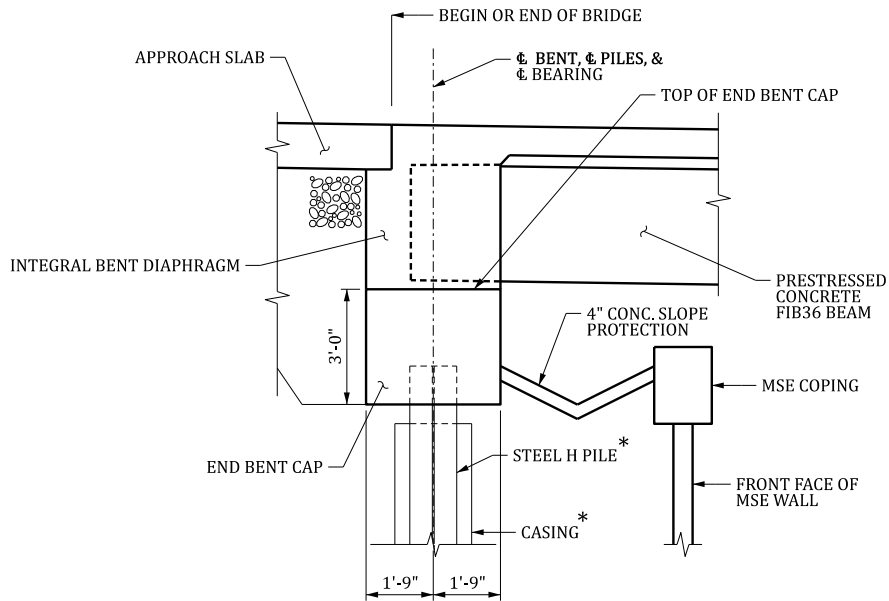


PLAN  
(END BENT 1 SHOWN; END BENT 3 SIMILAR)



ELEVATION  
(LOOKING IN DIRECTION OF STATIONING - END BENT 1)  
(LOOKING IN OPPOSITE DIRECTION OF STATIONING - END BENT 3)

\* FOUNDATION SIZE DEPENDENT UPON  
FINAL GEOTECHNICAL REPORT



SECTION THRU END BENT

TECHNICAL PROPOSAL  
PLANS



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GREAT FALLS, SC 29055

SOUTH CAROLINA  
DEPARTMENT OF TRANSPORTATION

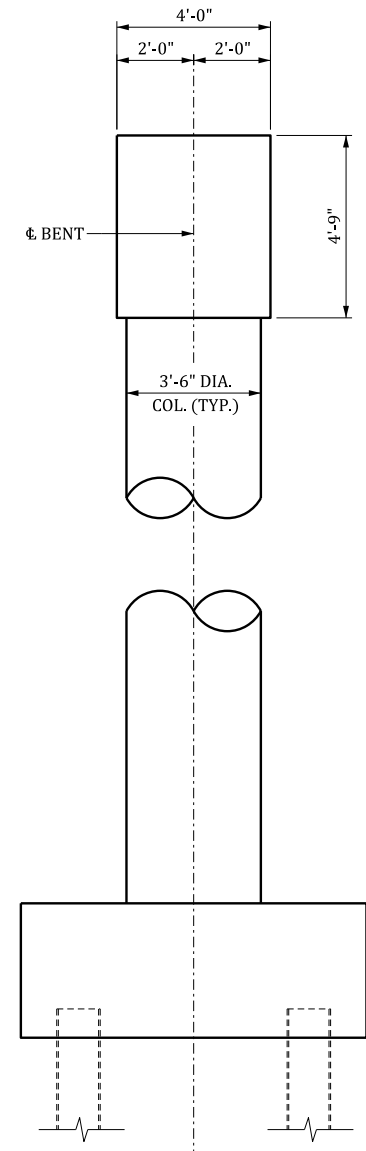
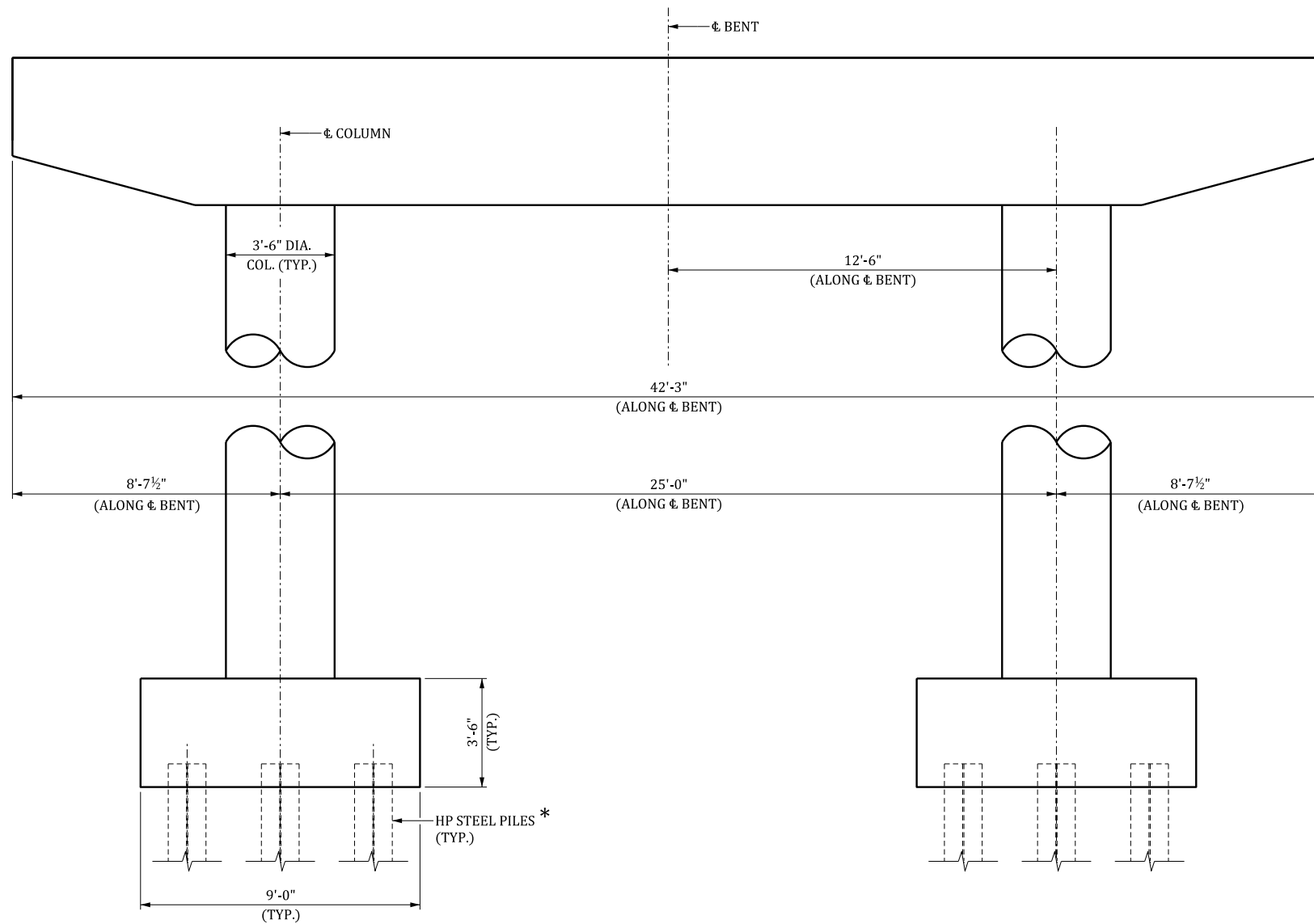
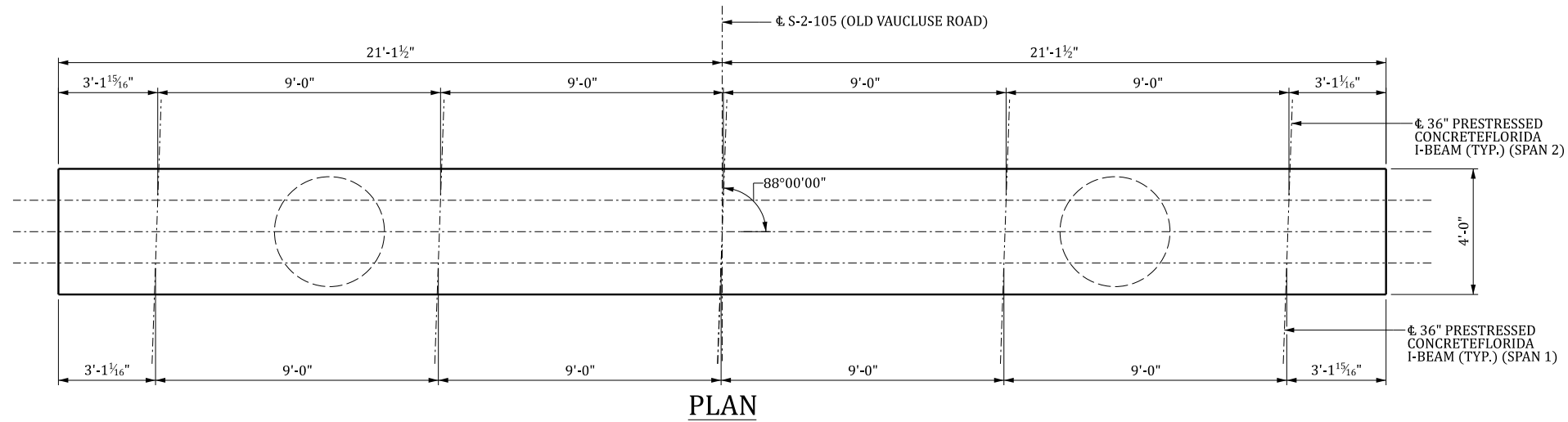
END BENT 1 & 3

S-2-105 (OLD VAUCLUSE ROAD) OVER I-20

COUNTY: AIKEN

ROUTE: S-2-105





SECTION THRU BENT

REVIEWED	DLK		
QUAN.			
DR.	BPK	JMV	6-25
DES.			
BY	CHK.	DATE	

REV.			
REV.			
REV.			
BY	CHK.	DATE	DESCRIPTION OF REVISION

REVIEWED DLK			
QUAN.			
DR.	BPK	JMV	6-25
DES.			
	BY	CHK.	DATE

COLUMN NO. ➡

1

### ELEVATION

\* FOUNDATION SIZE DEPENDENT UPON  
FINAL GEOTECHNICAL REPORT

2

## TECHNICAL PROPOSAL PLANS



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GREAT FALLS, SC 29055

SOUTH CAROLINA  
DEPARTMENT OF TRANSPORTATION

## INTERIOR BENT 2

S-2-105 (OLD VAUCLUSE ROAD) OVER I-20

COUNTY: AIKEN

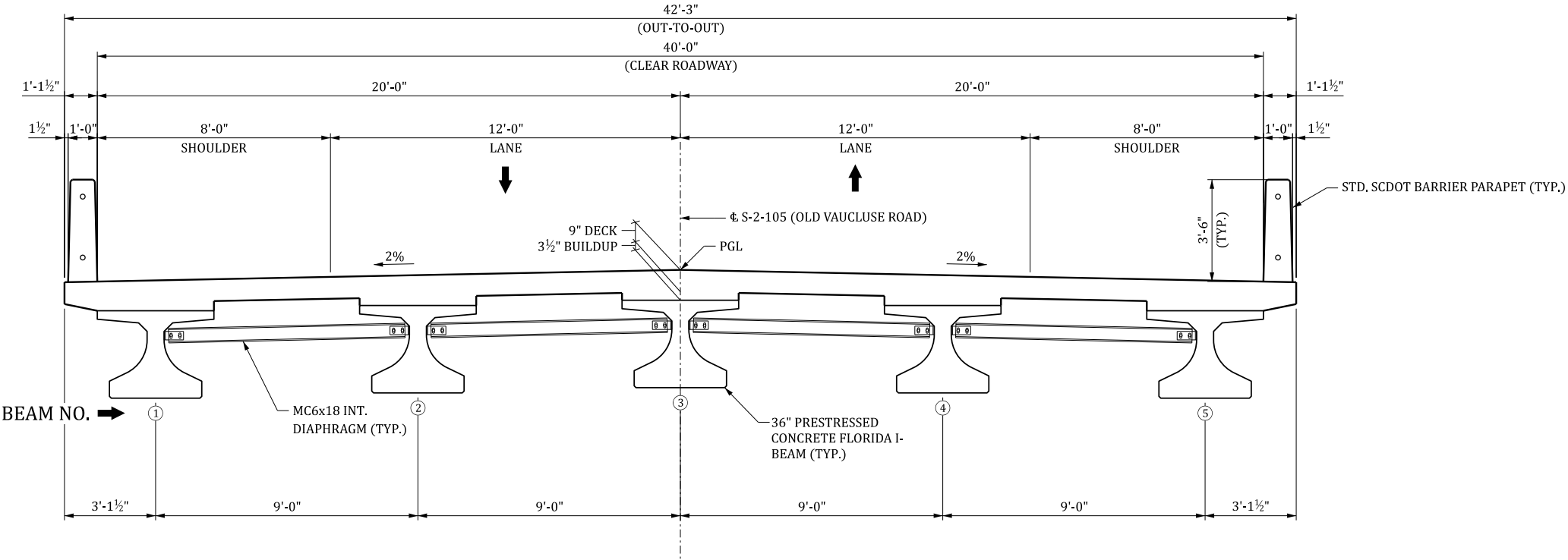
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REV.					
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BY	CHK.	DATE	DESCRIPTION OF REVISION		

REVIEWED	DLK				
QUAN.					
DR.	AAA	OAA	7-25		
DES.					
BY	CHK.	DATE			



TYPICAL SECTION

TECHNICAL PROPOSAL  
PLANS



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GREAT FALLS, SC 29055

SOUTH CAROLINA  
DEPARTMENT OF TRANSPORTATION

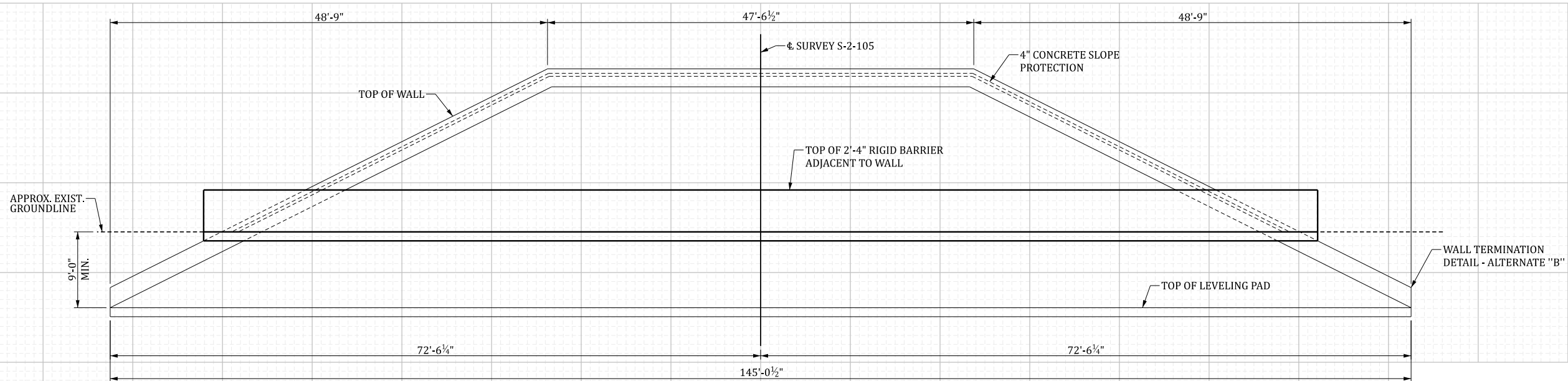
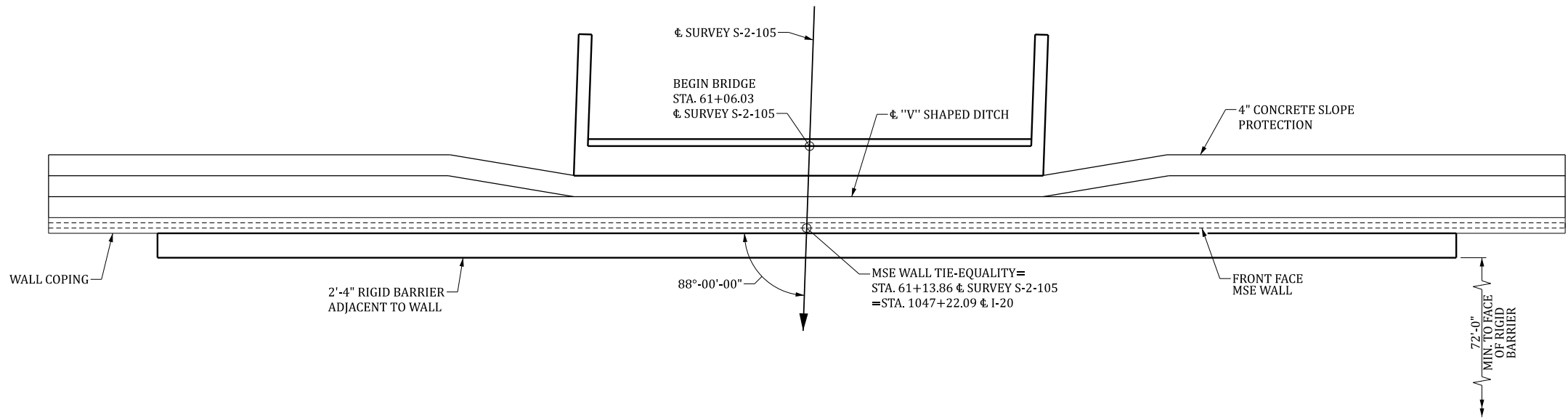
SUPERSTRUCTURE  
TYPICAL SECTION

S-2-105 (OLD VAUCLUSE ROAD) OVER I-20

COUNTY: AIKEN

ROUTE: S-2-105





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REV.	BY	CHK.	DATE	DESCRIPTION OF REVISION

QUAN.	DLK	GET	DATE

ELEVATION - MSE WALL 1

TECHNICAL PROPOSAL  
PLANS



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GREAT FALLS, SC 29055

SOUTH CAROLINA  
DEPARTMENT OF TRANSPORTATION

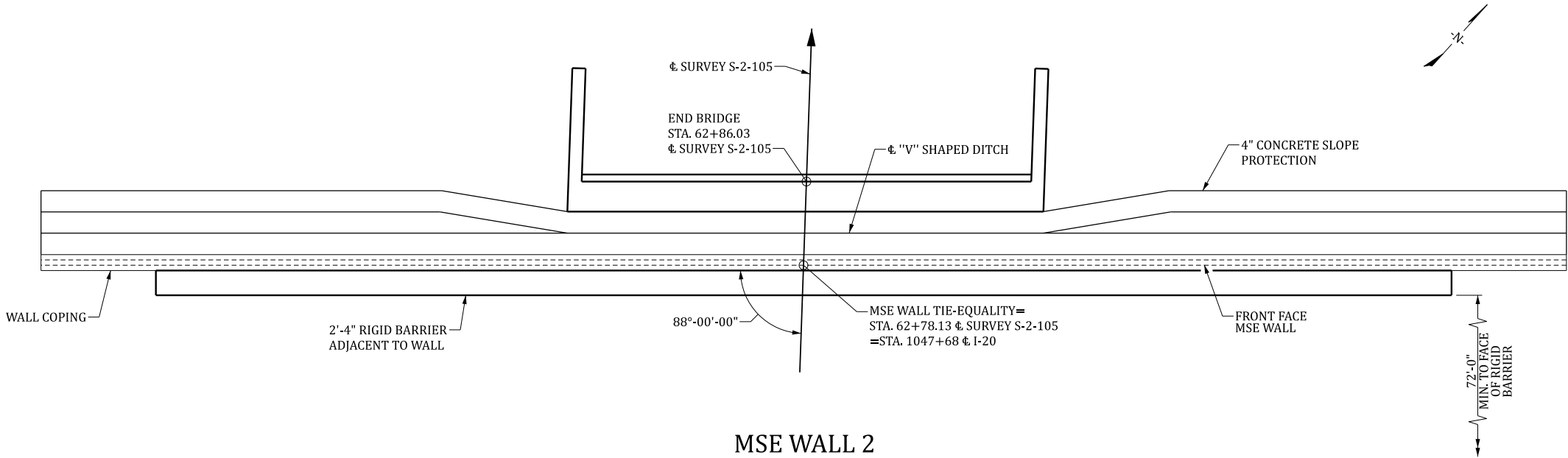
MSE WALL 1  
PLAN AND PROFILE

S-2-105 (OLD VAUCLUSE ROAD) OVER I-20

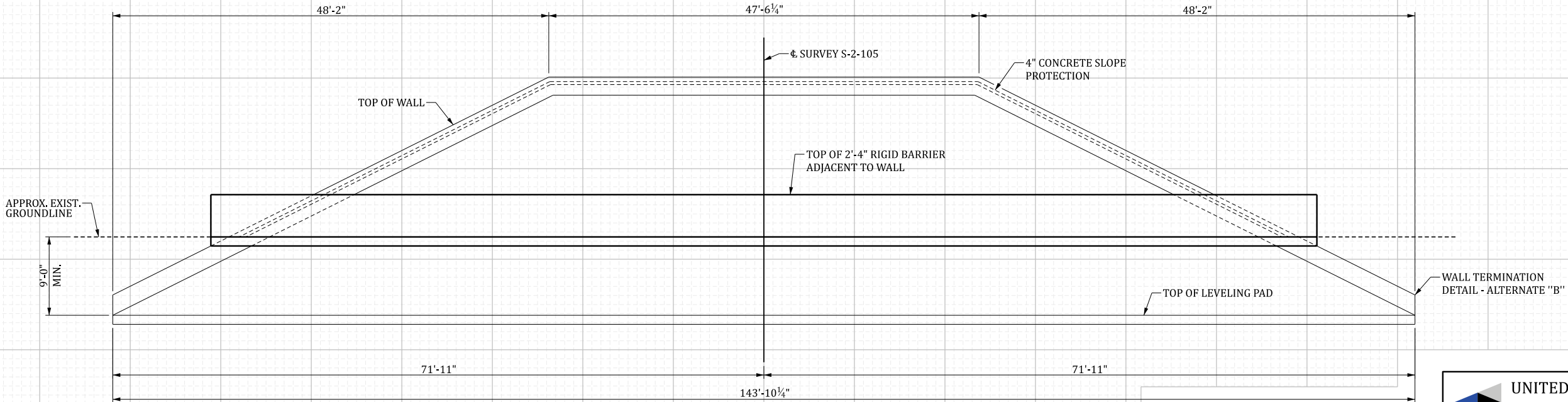
COUNTY: AIKEN

ROUTE: S-2-105





MSE WALL 2  
PLAN AND PROFILE



ELEVATION - MSE WALL 2

TECHNICAL PROPOSAL  
PLANS



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5562 PENDERGRASS BLVD  
GREAT FALLS, SC 29055

SOUTH CAROLINA  
DEPARTMENT OF TRANSPORTATION

MSE WALL 2  
PLAN & PROFILE

S-2-105 (OLD VAUCLUSE ROAD) OVER I-20

COUNTY: AIKEN

ROUTE: S-2-105

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QUAN.	DLK	GET	DATE



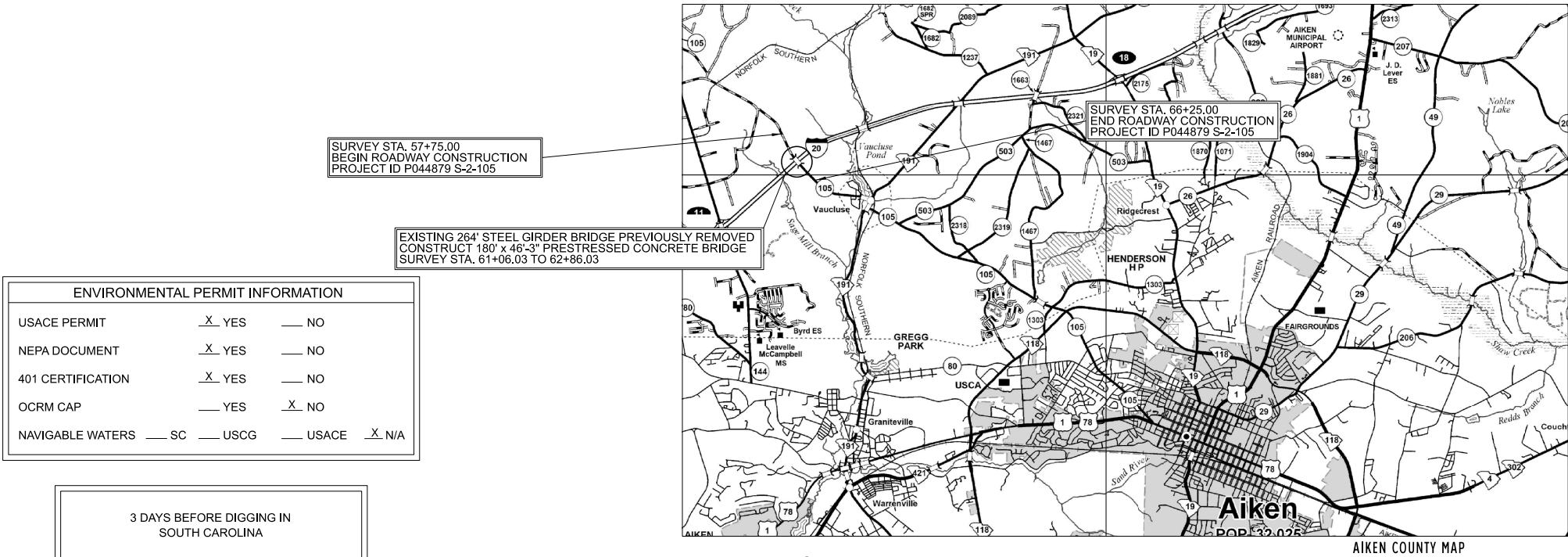
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INDEX OF SHEETS

SHEET NO.	DESCRIPTION OF SHEETS	SHEET TOTAL
1	TITLE SHEET	1
2-4	TYPICAL SECTION SHEETS	3
5	PLAN & PROFILE SHEETS	1
TOTAL NUMBER OF SHEETS = 5		



PROPOSED PLANS  
FOR  
AIKEN COUNTY  
PROJECT ID: P044879  
S-2-105 OVER I-20  
EMERGENCY BRIDGE REPLACEMENT



ENVIRONMENTAL PERMIT INFORMATION			
USACE PERMIT	<input checked="" type="checkbox"/> YES	<input type="checkbox"/> NO	
NEPA DOCUMENT	<input checked="" type="checkbox"/> YES	<input type="checkbox"/> NO	
401 CERTIFICATION	<input checked="" type="checkbox"/> YES	<input type="checkbox"/> NO	
OCRM CAP	<input type="checkbox"/> YES	<input checked="" type="checkbox"/> NO	
NAVIGABLE WATERS	<input type="checkbox"/> SC	<input type="checkbox"/> USCG	<input type="checkbox"/> USACE <input checked="" type="checkbox"/> N/A

3 DAYS BEFORE DIGGING IN  
SOUTH CAROLINA

CALL 811

SOUTH CAROLINA 811 (SC811)  
WWW.SC811.COM  
ALL UTILITIES MAY NOT BE A MEMBER OF SC811

RAILROAD INVOLVEMENT?  
YES ☒ NO

TRAFFIC DATA	
2025 ADT	1,700
2045 ADT	5,100
TRUCKS	3 %

	TOTAL PROJECT
NET LENGTH OF ROADWAY	0.161 MILES
NET LENGTH OF BRIDGES	0.034 MILES
NET LENGTH OF PROJECT	0.195 MILES
LENGTH OF EXCEPTIONS	0.000 MILES
GROSS LENGTH OF PROJECT	0.195 MILES

EQUALITIES IN STATIONING  
NONE

NOTE: EXCEPT AS MAY OTHERWISE BE SPECIFIED ON THE PLANS OR IN THE SPECIAL PROVISIONS, ALL MATERIALS AND WORKMANSHIP ON THIS PROJECT SHALL CONFORM TO THE SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (2025 EDITION) AND THE STANDARD DRAWINGS FOR ROAD CONSTRUCTION IN EFFECT AT THE TIME OF LETTING.

CONTRACTOR FIRM  
(STRUCTURES & UTILITY COORDINATION)

**UNITED**  
INFRASTRUCTURE GROUP

5562 PENDERGRASS BLVD  
GREAT FALLS, SC 29055  
PHONE (803) 381-6000  
WWW.UIG.NET

CONSULTING ENGINEERING FIRM  
(HYDRAULICS, & EROSION CONTROL)

**McCORMICK TAYLOR**

1441 MAIN STREET, SUITE 305  
COLUMBIA, SC 29201  
PHONE (803) 978-2744  
WWW.MCCORMICKTAYLOR.COM

CONSULTING ENGINEERING FIRM  
(ROADWAY, TRAFFIC CONTROL, PAVEMENT MARKING, & SIGNING)

**VIAS**  
INFRASTRUCTURE

220 HORIZON DRIVE, SUITE 117  
RALEIGH, NC 27615  
PHONE (727) 214-7888  
WWW.VIASINFRASTRUCTURE.COM

CONSULTING ENGINEERING FIRM  
(GEOTECHNICAL)

**F&ME**  
CONSULTANTS

211 BUSINESS PARK BOULEVARD  
COLUMBIA, SC 29203  
PHONE (803) 254-4540  
WWW.FMECONSULTANTS.COM

FED. ROAD DIST. NO.	STATE	COUNTY	PROJECT NO.	ROAD/ROUTE NO.	SHEET NO.	TOTAL SHEETS
7	S.C.	AIKEN	P044879	S-2-105	1	5

Design Reference for these plans is the:

2021

SCDOT Roadway Design Manual

Design Reference for these plans is supplemented by the:

2018

AASHTO "A Policy on Geometric Design of Highways and Streets"

Hydraulic Design Reference for these plans is the:

2009

Edition of SCDOT's "Requirements for Hydraulic Design Studies"

NPDES PERMIT INFORMATION

Disturbed Area = \_\_\_\_\_ Acre(s)

Project Area = \_\_\_\_\_ Acre(s)

Approximate Location of Roadway is

Begin

Latitude 33° 37' 12" N

Longitude 81° 49' 23" W

End

Latitude 33° 37' 19" N

Longitude 81° 49' 30" W

Hydraulic and NPDES Design provided by:

MCCORMICK TAYLOR

Designs may be obtained from the SCDOT Regional Production Group

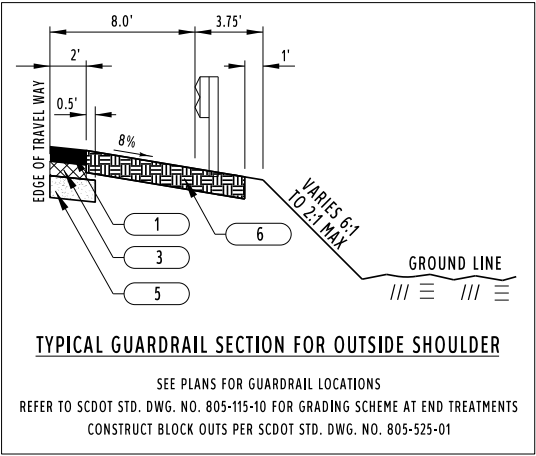
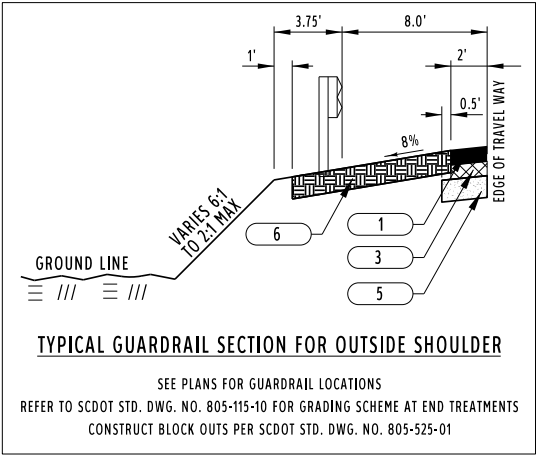
TECHNICAL  
PROPOSAL  
PLANS

— DESIGN NOT CONSIDERED FINAL —  
UNLESS ALL SIGNATURES ARE COMPLETE

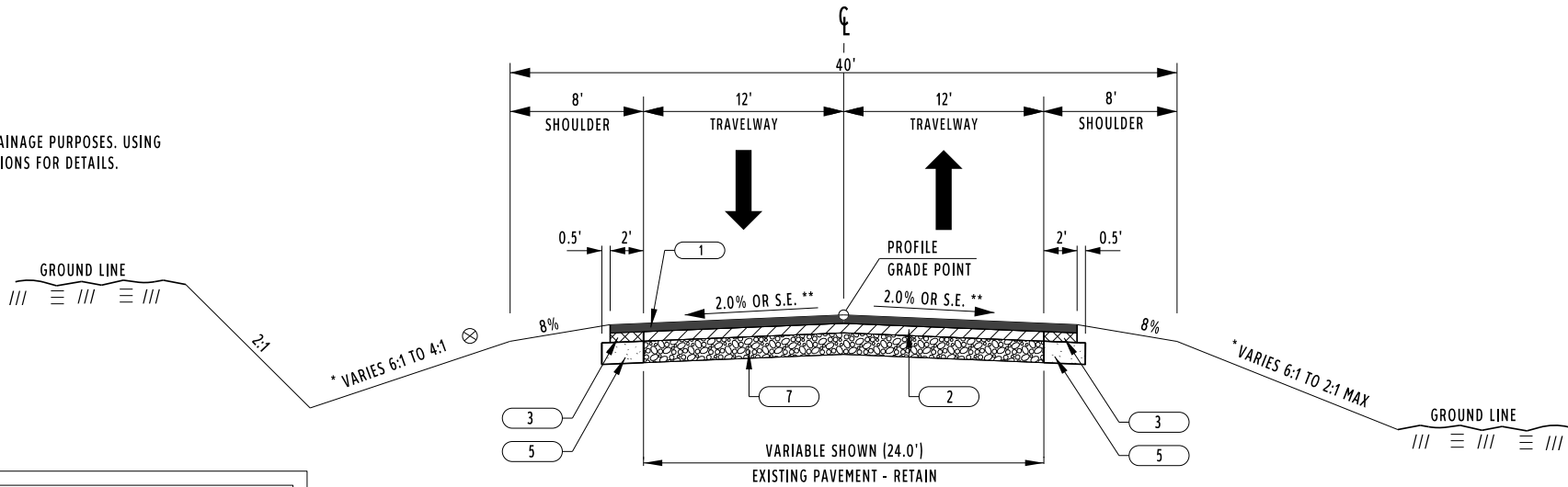
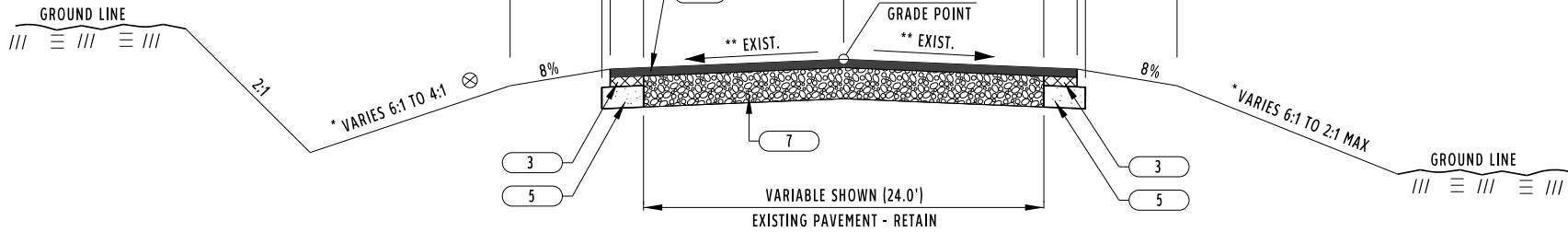


TYPICAL SECTION OF IMPROVEMENT  
SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION  
COLUMBIA, S.C.

FED. ROAD DIST. NO.	STATE	COUNTY	PROJECT ID	RTE. NO.	SHEET NO.
7	S.C.	AIKEN	P044879	S-2-105	2



- GENERAL NOTES:
- SEE PLANS AND CROSS SECTIONS FOR LOCATIONS OF DITCH AND FILL SECTIONS.
- TIE TO EXISTING TRAFFIC LANE AND SHOULDER WIDTHS
- NEW PAVEMENT DEPICTED IN THE TYPICAL SECTIONS TO BE CONSTRUCTED TO OPTION 1 OF THE RFP DESIGN CRITERIA (ASPHALT OPTION). 300 LBS./S.Y. HMA BASE TYPE B & 6" OF GRADED AGGREGATE BASE MAY BE USED IN LIEU OF 600 PSY HMA BASE TYPE B.
- SHOULDER WIDTH VARIES AT GUARDRAIL LOCATIONS, SEE "TYPICAL GUARDRAIL SECTION FOR OUTSIDE SHOULDER". SEE PLANS FOR GUARDRIAL LOCATIONS.
- SECTION NOTES:
- \* CUT/FILL SLOPES VARY. SEE CROSS SECTIONS FOR REQUIRED SLOPE VALUES. PROVIDE SMOOTH TRANSITIONS BETWEEN SLOPE CHANGES
  - 6:1 SLOPE 0'-5' FILL HEIGHT
  - 4:1 SLOPE 5'-10' FILL HEIGHT
  - 2:1 MAX SLOPE >10' FILL HEIGHT AND AT BRIDGE ENDS
  - \*\* SEE CROSS SECTIONS FOR SUPERELEVATION DETAILS.
  - ⊗ DITCH NOTES:  
THIS SLOPE MAY BE VARIED WHEN A DEEPER DITCH IS NECESSARY FOR DRAINAGE PURPOSES, USING A MINIMUM SLOPE OF 6:1 AND A MAXIMUM SLOPE OF 2:1. SEE CROSS SECTIONS FOR DETAILS.  
TYPICAL DITCH WIDTH FOR 6:1 FRONT SLOPE IS 10' TO COVER CLEARZONE.



LEGEND		
1		HOT MIX ASPHALT SURFACE COURSE TYPE C (150 LBS./S.Y.)
2		HOT MIX ASPHALT SURFACE COURSE TYPE E FOR BUILDUP/LEVELING UP TO 1.5" OR HOT MIX ASPHALT INTERMEDIATE COURSE TYPE B FOR BUILDUP/LEVELING ABOVE 1.5"
3		HOT MIX ASPHALT INTERMEDIATE COURSE TYPE C (200 LBS./S.Y.)
4		HOT MIX ASPHALT INTERMEDIATE COURSE TYPE B SPECIAL (1100 LBS./S.Y.) NOT TO BE CONSTRUCTED IN LIFTS GREATER THAN 550 LBS./S.Y.
5		HOT MIX ASPHALT BASE COURSE TYPE B (600 LBS./S.Y.)
6		4" HOT MIX ASPHALT SURFACE COURSE (NON-MOWABLE STRIP)
7		EXISTING PAVEMENT (TO BE RETAINED)

CONTRACTOR FIRM

**UNITED**  
INFRASTRUCTURE GROUP

5562 PENDERGRASS BOULEVARD  
GREAT FALLS, SC 29055  
PHONE 803-581-4000  
WWW.UIG.NET

CONSULTING ENGINEERING FIRM

**IAS**  
INFRASTRUCTURE

220 HORIZON DRIVE, SUITE 117  
RALEIGH, NC 27615  
PHONE 727-214-7698  
WWW.IASINFRASTRUCTURE.COM

— DESIGN NOT CONSIDERED FINAL —  
UNLESS ALL SIGNATURES ARE COMPLETE

TECHNICAL PROPOSAL  
PLANS

RTE.	DESIGN SPEED		PAVEMENT DESIGN	SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION ROAD DESIGN COLUMBIA, S.C.
	MPH	FROM STA.	TO STA.	
	50	57+75.00	66+25.00	S-2-105 OVER I-20 EMERGENCY BRIDGE REPLACEMENT TYPICAL SECTIONS
				SCALE: NTS RTE. S-2-105 DWG. NO. 2



TYPICAL SECTION OF IMPROVEMENT  
SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION  
COLUMBIA, S.C.

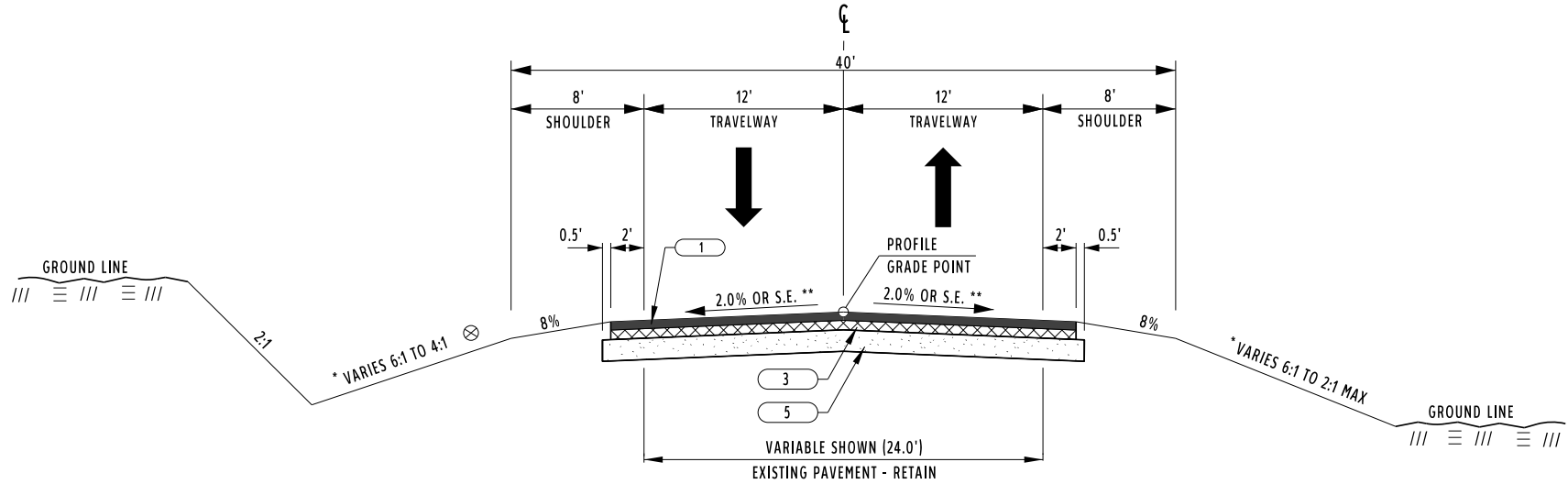


Diagram illustrating a proposed 40' wide road cross-section. The road consists of two 12' travelways and two 8' shoulders. A centerline is marked. A profile grade point is shown with a 2.0% or steeper slope on both sides. A variable shown section (24.0' wide) is indicated for the existing pavement to be retained. Callouts 1, 3, and 5 point to specific features: 1 points to the shoulder, 3 points to the travelway, and 5 points to the existing pavement.

Diagram illustrating a 40' wide road cross-section with a 2.00% sag vertical curve. The road consists of two 12' travelways and two 8' shoulders. The centerline (CL) is marked. A 'PROFILE GRADE POINT' is indicated on the road surface, with arrows showing the 2.00% grades on either side. Large black arrows point down and up from the travelways.

Diagram illustrating the detail of a ditch at the toe of a fill slope. The diagram shows a proposed fill slope with a 2:1 (MAX) ratio, an existing ground line, a ditch elevation, and a 20:1 slope leading to the ditch. A 5' dimension and a 'VAR.' (variable) dimension are indicated for the ditch width.

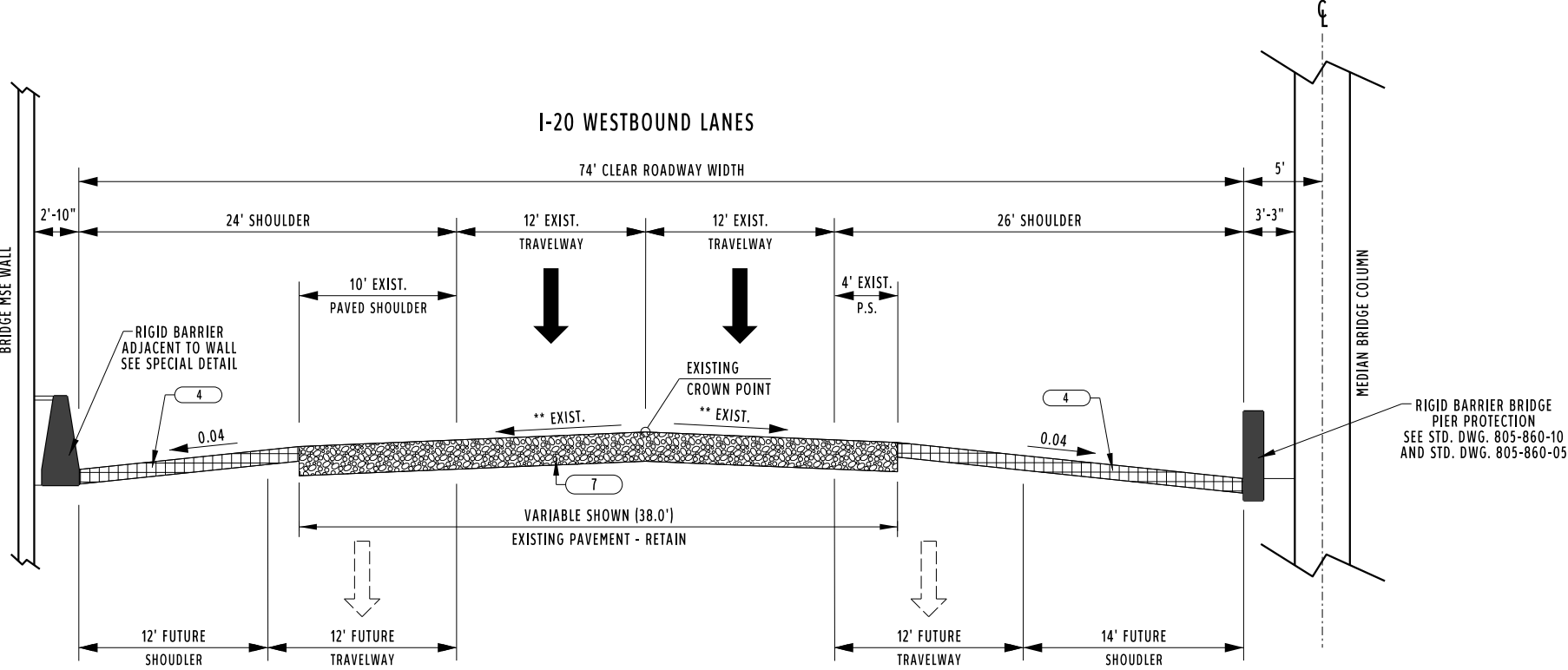
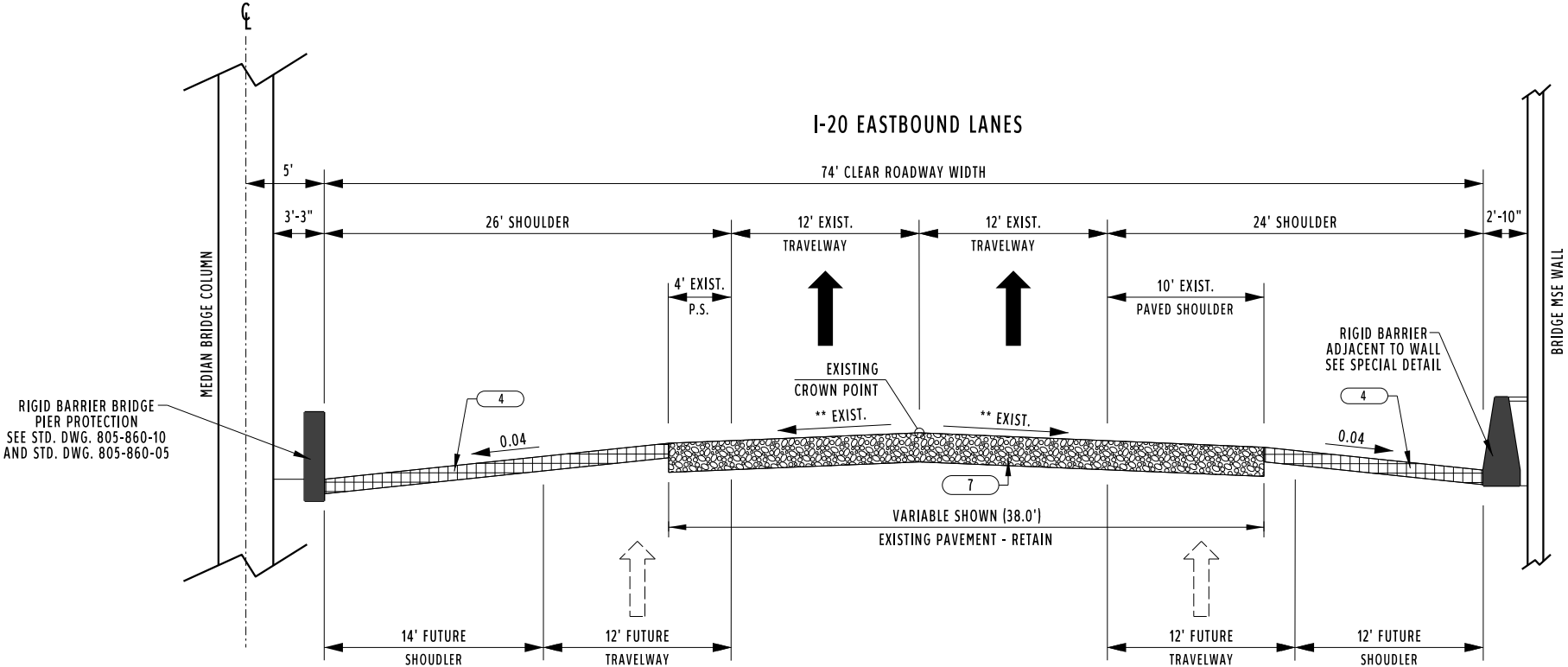
RTE.			DESIGN SPEED		SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION ROAD DESIGN COLUMBIA, S.C.	
MPH	FROM STA.		TO STA.			
50	57+75.00		66+25.00		S-2-105 OVER I-20 EMERGENCY BRIDGE REPLACEMENT TYPICAL SECTIONS	
					SCALE:    NTS                      RTE. S-2-105                      DWG. NO. 3	



TYPICAL SECTION OF IMPROVEMENT  
SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION  
COLUMBIA, S.C.

FED. ROAD DIST. NO.	STATE	COUNTY	PROJECT ID	RTE. NO.	SHEET NO.
7	S.C.	AIKEN	P044879	S-2-105	4

LEGEND		
1		HOT MIX ASPHALT SURFACE COURSE TYPE C (150 LBS./S.Y.)
2		HOT MIX ASPHALT SURFACE COURSE TYPE E FOR BUILDUP/LEVELING UP TO 1.5" OR HOT MIX ASPHALT INTERMEDIATE COURSE TYPE B FOR BUILDUP/LEVELING ABOVE 1.5"
3		HOT MIX ASPHALT INTERMEDIATE COURSE TYPE C (200 LBS./S.Y.)
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5		HOT MIX ASPHALT BASE COURSE TYPE B (600 LBS./S.Y.)
6		4" HOT MIX ASPHALT SURFACE COURSE (NON-MOWABLE STRIP)
7		EXISTING PAVEMENT (TO BE RETAINED)



CONTRACTOR FIRM	CONSULTING ENGINEERING FIRM
<b>UNITED</b> INFRASTRUCTURE GROUP 5562 PENDERGRASS BOULEVARD GREAT FALLS, SC 29055 PHONE 803-581-4000 WWW.UIG.NET	<b>IAS</b> INFRASTRUCTURE 220 HORIZON DRIVE, SUITE 117 RALEIGH, NC 27615 PHONE 727-214-7688 WWW.IASINFRASTRUCTURE.COM

— DESIGN NOT CONSIDERED FINAL —  
UNLESS ALL SIGNATURES ARE COMPLETE

TECHNICAL PROPOSAL  
PLANS

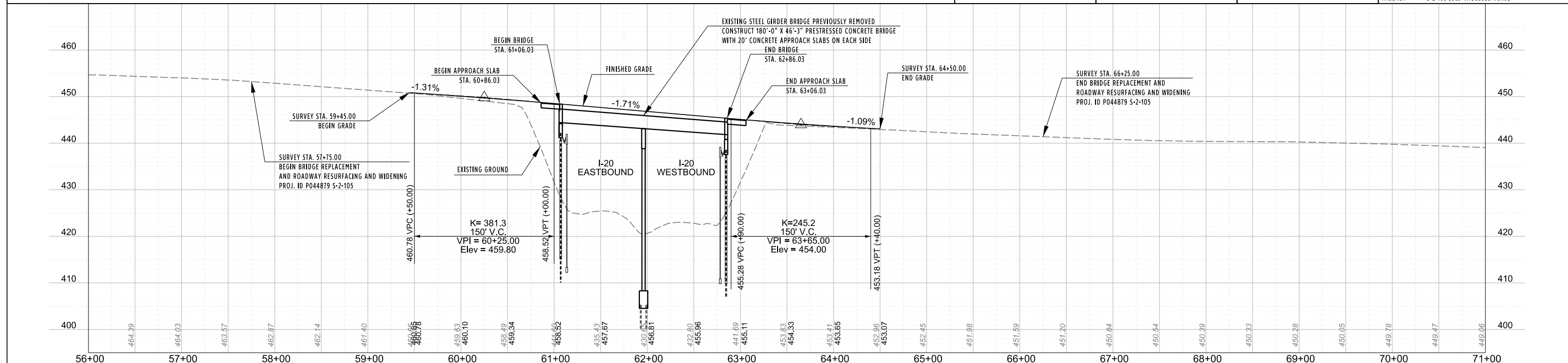
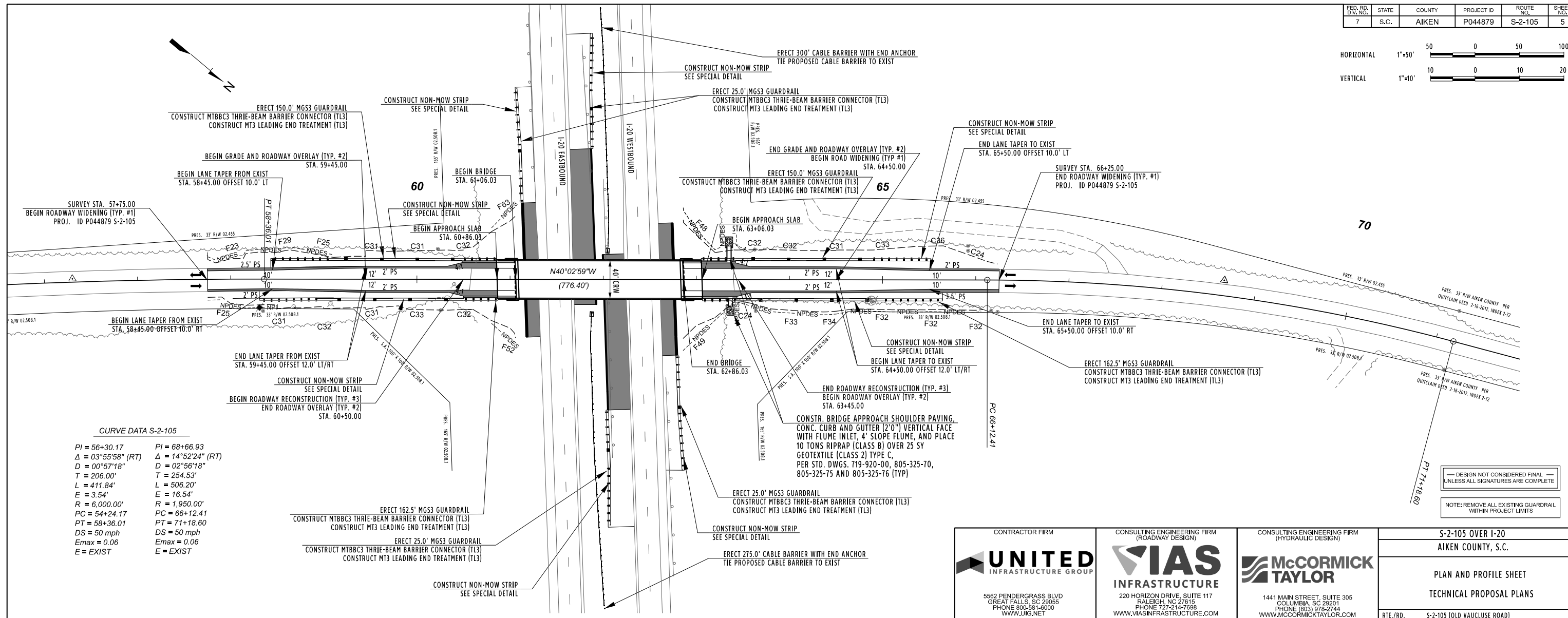
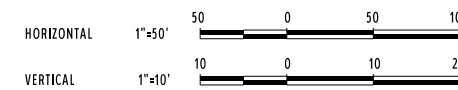
RTE.	DESIGN SPEED	
	MPH	FROM STA. TO STA.
75		

PAVEMENT DESIGN
APPROVED BY
DATE

SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION ROAD DESIGN COLUMBIA, S.C.
S-2-105 OVER I-20 EMERGENCY BRIDGE REPLACEMENT TYPICAL SECTIONS
SCALE: NTS RTE. S-2-105 DWG. NO. 4



FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROUTE NO.	SHEET NO.
7	S.C.	AIKEN	P044879	S-2-105	5







## **APPENDIX B:**

*Required Forms, Confidential and Proprietary Information Page List*



## 12. EQUAL EMPLOYMENT OPPORTUNITY CERTIFICATION

### (COMPLETE THIS SECTION FOR FEDERAL PROJECTS ONLY) EQUAL EMPLOYMENT OPPORTUNITY PERFORMANCE

Select the Certification that applies to the PROPOSER:

Certification ( ☐ ) or Certification ( ☐ )

Select the appropriate responses in the applicable Certification:

Certification (1): Pursuant to 41 C.F.R. §60-1.7(b)(1), Previous Equal Employment Opportunity Performance

Certification, as the Prospective Prime Contractor, I HEREBY CERTIFY THAT I:

(a) **(HAVE)** / **HAVE NOT** developed and filed an Affirmative Action Program pursuant to 41C.F.R. §60-2 and/or 60-4;

(b) **(HAVE)** / **HAVE NOT** participated in a previous contract or subcontract subject to the equal opportunity clause;

(c) **(HAVE)** / **HAVE NOT** filed with the Joint Reporting Committee, the Director of Office of Federal Contract Compliance, or the Equal Employment Opportunity Commission, all reports due under the applicable filing requirements,

OR

Certification (2): I, HEREBY CERTIFY that as the Prospective Prime Contractor submitting this Proposal, **(CLAIM / DO NOT CLAIM)** exemption from the submission of the Standard Form 100 (EEO-1) due to the fact that it employs a total of less than fifty (50) employees under C.F.R. §60-1.7, or qualifies for an exempted status under 41 C.F.R. §60-1.5.

I FURTHER CERTIFY that the above Certification will be made part of any Subcontract Agreement involved with this project.

Executed on 08/05, 20 25 .

Signed: David Michael Grey  
(Officer/PROPOSER)

Title: Executive Vice President & Chief Business Officer

Company: United Infrastructure Group, Inc.

Address: 5562 Pendergrass Blvd, Great Falls, SC 29055

Note: The above certification is required by the Equal Employment Opportunity Regulations of the Secretary of Labor (41 CFR 60-1.7(b)(1)), and must be submitted by PROPOSERS only in connection with contracts which are subject to the equal opportunity clause. Contracts that are exempt from the equal opportunity clause are set forth in 41 CFR 60-1.5. (Generally, only contracts of \$10,000 or under are exempt.)



## 11. NON-COLLUSION CERTIFICATION

### NON-COLLUSION CERTIFICATION

**Project ID: P044879**

IN ACCORDANCE WITH THE PROVISIONS OF S.C. CODE ANN. §§ 39-3-10 ET. SEQ., 39-5-10 ET. SEQ., 15 U.S.C. §45; 23 C.F.R. §635.112(F); AND 28 U.S.C. §1746, I HEREBY ACKNOWLEDGE THAT I AM AN OFFICER OF THE PROPOSER FIRM AND, UNDER PENALTY OF PERJURY UNDER THE LAWS OF THE UNITED STATES AND SOUTH CAROLINA, DECLARE, BY MY CERTIFICATION BELOW, THAT THE FOLLOWING IS TRUE AND CORRECT, AND FURTHER, THAT THIS FIRM, ASSOCIATION OR CORPORATION HAS NOT, EITHER DIRECTLY OR INDIRECTLY, ENTERED INTO ANY AGREEMENT, PARTICIPATED IN ANY COLLUSION, OR OTHERWISE TAKEN ANY ACTION IN RESTRAINT OF FREE COMPETITIVE BIDDING IN CONNECTION WITH THE SUBMISSION OF A BID PROPOSAL ON THE ABOVE REFERENCED PROJECT.

BY CHECKING THIS BOX ☐ , I CERTIFY THAT I HAVE READ, UNDERSTAND, ACCEPT, AND ACKNOWLEDGE ALL OF THE ABOVE STATEMENTS.

Executed on 08/05/2025  
(Date)

Signed: David Michael Grey  
(Officer/Proposer)

EVP & Chief Business Officer  
(Title)

5562 Pendergrass Blvd,  
(Address)

Great Falls, SC 29055





South Carolina  
Department of Transportation

## NOTICE TO PROPOSERS

### **S-2-105 (Old Vaucluse Road) over I-20 Emergency Bridge Replacement Design-Build – Project ID P044879 Aiken County**

July 30, 2025

NOTICE TO PROPOSERS - Enclosed is **Addendum 1** to the Request for Proposals (RFP) for the S-2-105 (Old Vaucluse Road) over I-20 Emergency Bridge Replacement design-build project. The information provided in this notice and the addendum shall be made part of the contract documents.

The **yellow** highlights identify the revisions associated with Addendum 1.

This addendum is being issued in order to provide clarification and additional information for the project. The following sections of the RFP contain revisions:

- Request for Proposals Instructions (Title Page)
- Attachment B – Supplemental Project Design Criteria
  - Revised survey file

Proposers are responsible for reviewing the Addendum and determining if any prior approved ATCs are affected by the revisions, if applicable. The Proposer must submit a request for approval of all additional variances required within five (5) business days of receipt of the Addendum.





**NOTICE OF RECEIPT**  
**S-2-105 (Old Vaucluse Road) over I-20 Emergency Bridge Replacement**  
**Design-Build – Project ID P044879**  
**Aiken County**

**Addendum 1**

The information in this addendum shall be made part of the contract documents. PROPOSERS are instructed to incorporate the information into the previously provided RFP documents.

PROPOSERS are required to sign this document and enclose it with their Technical Proposal. Receipt of this signed document by The South Carolina Department of Transportation serves as confirmation that the PROPOSER has received and incorporated this Addendum into the contract documents.

**Confirmation Statement:**

I, the PROPOSER confirm that I have received this addendum package and have incorporated the information provided in the addendum into the contract documents.

  
PROPOSER's Signature

August 5, 2025  
Date

D. Michael Grey, PE  
Printed Name

For: United Infrastructure Group, Inc.  
Design-Build Team Name







South Carolina  
Department of Transportation

## NOTICE TO PROPOSERS

### **S-2-105 (Old Vaucluse Road) over I-20 Emergency Bridge Replacement Design-Build – Project ID P044879 Aiken County**

July 31, 2025

NOTICE TO PROPOSERS - Enclosed is **Addendum 2** to the Request for Proposals (RFP) for the S-2-105 (Old Vaucluse Road) over I-20 Emergency Bridge Replacement design-build project. The information provided in this notice and the addendum shall be made part of the contract documents.

The **yellow** highlights identify the revisions associated with Addendum 1. The **green** highlights identify the revisions associated with Addendum 2.

This addendum is being issued in order to provide clarification and additional information for the project. The following sections of the RFP contain revisions:

- Request for Proposals Instructions (Title Page)
- Exhibit 4a
- Exhibit 4b
- Attachment B – Supplemental Project Design Criteria
  - Existing ROW survey zip file.

Proposers are responsible for reviewing the Addendum and determining if any prior approved ATCs are affected by the revisions, if applicable. The Proposer must submit a request for approval of all additional variances required within five (5) business days of receipt of the Addendum.





**NOTICE OF RECEIPT**  
**S-2-105 (Old Vaucluse Road) over I-20 Emergency Bridge Replacement**  
**Design-Build – Project ID P044879**  
**Aiken County**

**Addendum 2**

The information in this addendum shall be made part of the contract documents. PROPOSERS are instructed to incorporate the information into the previously provided RFP documents.

PROPOSERS are required to sign this document and enclose it with their Technical Proposal. Receipt of this signed document by The South Carolina Department of Transportation serves as confirmation that the PROPOSER has received and incorporated this Addendum into the contract documents.

**Confirmation Statement:**

I, the PROPOSER confirm that I have received this addendum package and have incorporated the information provided in the addendum into the contract documents.

  
PROPOSER's Signature

August 5, 2025

Date

D. Michael Grey, PE  
Printed Name

For: United Infrastructure Group, Inc.  
Design-Build Team Name





Company



### 13. STIPEND ACKNOWLEDGEMENT FORM

#### Stipend Acknowledgement Form

#### S-2-105 (Old Vacluse Road) over Interstate 20 Emergency Bridge Replacement Aiken County

Proposer: United Infrastructure Group, Inc.

ADDRESS:

5562 Pendergrass Blvd, Great Falls, SC 29055

The undersigned Proposer, hereby:

☐

Waives the stipend for this Project.

☐

Accepts the stipend for this Project.

By accepting the stipend for this Project, Proposer agrees:

1) to execute and include the Stipend Agreement in Article XIII of the RFP with its RFP response;

2) to submit an invoice with FEIN number for the stipend amount to the SCDOT POC after SCDOT's posting of the Notice of Award on SCDOT's Design-Build Website.;

3) to transfer all rights to its Work Product used to develop the Proposal as of the date of this acknowledgement. "Work Product" means all submittals, ideas, innovations, solutions, methods, processes, design concepts, materials, electronic files, marked up drawings, cross sections, quantity lists and intellectual property, made by Proposer during the RFP process, including the Proposal, exchange of information during the pre-Proposal and post-Proposal period.

SCDOT will pay the stipend to each eligible unsuccessful Proposer, who has signed a Stipend Agreement, within ninety (90) days after execution of the Contract or the decision to not award a contract.

08/05/2025

Date

David Michael Grey  
Proposer

D. Michael Grey, PE

Print Name



## 14. STIPEND AGREEMENT

### STIPEND AGREEMENT

Project ID: P044879

#### **S-2-105 (Old Vacluse Road) over Interstate 20 Emergency Bridge Replacement Aiken County**

**THIS STIPEND AGREEMENT** (the "Agreement") is made and entered into as of the 5<sup>th</sup> day of August, 2025 by and between the SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION (hereinafter "SCDOT"), and United Infrastructure Group, Inc. ("Proposer"), with reference to the following facts:

SCDOT issued a Request for Proposal ("RFP") for design and construction of the above-referenced Design-Build Project ("Project"), pursuant to procurement authority granted in Section 57-5-1625 of the S.C. Code of Laws, 1976, as amended. The RFP provided for payment of stipends as provided herein. Capitalized terms used, but not defined, have the meanings ascribed in the RFP.

**NOW, THEREFORE**, Proposer hereby agrees as follows:

#### **1. Work Product.**

**1.1** Proposer shall prepare and submit a responsible and responsive Technical Proposal and Cost Proposal that conforms in all material respects to the requirements and provisions of the RFP, as determined by SCDOT, and are timely received by SCDOT in accordance with the RFP Milestone Schedule.

**1.2** By signing this Stipend Agreement, Proposer agrees to transfer full and complete ownership to SCDOT of all Work Product. The Work Product (as defined below) shall become the property of SCDOT without restriction or limitation on its use, without further compensation or consideration, and can be used in connection with this Project or any future projects by SCDOT. Neither Proposer nor any of its team members shall copyright any of the material developed under this Agreement.

**1.3** The term "Work Product" shall mean the Proposal and all material, electronic files, marked up drawings, cross sections, quantity lists, submittals, ideas, innovations, solutions, methods, processes, design concepts, Trade Secrets or confidential information, and intellectual property, made by or produced for Proposer in the development and submission of the Technical and Cost Proposal, including exchanges of information during the pre-Proposal and post-Proposal period.

#### **2. Compensation and Payment.**

**2.1** A stipend to Proposer for the Work Product described herein shall be \$32,500.00 and is payable to Proposer that was determined to be responsible and (1) submitted a responsive Technical Proposal and responsive Cost Proposal to the RFP which is not selected for award of this Project, or (2) was awarded the Contract but the Contract was terminated by SCDOT for convenience after the Submittal of Proposal Due Date (See Final RFP Milestone schedule) but prior to the Notice to Proceed #1. Responsibility of Proposers and responsiveness of the Technical Proposal and Cost Proposal will be determined by SCDOT as a condition of payment.

**2.2** SCDOT will pay the stipend to Proposer as follows, subject (as applicable) to the following conditions:

(a) Proposer has submitted this signed Stipend Agreement, unchanged with its response to the RFP.

(b) After posting of the Notice of Award on SCDOT's Design-Build Website, Proposer has submitted to SCDOT an invoice, with FEIN Number, for the Stipend amount.

(c) After execution of the Contract or the decision not to award a contract, SCDOT will pay the invoice for the stipend amount to the unsuccessful Proposer meeting the criteria of Section 2.1 within 90 calendar days of receipt of the invoice from Proposer.



(d) If the procurement is suspended or cancelled prior to the Proposal Due Date (see FINAL RFP Milestone schedule), no stipend will be paid to Proposer.

(e) After the submittal of Proposals, but prior to award, if the procurement is cancelled, all Proposers that provide a responsive Technical Proposal and Cost Proposal to the final RFP and submitted a signed Stipend Agreement with their RFP shall receive the stipend

(f) In the event of a Best and Final Offer, only one stipend will be paid to each Proposer that executed a Stipend Agreement and met the other criteria and conditions herein.

(g) No stipends will be paid to a Proposer who withdraws at any time from this procurement.

**2.3** Acceptance by the Proposer of payment of the stipend amount from SCDOT shall constitute a waiver by Proposer of any and all right, equitable or otherwise, to bring any claim in connection with this procurement, procurement process, award of the Contract, or cancellation of this procurement.

**2.4** The Proposer awarded the contract shall be not eligible to receive a stipend.

**2.5** If Proposer elects to waive payment of the stipend, SCDOT will not use the ideas or information contained in that Proposer's Proposal for this Project. However, the Proposer's Proposal will be subject to the South Carolina Freedom of Information Act.

### **3. Indemnities.**

**3.1** Subject to the limitations contained in Section 3.2, Proposer shall indemnify, protect and hold harmless SCDOT and its directors, officers, employees and contractors from, and Proposer shall defend at its own expense, all claims, costs, expenses, liabilities, demands, or suits at law or equity arising, in whole or in part, from the negligence or willful misconduct of Proposer or any of its agents, officers, employees, representatives or subcontractors or breach of any of Proposer's obligations under this Agreement.

**3.2** This indemnity shall not apply with respect to any claims, demands or suits arising from use of the Work Product by SCDOT.

### **4. Compliance With Laws.**

**4.1** Proposer shall comply with all federal, state, and local laws, ordinances, rules, and regulations applicable to the work performed or paid for under this Agreement and covenants and agrees that it and its employees shall be bound by the standards of conduct provided in applicable laws, ordinances, rules, and regulations as they relate to work performed under this Agreement. Proposer agrees to incorporate the provisions of this paragraph in any subcontract into which it might enter with reference to the work performed pursuant to this Agreement.

**4.2** The Proposer agrees (a) not to discriminate in any manner against an employee or applicant for employment because of race, color, religion, creed, age, sex, marital status, national origin, ancestry or disability of a qualified individual with a disability; (b) to include a provision similar to that contained in subsection (a) in any subcontract; and (c) to post and to cause subcontractors to post in conspicuous places available to employees and applicants for employment, notices setting forth the substance of this clause.

### **5. Assignment.**

Proposer shall not assign this Agreement without SCDOT's prior written consent. Any assignment of this Agreement without such consent shall be null and void.

### **6. Miscellaneous.**

**6.1** Proposer and SCDOT agree that Proposer, its team members, and their respective employees are not agents of SCDOT as a result of this Agreement.

**6.2** This Agreement, together with the RFP, as amended from time to time, the provisions of which are incorporated herein by reference, embodies the entire agreement of the parties. There are no promises, terms, conditions, or obligations other than those contained herein or in the RFP, and this Agreement shall supersede all previous communications, representation, or agreements, either oral or written, between the parties hereto.



**6.3** It is understood and agreed by the parties hereto that if any part, term, or provision of this Agreement is by the courts held to be illegal or in conflict with any law of the State of South Carolina, the validity of the remaining portions or provisions shall not be affected, and the rights and obligations of the parties shall be construed and enforced as if the Agreement did not contain the particular part, term, or provisions to be invalid.

**6.4** This Agreement shall be governed by and construed in accordance with the laws of the State of South Carolina.

**IN WITNESS WHEREOF**, the parties have executed this Agreement as of the date first written above.

Witness:

\_\_\_\_\_

Recommended:

\_\_\_\_\_

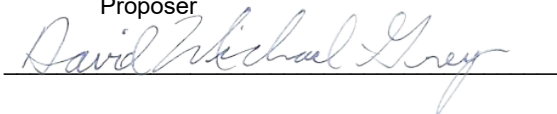
Michael Pitts

SOUTH CAROLINA DEPARTMENT  
OF TRANSPORTATION

By: \_\_\_\_\_

Jae Mattox  
Preconstruction Alternative Delivery Engineer

Proposer

  
\_\_\_\_\_