SCDOT

Bridge Package 27

DESIGN-BUILD PROJECT

Technical Proposal

Dillon and Marlboro Counties, South Carolina

June 16, 2025 | Contract ID 5570770

CROWDER

INFRASTRUCTURE CONSULTING & ENGINEERING

NAVIGATION

FOR EASE OF REFERENCE

Blue Bold Underlined Text indicates links to various items in the Appendix.

FOR ADOBE ACROBAT 25.1 VERSION:

To return to previous view, click ALT + left arrow. You can also set your PDF viewing preferences by following these steps:

1. Select 1:1 Icon in Bottom Right Page View Toolbar
2. Pin "Previous View"

A back button will appear on your toolbar and can be used to go directly to your previous view.

FOR OLDER ADOBE ACROBAT VERSIONS:

To return to previous view, click ALT + left arrow. You can also set your PDF viewing preferences by following these steps:

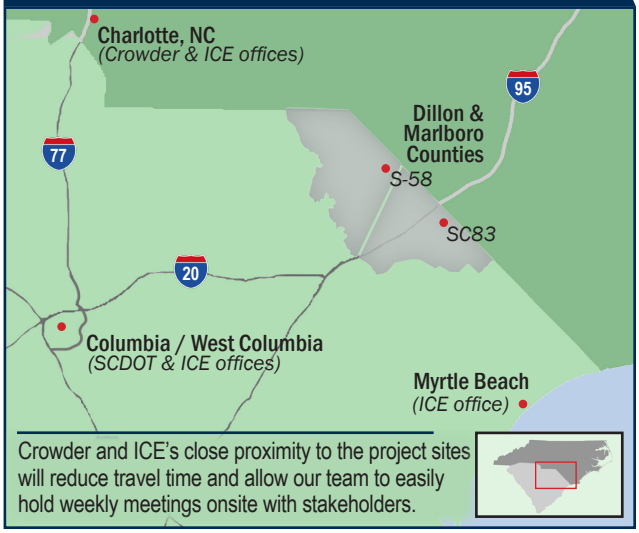
1. Select View Tab
2. Show/Hide Toolbar Items
3. Show Page Navigation Tools
4. Check "Previous View"

A back button will appear on your toolbar and can be used to go directly to your previous view.

Project Delivery and Approach

Our design-build team for SCDOT Bridge Package 27 consists of Crowder Construction Company as the Prime Contractor and Infrastructure Consulting & Engineering, PLLC (ICE) as the Design Engineer. RND Consultants will provide Right of Way Acquisition support and Construction Support Services (CSS) will provide Survey/SUE services for the team.

Project and Office Locations



Crowder and ICE's close proximity to the project sites will reduce travel time and allow our team to easily hold weekly meetings onsite with stakeholders.

Our design approach focuses on balancing schedule and cost to facilitate early utility relocations, material procurement, and construction activities together with producing design and plans. Our approach provides low-maintenance replacement bridges with prestressed girder/flat slab superstructures and pile bents for SC-83 and cored slab superstructures for S-58 in full compliance with RFP requirements.

Crowder will manage the project from our Charlotte, NC, office. Additionally, Crowder will mobilize a mobile project office space near the job sites to serve as the Construction Manager's office. ICE will perform all elements of design from its West Columbia, SC, office. Based on the proximity to SCDOT HQ, this will facilitate close coordination and responses of all design submittals to SCDOT. An ICE design management office in Charlotte also allows for convenient access to Crowder's base of operations for both office meetings and on-site meetings.

ASSURANCE & ABILITY TO COMPLETE PROJECT WITHIN REQUIRED TIMEFRAME

Crowder has the financial, equipment, personnel, and technological resources available to meet and exceed the needs of this project. Crowder's backlog is currently $824.6 million with a total bonding capacity of $1.5 Billion. Crowder Heavy Civil maintains 14 crews performing structures, roadway (grading/drainage) and associated equipment. Crowder will self-perform approximately 70% of the total contract to maintain schedule control. For this project, Crowder will commit a minimum of two (2) structures crews and one (1) grading crew. In addition, Crowder maintains an in-house

SC-83 over the Little Pee Dee River

fleet of cranes, excavators, dozers, and other small equipment at our local equipment yard in Charlotte, NC. When these pieces of equipment are not available, we maintain strong relationships with key small and large equipment suppliers in all regions of our work area. Equipment sourcing is not a problem for our work crews. Furthermore, Crowder will allocate additional resources as necessary to ensure any unforeseen schedule impacts are recovered so that the project is completed on time, or ahead of schedule, to meet SCDOT and public expectations. In addition to Crowder's abundant construction resources, ICE's experienced staff have the capacity to meet the design schedule demands of this project. ICE has subconsulted with RND Consultants and Construction Support Services (CSS) to bring additional resources to the Bridge Package 27 project.

ICE will first submit the Design Review Submittal Schedule and the Design Quality Control (QC) Plan for approval by SCDOT to pave the way for the official submittal process to begin. ICE will then expedite the geotechnical investigation work for the SC-83 bridge so all of the foundation design can be completed and checked by our quality control team before submittal to the Department. All submittals will follow a logical order in correlation to the sequence of work with the final bridge and road plans for SC-83 submitted first followed by final bridge and road plans for S-58.

The Crowder+ICE Team is currently completing work on the US 76 Bridge in Oconee County for SCDOT Bridge Package 29, with the opening of the last bridge (US-76 over Chauga River) scheduled for June 16, 2025. Lessons learned on this project will be applied to construction on Package 27.

Submittals related to both design and construction will get an internal QC review before submission to SCDOT to reduce the likelihood of errors, minimize comments, and eliminate extended reviews that may create schedule uncertainty. The Crowder+ICE SOQ-defined and committed Project Manager, Lead Design Engineer, and Construction Manager have been integrally involved with developing the best delivery approach to the project during the RFP process. The Crowder/ICE Team's project development and construction staff have positive and recent work experience through the SCDOT Bridge Package 29 Design-Build project, which will be completed as scheduled in June 2025.

Quality, safety, value, and constructability have consistently been at the forefront of our design decisions. Upon award, these same managers, engineers, and coordinators will finalize constructible designs and execute these designs on site, allowing for an expedient project start-up without introducing new management to the project. ICE's design team that developed the RFP design will also transition into the final design upon award of the contract to maintain continuity and quality in submittals to SCDOT.

Construction Approach and Schedule

During construction on SC-83, after bridge demolition, Crowder will utilize a trestle on the existing west embankment to access the interior bents and build the center span. This will allow our team to have a crane set on each side of the bridge. We will also drive piles as well as excavate dirt around the substructures and work our way back to the new embankment. We will then drive piles on the east bank and excavate as we back our crane out to achieve the required channel width. Crowder has had success with this construction sequence on several previous projects.

Due to the proximity of the existing bank to the newly located center bents, S-58 will not require a trestle to set the 50" cored slab. Crowder will place cranes on both sides of the bridge and set the center slab, then end bents while backing out.

The CROWDER+ICE Team understands that starting construction is dependent on design submittals, utilities, and permitting. Early construction activities are planned to provide the groundwork for bridge and roadway construction. Crowder will mobilize adequate construction equipment and dedicate an appropriate number of field personnel. All work items can either be self-performed, or performed by a subcontractor with whom Crowder has an established relationship with, which will ensure that the work progresses smoothly and expeditiously.

Crowder plans to work a minimum of five 10-hour days per week. Crowder will develop a cost loaded project schedule which will include all construction activities and materials delivery and establish the critical risk components to stay abreast and manage the schedule, effectively eliminating the

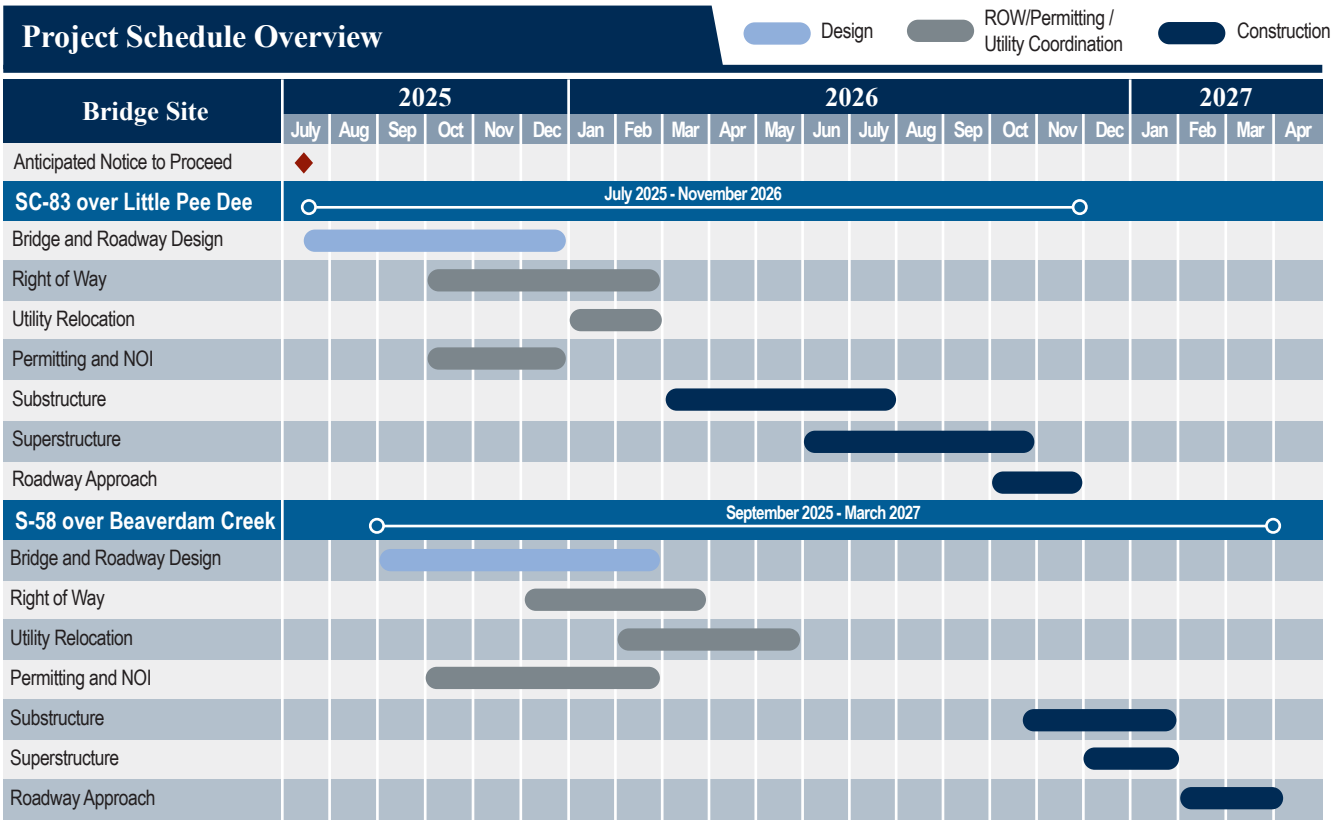
On US 301 over Four Hole Swamp, Crowder utilized a trestle to access interior piles. Our team will use the same method for Package 27's SC-83 bridge construction.

Crowder constructed two types of superstructures for the SC-4 over South Edisto River 610' 3-span bridge. The 250'+ approach spans utilized a 40' long span flat slabs system, and for over the main 90' channel, the team used a more traditional approach with type III precast girders and an 8" cast in place deck.

Crowder replaced a 240' long bridge with a 270' long structure in Moorseville, NC. This three-span structure was founded on four each 36" diameter drilled shaft at the two interior bents and H Pile end bents. The superstructure consisted of 39" Box Beams, 540 LF of cast in place barrier rail, and an asphalt overlay.

risk of schedule slippage. The master schedule will include subcontractor schedules and Crowder will communicate often with subcontractors and suppliers to maintain an expedited completion of this project.

The **CPM schedule** has been developed to account for permit packages submitted on an accelerated schedule to get the Package 27 projects operational as quickly and safely as possible. The CROWDER+ICE Team understands the requirements of the RFP and has planned for, and committed staff and equipment to complete the project within the required timeline.



The interim completion date for SC-83 is January 8, 2027 | S-58 shall be substantially complete by April 6, 2027

Approach for SC-83 Utility Coordination

In our discussions with the utilities, it was determined that MPD Electric Cooperative's (MPD) three-phase pole line will conflict with the proposed bridge construction. This pole line will need to be the first utility relocated to allow room for the crane radius for demo, pile driving, and flat slab form installation. The relocation will start at the pole at the west end of the project limits and then shift back to just inside the present 55FT right-of-way line and continue east for three poles. It is expected to take about a month for this power relocation.

Once this relocation is completed, Sandhill telephone will follow the new pole alignment, since their fiber cable is already attached, expected construction time is two weeks. Following Sandhill's relocation, Charter is planning on eliminating the buried Fiber cable on the south side of SC-83 and going overhead by attaching to the relocated MPD pole line, through the project limits. This relocation is expected to take two to three weeks.

Finally, AT&T has copper lines on the north side of the bridge that will be abandoned, and no relocation will be needed. One additional area of concern is the pipe drainage crossing Bluff Cemetery Road, both Sandhill and Charter have fiber cables that will cross this drainage pipe and may be in conflict. Possible test holes or depth data will be needed to determine if these lines will need relocation or if they can remain in place. All relocations for this project will be at no cost to SCDOT.

Approach for S-58 Utility Coordination

SC-83 over the Little Pee Dee River

In our discussions with the utilities, it was determined that Duke Energy's single-phase poles may conflict with the proposed construction in two areas. The first one is on the west end where the overhead line crosses S-58 and the other on the east end where the new proposed driveway may impact the minimum height of the overhead line and crane radius proximity. Duke Energy stated that it would take three months to complete the relocation from engineering to actual construction and they are claiming prior rights on this project. AT&T has a copper cable that runs on the south side of SC-58 and is attached to the bridge. This line will be temporarily relocated on Duke Energy's overhead pole line through the project limits; it will need to be relocated prior to demolition of the bridge. All of AT&T's relocations will be completed at no-cost to SCDOT.

Tri County water has an 8" water main that runs along the north side of S-58. It is currently not active across the bridge. There is already a relocation plan in place using horizontal directional drilling to install a new water main to replace the current line attached to the bridge. This work will be performed in-contract under ACT36 and will be covered 100 percent by SCDOT, as they are a small utility. This relocation can be completed after the bridge is reconstructed. Charter has an underground fiber optic cable that crosses SC-58 on the west end of the project limit, and then heads east on the north side of SC-58. At this time, it appears to not conflict with the proposed construction, but a couple of drainage issues may cause handhole adjustments. Additionally, the water relocation may be close to impacting the underground fiber.

MINIMIZING NEED FOR NEW RIGHT-OF-WAY

The Crowder+ICE Team's approach to design and right of way has been tailored to each site. Our team performed field investigations to collect the following data:

- Verify the survey provided in the RFP was accurate
- Presence of debris accumulation
- Condition of channel
- Visual inspection of how channel/bridge is performing
- Condition of roadway drainage structures and ditches
- Measured depth of channel/structure and freeboard
- Looked for ways to improve the hydraulic opening
- Delineate WOTUS

SC-83 over the Little Pee Dee River

The NEPA studies have assumed a minor amount of new right of way will be required and it will not have negative effects to resources or landowners and will be located within the project study area. During our weekly pursuit meetings, we discussed impacts such as damage to remainders and ingress/egress requirements.

Any potential issues were addressed and eliminated with our design. The scope of work requires that the Contractor shall, "Provide sufficient right of way cover all permanent facilities." Our team will submit Right of Way and Final plans for the Department to review that fully comply with all RFP requirements and NEPA assumptions.

Our team will implement the following strategies to minimize the purchase new right of way and impacts to WOTUS:

At SC-83 over Little Pee Dee River:

- A trestle will be used for bridge construction access to avoid temporary impacts to WOTUS.
- Our design utilizes a FIB 45 for the span over the Little Pee Dee River. This allows us to reduce the roadway profile on either side of the bridge by approximately 8 inches.
- We optimized the layout of the new right of way saving over 8,000 SF while remaining fully compliant with all RFP criteria.

SC-83	NEW RIGHT OF WAY	WETLAND IMPACTS
RFP Conceptual Design	50,250 SF	0.340 Acres
Technical Proposal Design	42,000 SF	0.421 Acres
Difference	-8,250 SF	+0.081 Acres*

* The increase in wetland impacts is due to removing the skew from the bridge end bents.

At S-58 Over Beaverdam Creek:

- By moving the location of the new driveway in the northeast quadrant closer to the bridge we reduced the amount of new right of way and impacts to wetlands.

S-58	NEW RIGHT OF WAY	WETLAND IMPACTS
RFP Conceptual Design	8,316 SF	0.091 Acres
Technical Proposal Design	6,216 SF	0.073 Acres
Difference	-2,100 SF	-0.018 Acres

PROPOSED DESIGN SUBMITTAL PROCESS

The Crowder/ICE Team has developed a CPM schedule that summarizes the preparation and submittal of all required design deliverables, SCDOT reviews, ROW acquisitions, and permit submittals. Shortly after award, the Design Team will submit this schedule with a Gantt chart along with the Design QC Plan as Submittal 000.

To ensure efficient and complete reviews we will follow the steps listed in the table below.

S-58 over Beaverdam Creek

Key Features of Our Design Submittal Process

✓	Independent QA/QC for all design submittals prior to submission to SCDOT to ensure RFP compliance and minimize SCDOT review comments.
✓	Comprehensive constructability review by the Contractor to improve design and minimize RFIs during construction.
✓	All design submittals will be uploaded to SCDOT's ProjectWise system in PDF format. Plan submissions will be sized at 11"x17" so that they may be printed on a standard printer or any plotter at a specific scale.
✓	The design submission schedule will be posted to SCDOT's ProjectWise site and updated monthly.
✓	Facilitate monthly design meeting/conference call to allow for status updates of the design, to provide a forum for asking questions related to the design during review, and to use as an opportunity for clarification/discussion of comments.

SC-83 over the Little Pee Dee River

Only one new submittal package will be uploaded to SCDOT ProjectWise within five business days. SCDOT develops a Bluebeam Studio session for the review of the submittal and the Department will have 15 business days to complete the review. SCDOT review comments/access to the Bluebeam Session will be provided to Crowder, who will respond to comments in the form of Bluebeam reply within five business days. No additional submittals will be sent during this response time. Crowder and ICE will respond to all comments within five business days of receipt. Our team has developed the CPM schedule to submit revised verification plans at the end of the five day comment response period. In the event that a comment requires a significant design revision that cannot be addressed in the given day response period, SCDOT will be notified in writing within two days of receipt of comment and details of the verification set will be coordinated with the Department's Project Manager.

Once all comments have been verified and closed, the design team will advance to the next package submittal until RFC plans are approved, sealed, and provided to the contractor for construction.

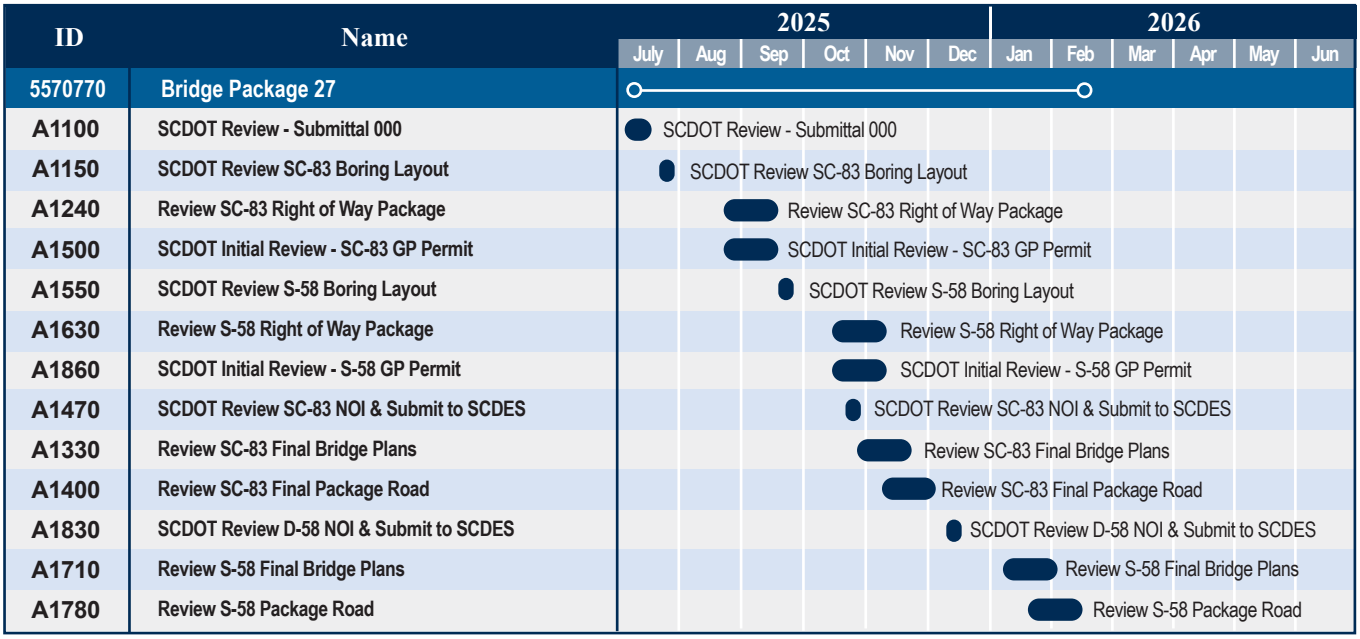
To clearly define project tasks, dependencies and deliverables, our design schedule uses the following Work Breakdown Structure (WBS):

5570770.1	Milestones	5570770.2.2.10	WOTUS Permit (SC-83)
5570770.1.1.1	Design Review Submittal Package 000	5570770.3	S-58 Over Beaverdam Creek
5570770.2	SC-83 Over Little Pee Dee River	5570770.3.3.1	Geotechnical Investigation
5570770.2.2.1	Geotechnical Investigation	5570770.3.3.2	Right of Way Package
5570770.2.2.2	Right of Way Package	5570770.3.3.3	Review Right of Way Package
5570770.2.2.3	Review Right of Way Package	5570770.3.3.4	Final Design Bridge Package
5570770.2.2.4	Final Design Bridge Package	5570770.3.3.5	Review Final Design Bridge Package
5570770.2.2.5	Review Final Design Bridge Package	5570770.3.3.6	Final Package Road
5570770.2.2.6	Final Package Road	5570770.3.3.7	Review Final Package Road
5570770.2.2.7	Review Final Package Road	5570770.3.3.8	ROW Acquisitions
5570770.2.2.8	ROW Acquisitions	5570770.3.3.9	SCDES NOI
5570770.2.2.9	SCDES NOI	5570770.3.3.10	WOTUS Permit (S-58)

Our CPM schedule (**Appendix A.3**) lists the submittal package contents by package type. We have elected to eliminate Preliminary Roadway and Bridge package submittals at our own risk and begin with Right of Way and Final Bridge packages for each site. For our first roadway submittal packages, we have included any appropriate information required with a preliminary submittal with the Right of Way packages as allowed by RFP Exhibit 4z.

The flow of the design reviews is shown in the Gantt chart below.

Gantt Chart

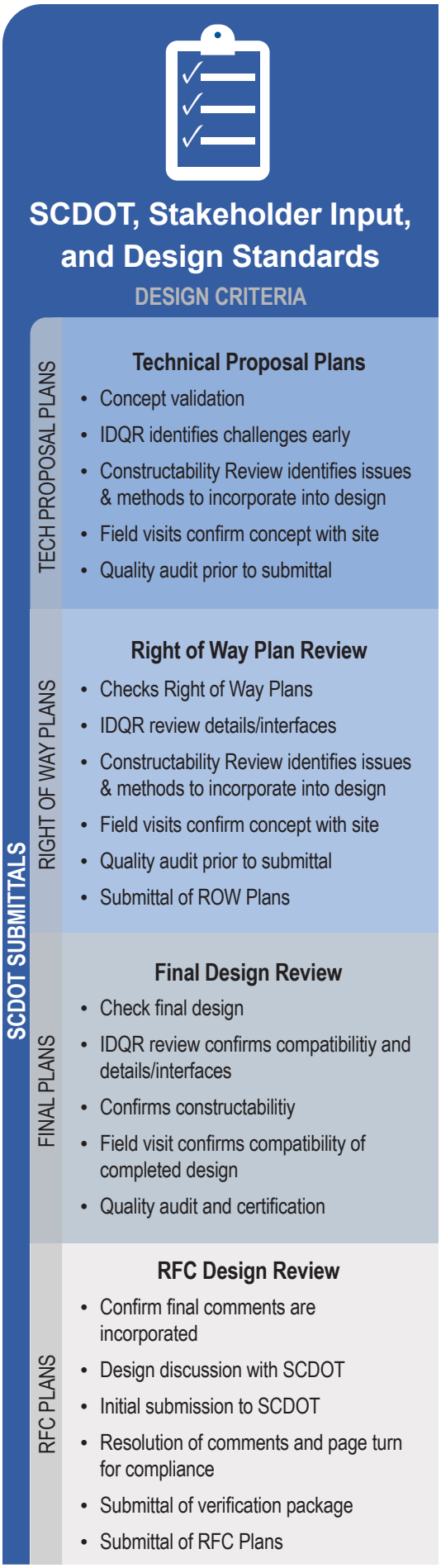


Quality Control

The Crowder+ICE Team has a comprehensive understanding of the quality needs for this project. Our Quality Control (QC) programs will provide an integrated approach that meets the required standards and specifications for the Bridge Package 27 Project. We understand that quality is not inserted into the project at a given point; it starts at the beginning of the design phase and continues through construction.

The basis of the design QC plan includes check prints stamped to confirm that each sheet was checked, reviewed, back checked, and approved in compliance with the RFP. ICE's Design Quality Management Plan (DQMP) is a two-tier review process. The first-tier reviews are internal peer reviews with other design team members. The second-tier reviews will be conducted by Independent Design Quality Review (IDQR) Team not involved with production. These reviews will include checking plans, spreadsheets, calculations, and other software methods or outputs used in the development of the design and construction plans. Our Design Quality Process incorporates a Quality Audit prior to every submittal. These QA reviews coupled with a comprehensive constructability review performed by the DBPM, CM, and DB Coordinator, will be completed prior to submitting to SCDOT. Comments will be assimilated/addressed, and plans will be revised/resubmitted expeditiously until approval of RFC plans with the expectation that our comprehensive internal quality review process will result in minimal resubmissions and RFIs.

The Crowder+ICE team fully understands the responsibilities outlined in the RFP of the Bridge Package 27 Project regarding the Quality Control (QC) Program. With the role of Quality Acceptance being performed by the SCDOT Resident Construction Engineer (RCE), we understand that close coordination with the SCDOT Construction Manager will be necessary to incorporate the results



of independent testing into the quality program. For the Bridge Package 27 Project, the SCDOT Construction Manager will oversee acceptance testing, independent assurance testing, and materials certification. The Crowder+ICE Team Quality Control Manager will establish clear lines of communication in the Quality Control Plan. Sampling and testing personnel shall be SCDOT-certified and/or have the appropriate specialized certification prior to performing any work. Crowder+ICE will perform QC testing, as needed, to ensure that all workmanship and materials are compliant with the RFP contract requirements.

The Quality Control Manager's responsibilities will include both administrative and technical procedures to ensure that the work is inspected and tested to verify compliance with the released-for-construction plans, approved shop drawings, specifications, and any other contract documents. Quality Acceptance will be separate from the Contractor's QC program. However, contractor-performed QC test results for material testing will be used in the acceptance decision if they are validated and/or verified by the Owner Verification test results.

The Quality Control Manager will define the following roles and responsibilities:

- Sampling
- Testing and analysis
- Document control
- Coordination & communication
- Audits
- Mix designs and approvals
- Material certifications processes
- Training and certification
- Acceptance inspection
- Testing material certifications process
- Referee testing
- Maintenance/protection of traffic
- Environmental compliance
- Non-conformance
- Dispute resolution

Innovation and Added Value

Reduced Right-of-Way Impacts

- **SC-83 —** Using FIB 45 for the span will reduce the roadway profile on either side of the bridge and save 8,250 SF of new right of way. Using FIB 45 will shrink the footprint of our improvements while still remaining compliant with RFP criteria.
- **S-58 —** Relocating the existing driveway closer to the bridge will shorten the realignment and allow the owner easier access. This will also save over 2,000 SF of new right of way and reduce the impact to wetlands by over 700 SF.

APPENDIX

CONCEPTUAL PLANS

A

A.1 CONCEPTUAL ROADWAY PLANS

SHEET NO.	TOTAL SHEETS
1	16

INDEX OF SHEETS

SHEET NO.	DESCRIPTION	SHEET
1	TITLE SHEET	1
3	TYPICAL SECTIONS	1
4	RIGHT–OF–WAY DATA SHEET	1
4A	PROPERTY STRIP MAP	1
5	GENERAL CONSTRUCTION NOTES	1
5A–5B	REFERENCE DATA SHEETS	2
6–6A	PLAN & PROFILE SHEETS	2
TC1	TRAFFIC CONTROL SHEETS	OMITTED
PM1	PAVEMENT MARKING & SIGNING SHEET	OMITTED
G1–G3	GEOTECHNICAL DETAIL SHEETS	OMITTED
EC1	EROSION CONTROL DATA SHEETS	OMITTED
U01	UTILITY COORDINATION SHEET	OMITTED
X1–X7	CROSS SECTIONS	7
		16

BRIDGE PLANS BOUND
UNDER A SEPARATE COVER

Design Reference for these plans is the:
2021
SCDOT "Roadway Design Manual"

Hydraulic Design Reference for these plans is the:
2009
Edition of SCDOT's "Requirements for Hydraulic Design Studies"

ENVIRONMENTAL PERMIT INFORMATION		
USACE PERMIT	X YES	___NO
NEPA DOCUMENT	X YES	___NO
401 CERTIFICATION	___YES	X NO
OCRM CAP	___YES	X NO
NAVIGABLE WATERS	X SC ___USCG	___USACE ___N/A

3 DAYS BEFORE DIGGING IN
SOUTH CAROLINA
CALL 811
SOUTH CAROLINA 811 (SC811)
WWW.SC811.COM
ALL UTILITIES MAY NOT BE A MEMBER OF SC811

RAILROAD INVOLVEMENT?
YES / (NO)

TRAFFIC DATA SC83

2026 ADT 570
2046 ADT 700
TRUCKS 12 %

TRAFFIC DATA L-882

2026 ADT 200
2046 ADT NA
TRUCKS NA %

SCDOT
South Carolina Department of Transportation

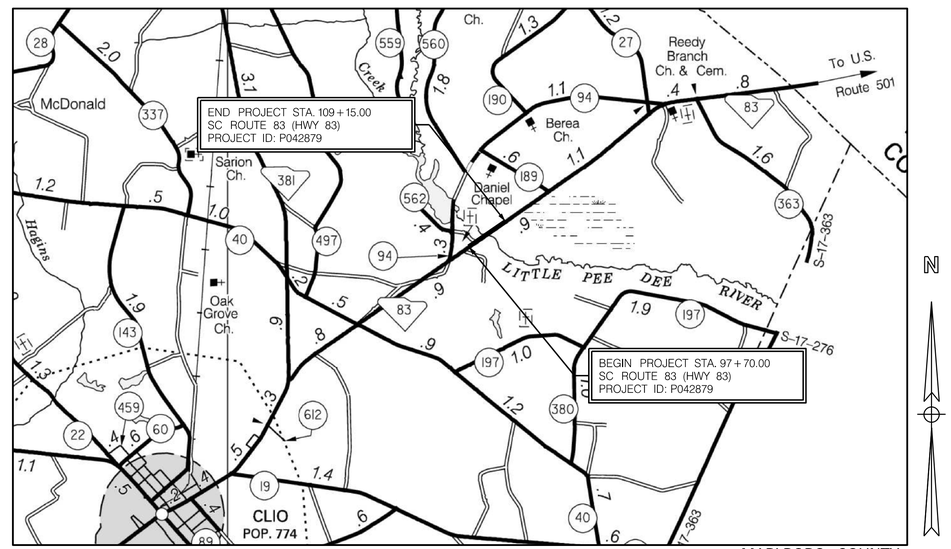
PROPOSED PLANS FOR

MARLBORO COUNTY

PROJECT ID: P042879

SC ROUTE 83 (HWY 83)

ROADWAY APPROACHES TO BRIDGE OVER LITTLE PEE DEE RIVER



LAYOUT
NOT TO SCALE

MARLBORO COUNTY

NPDES PERMIT INFORMATION	
Disturbed Area =	1.80 Acre (s)
Project Area =	3.80 Acre(s)

Approximate Location of Roadway is

Begin
Latitude 34° 36' 37.79" N
Longitude 79° 30' 07.05" W

End
Latitude 34° 36' 43.53" N
Longitude 79° 29' 58.12" W

Hydraulic and NPDES Design provided by:
INFRASTRUCTURE CONSULTING & ENGINEERING
Designs may be obtained from the Design Build Group

CONTRACTOR FIRM
CROWDER

NET LENGTH OF ROADWAY	0.137	MILES
NET LENGTH OF BRIDGES	0.079	MILES
NET LENGTH OF PROJECT	0.217	MILES
LENGTH OF EXCEPTIONS	0.000	MILES
GROSS LENGTH OF PROJECT	0.217	MILES

EQUALITIES IN STATIONING
NONE

NOTE: EXCEPT AS MAY BE OTHERWISE SPECIFIED ON THE PLANS OR IN THE SPECIAL PROVISIONS, ALL MATERIALS AND WORKMANSHIP ON THIS PROJECT SHALL CONFORM TO THE SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (2025 EDITION) AND THE STANDARD DRAWINGS FOR ROAD CONSTRUCTION IN EFFECT AT THE TIME OF FINAL RFP.

CONSULTING ENGINEERING FIRM
INFRASTRUCTURE
CONSULTING & ENGINEERING

ENGINEER OF RECORD

PRELIMINARY
NOT FOR CONSTRUCTION

FOR CONSTRUCTION : _____ DATE _____

c:\bms\ice-eng-pw-01\d013241\SHT_01.dgn
6/12/2025

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	MARLBORO	P042879	SC83	3

TYPICAL SECTION OF IMPROVEMENT
SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION
COLUMBIA, S.C.

GENERAL NOTES:

SEE PLANS AND CROSS SECTIONS FOR LOCATIONS OF DITCH AND FILL SECTIONS

TIE TO EXISTING TRAFFIC LANE AND SHOULDER WIDTHS

SHOULDER WIDTH VARIES AT GUARDRAIL LOCATIONS, SEE "TYPICAL GUARDRAIL SECTION FOR OUTSIDE SHOULDER". SEE PLANS FOR GUARDRAIL LOCATIONS.

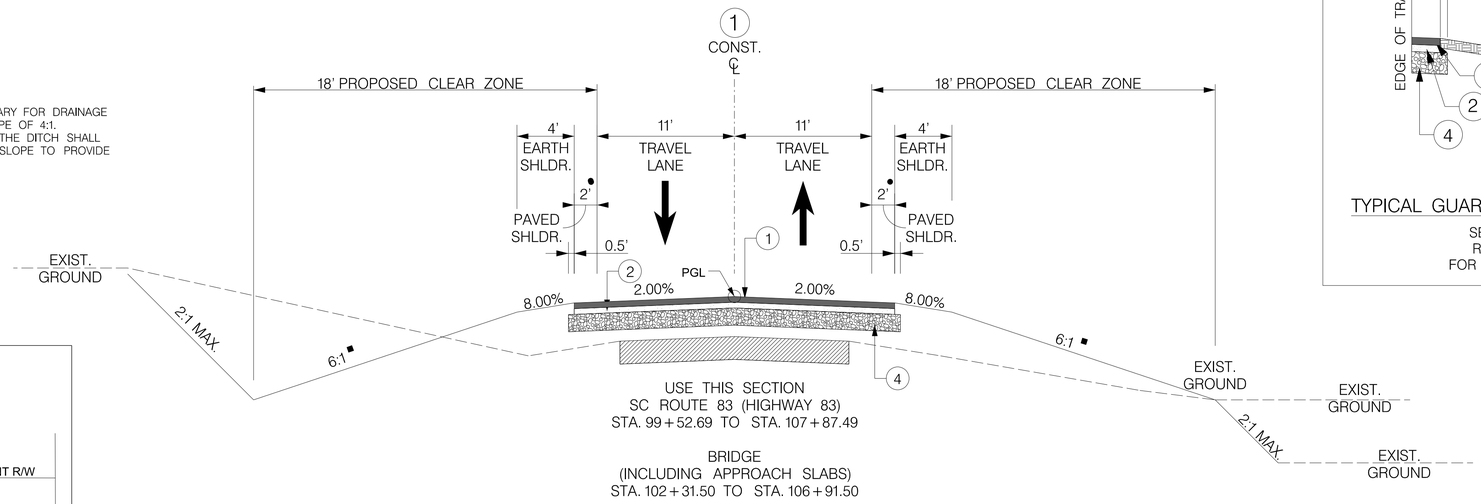
SECTION NOTES:

■ VARIABLE – SEE CROSS SECTIONS FOR REQUIRED SLOPE VALUES PROVIDE SMOOTH TRANSITIONS BETWEEN SLOPE CHANGES

6:1 SLOPE 0'–5' FILL HEIGHT
4:1 SLOPE 5'–10' FILL HEIGHT
2:1 MAX SLOPE >10' FILL HEIGHT AND AT BRIDGE ENDS

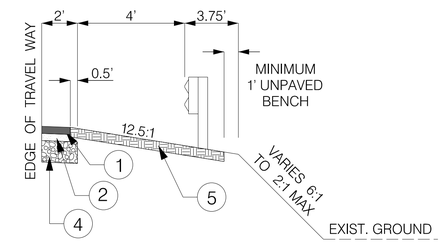
● INSTALL MILLED IN RUMBLE STRIP IN ACCORDANCE WITH STD. DWG. 633–105–00

DITCH SLOPES MAY BE VARIED WHEN A DEEPER DITCH IS NECESSARY FOR DRAINAGE PURPOSES, USING A MINIMUM SLOPE OF 12:1 AND A MAXIMUM SLOPE OF 4:1. WHERE A DEEPER DITCH PROVIDED BY A 4:1 SLOPE IS NECESSARY, THE DITCH SHALL BE PLACED FURTHER FROM THE CENTERLINE CONTINUING THE 4:1 SLOPE TO PROVIDE THE NECESSARY DEPTH.



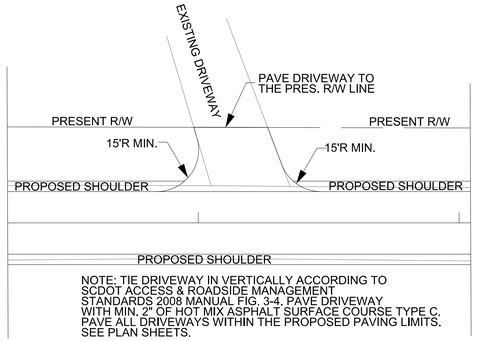
USE THIS SECTION
SC ROUTE 83 (HIGHWAY 83)
STA. 99+52.69 TO STA. 107+87.49

BRIDGE
(INCLUDING APPROACH SLABS)
STA. 102+31.50 TO STA. 106+91.50



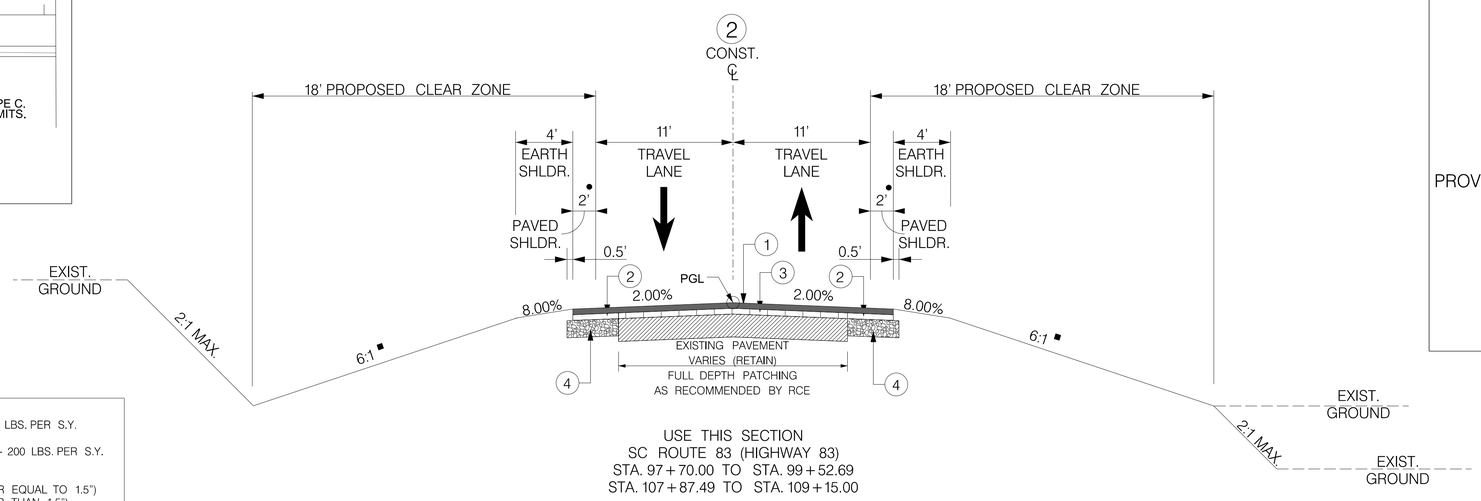
TYPICAL GUARDRAIL SECTION FOR OUTSIDE SHOULDER

SEE PLANS FOR GUARDRAIL LOCATIONS
REFER TO SCDOT STD. DWG. 805–115–10
FOR GRADING SCHEME AT END TREATMENTS



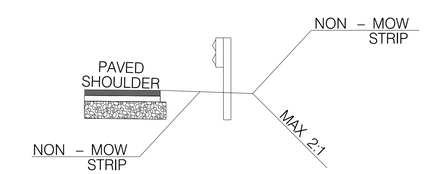
NOTE: TIE DRIVEWAY IN VERTICALLY ACCORDING TO SCDOT ACCESS & ROADSIDE MANAGEMENT STANDARDS 2008 MANUAL FIG. 3-4. PAVE DRIVEWAY WITH MIN. 2" OF HOT MIX ASPHALT SURFACE COURSE TYPE C. PAVE ALL DRIVEWAYS WITHIN THE PROPOSED PAVING LIMITS. SEE PLAN SHEETS.

DRIVEWAY DETAIL
N.T.S.



USE THIS SECTION
SC ROUTE 83 (HIGHWAY 83)
STA. 97+70.00 TO STA. 99+52.69
STA. 107+87.49 TO STA. 109+15.00

PAVING UNDER GUARDRAIL DETAIL



PROVIDE NON–MOW STRIP UNDER GUARDRAIL.

①	HOT MIX ASPHALT SURFACE COURSE TYPE C – 200 LBS. PER S.Y.	
②	HOT MIX ASPHALT INTERMEDIATE COURSE TYPE C – 200 LBS. PER S.Y.	
③	VARIABLE HOT MIX ASPHALT FOR BUILDUP HOT MIX ASPHALT SURFACE TYPE E (LESS THAN OR EQUAL TO 1.5") HOT MIX ASHPALT INTERMEDIATE TYPE B (GREATHER THAN 1.5")	
④	HOT MIX ASPHALT BASE COURSE TYPE B – 600 LBS. PER S.Y.	
⑤	4" HOT MIX ASPHALT SURFACE COURSE (NON–MOWABLE STRIP)	

CROWDER

INFRASTRUCTURE CONSULTING & ENGINEERING

FUNCTIONAL CLASSIFICATION
SC83 (HIGHWAY 83)
RURAL
MAJOR COLLECTOR

RTE. SC RTE 83 DESIGN SPEED		
MPH	FROM STA.	TO STA.
60	97+70.00	109+15.00
EXCEPTIONS TO DESIGN SPEED		

PRELIMINARY
NOT FOR CONSTRUCTION

N.T.S.

REV. NO.	BY	DATE	DESCRIPTION OF REVISION
6			
5			
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SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

TYPICAL SECTION

SC ROUTE 83
OVER LITTLE PEE DEE RIVER

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6/12/2025

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	MARLBORO	P042879	SC83	4

SCDOT
South Carolina Department of Transportation

RIGHT-OF-WAY DATA SHEET

TRACT NO.	PROPERTY OWNER	TAX MAP REFERENCE	TOTAL TRACT ACRES	OBTAIN: OUTFALL DITCH	OBTAIN: LEFT	OBTAIN: RIGHT	OBTAIN: TOTAL	REMAINDER LEFT ACRES [A]	REMAINDER RIGHT ACRES [A]	DATE ACQUIRED	TYPE OF INSTRUMENT	PERMISSION (YES): OUTFALL DITCH	PERMISSION (YES): SLOPE	PERMISSION (YES): DRAINAGE STRUCTURE	PERMISSION (YES): EROSION CONTROL	PERMISSION (YES): ENTRANCE CONSTRUCTION	REMARKS
1	MCDONALD, HUBBARD W. JR.	042-00-01-001	241.63														
2	ACKERMAN, DAVID AND JAMES ALLEN	035-01-02-028	0.15														
3	ACKERMAN, DAVID AND JAMES ALLEN	035-01-02-029	0.15														
4	AMERICAN LEGION	035-01-02-036	4.50		10608 SF (0.244 AC)		10608 SF (0.244 AC)	4.256									
5	MC AND G FARMS, LLC	035-01-02-037	470.00		2267 SF (0.052 AC)	14588 SF (0.335 AC)	16855 SF (0.387 AC)	155.549	339.459								
6	MCDONALD, HUBBARD W. JR.	042-00-01-002	23.00			14538 SF (0.334 AC)	14538 SF (0.334 AC)		22.666								

DATE	TRACT NO.	REMARKS

R/W NOTE: THE DEPARTMENT WILL UTILIZE THE PRESENT RIGHT OF WAY AS SHOWN BELOW EXCEPT AS OTHERWISE SHOWN ON PLANS.

ROAD / ROUTE #	FILE #	R/W WIDTH	YEAR ACQ'D.

NOTES:

A. SHOW REMAINDER IN SQUARE FEET WHEN LESS THAN 0.25 ACRE.

6/5/2025
c:\bms\ice-eng-pw-01\d0132411\RWDS_SC83.xlsx]RWDS

CROWDER

INFRASTRUCTURE CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

REV. NO.	BY	DATE	DESCRIPTION OF REVISION
6			
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SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

RIGHT-OF-WAY DATA SHEET

SC ROUTE 83
OVER LITTLE PEE DEE RIVER

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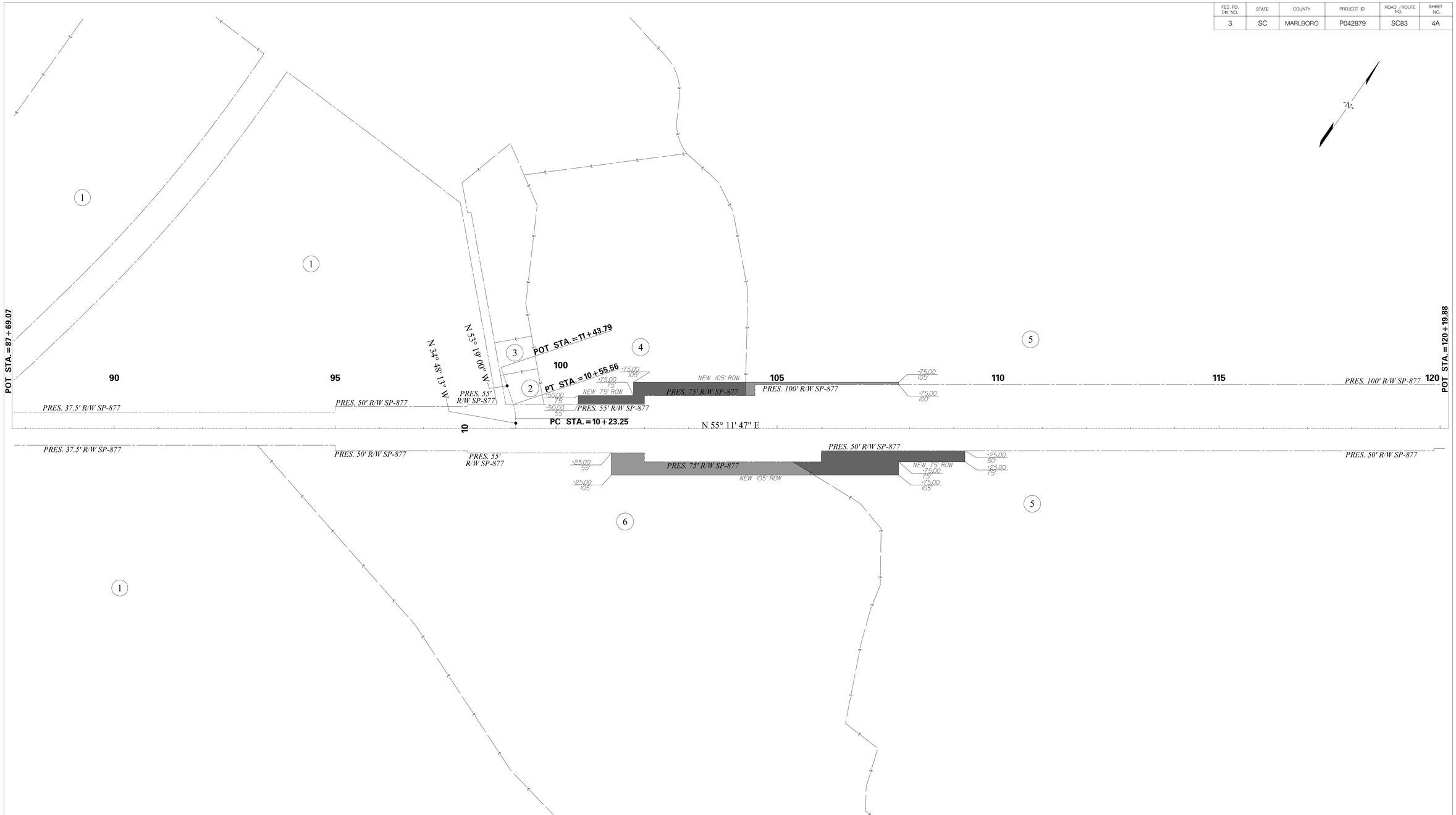
6/12/2025

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	MARLBORO	P042879	SC83	4A



CROWDER

0 50' 100' 200'
SCALE : 1" = 100'

INFRASTRUCTURE
CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

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SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

PROPERTY STRIP MAP

SC ROUTE 83
OVER LITTLE PEE DEE RIVER

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6/12/2025

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	MARLBORO	P042879	SC83	5

SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION

GENERAL CONSTRUCTION NOTE

THE DEPUTY SECRETARY OF ENGINEERING MUST SPECIFICALLY AUTHORIZE CHANGES INVOLVING INCREASED COST OF PROJECT OR CHANGES IN ALIGNMENT. THE DISTRICT ENGINEERING ADMINISTRATOR IS PERMITTED UNDER THE DIRECTION OF THE DEPUTY SECRETARY OF ENGINEERING TO AUTHORIZE MINOR ALTERATIONS NOT IN CONFLICT WITH THE STANDARD PRACTICES OF THE DEPARTMENT. FORWARD INFORMATION ON ANY PROPOSED CHANGES IN ALIGNMENT TO THE COLUMBIA OFFICE AS SOON AS POSSIBLE.

SEE INDIVIDUAL CURVES ON REFERENCE DATA SHEET FOR SUPERELEVATION RATE AND DESIGN SPEED, AS APPLICABLE.

NOTES:

1. THIS PROJECT IS BASED ON A LUMP SUM BASIS
2. SEE FINAL RFP FOR ADDITIONAL REQUIREMENTS

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6/12/2025

CROWDER

INFRASTRUCTURE
CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

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SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

GENERAL CONSTRUCTION NOTES

SC ROUTE 83
OVER LITTLE PEE DEE RIVER

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD/ROUTE NO.	SHEET NO.
3	SC	MARLBORO	N/A	SC 83 (EMERGENCY)	5A

PROPERTY MONUMENTS FOUND

ALIGNMENT	STATION	OFFSET	NORTHING	EASTING	DESCRIPTION
SC 83	98+50.94	-0.22	1013603.90	2450569.12	IP SPIKE
SC 83	92+91.10	-0.01	1013284.19	2450109.55	IP SPIKE
SC 83	OFF CHAIN		1012347.05	2449220.03	IP #5REBAR
SC 83	OFF CHAIN		1012432.53	2449341.79	IP #5REBAR
SC 83	98+65.93	-55.06	1013657.48	2450550.13	IP ROD
SC 83	OFF CHAIN		1013985.04	2450221.90	IP RR TIE

SURVEY CONTROL POINTS

POINT ID	ALIGNMENT	STATION	OFFSET	NORTHING	EASTING	ELEV.	DESCRIPTION
1	SC 83	OFF CHAIN		1012569.2615	2449056.7225	173.56	PSC RB/CAP
2	SC 83	87+69.02	-14.34	1012997.9716	2449672.6757	165.42	PSC RB/CAP
3	SC 83	95+68.49	13.71	1013431.2543	2450345.1439	158.67	CP #5REBAR
4	SC 83	101+68.57	-13.62	1013796.1981	2450822.2825	142.65	CP #5REBAR
5	SC 83	106+23.20	15.31	1014031.9245	2451212.0970	140.33	CP #5REBAR
6	SC 83	112+71.77	15.87	1014401.6445	2451744.9664	138.22	CP #5REBAR
7	SC 83	120+19.70	13.85	1014830.2007	2452357.9520	137.73	PSC RB/CAP
8	SC 83	OFF CHAIN		1015257.8242	2452973.9665	137.63	PSC RB/CAP

PROJECT BENCHMARKS

POINT ID	ALIGNMENT	STATION	OFFSET	NORTHING	EASTING	ELEV.	DESCRIPTION
9	SC 83	101+09.89	29.31	1013727.4493	2450798.5979	144.20	BM 1
10	SC 83	108+93.35	27.53	1014176.0799	2451440.8936	140.43	BM 2

NOTES:

1. The alignment Station and Offset are referenced to the existing Survey Centerline.
2. Date of Survey: 8-27-2024

The Property Monuments Found listed on this sheet are assumed to be property corner monuments, field located during the course of this survey. The Department makes no claim that these located monuments are the true position of any property and takes no responsibility for this information being used as such. These monuments are tied to the control of this project in an effort to document and preserve their location in the event they are disturbed or destroyed during the construction of the project.

SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

SURVEY CONTROL DATA

PROJECT DESCRIPTION

SC 83 (Emergency)

DATUM DESCRIPTION

This GRID Coordinate System developed for this project is based on NAD83(2011) South Carolina State Plane Coordinate System. A Combined Scale Factor (CSF) for each Survey Control Point must be computed and applied to horizontal ground distances. Elevations for this project are based on NAVD88 (VRS) for PSC 2 with an Elevation of 165.42'

SOUTH CAROLINA PROFESSIONAL LAND SURVEYOR No. 26610 MICHAEL WESLEY TODD II

2024.08.27 15:28:39 -04'00'

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6/12/2025
J:\ToddMW\[SC 83 (EMERGENCY).xlsm]SCDS 5A

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	MARLBORO	P042879	SC83	5B

SC83 (HWY 83)

Beginning chain PR_SC83 description
==========

Point 150 N 1,012,986.2268 E 2,449,680.9054 Sta 87+69.07

Course from 150 to 151 N 55° 11' 47.18" E Dist 3,250.8144

Point 151 N 1,014,841.6766 E 2,452,350.1937 Sta 120+19.88

==========
Ending chain PR_SC83 description

BLUFF CEMETERY ROAD

Beginning chain BLUFF description
==========

Point 250 N 1,013,636.5888 E 2,450,616.5296 Sta 10+00.00

Course from 250 to PC BLUFF-1 N 34° 48' 12.82" W Dist 23.2480

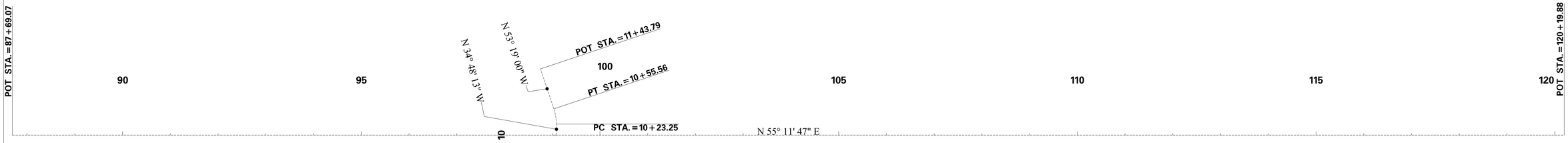
Curve Data

Curve BLUFF-1
P.I. Station 10+39.55 N 1,013,669.0603 E 2,450,593.9583
Delta = 18° 30' 46.86" (LT)
Degree = 57° 17' 44.81"
Tangent = 16.2977
Length = 32.3113
Radius = 100.0000
External = 1.3194
Long Chord = 32.1709
Mid. Ord. = 1.3022
P.C. Station 10+23.25 N 1,013,655.6780 E 2,450,603.2604
P.T. Station 10+55.56 N 1,013,678.7964 E 2,450,580.8884
C.C. N 1,013,598.6016 E 2,450,521.1491
Back = N 34° 48' 12.82" W
Ahead = N 53° 18' 59.68" W
Chord Bear = N 44° 03' 36.25" W

Course from PT BLUFF-1 to 251 N 53° 18' 59.68" W Dist 88.2272

Point 251 N 1,013,731.5028 E 2,450,510.1347 Sta 11+43.79

==========
Ending chain BLUFF description



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6/12/2025

CROWDER

0 50' 100' 200'
SCALE : 1" = 100'

INFRASTRUCTURE
CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

REV. NO.	BY	DATE	DESCRIPTION OF REVISION
6			
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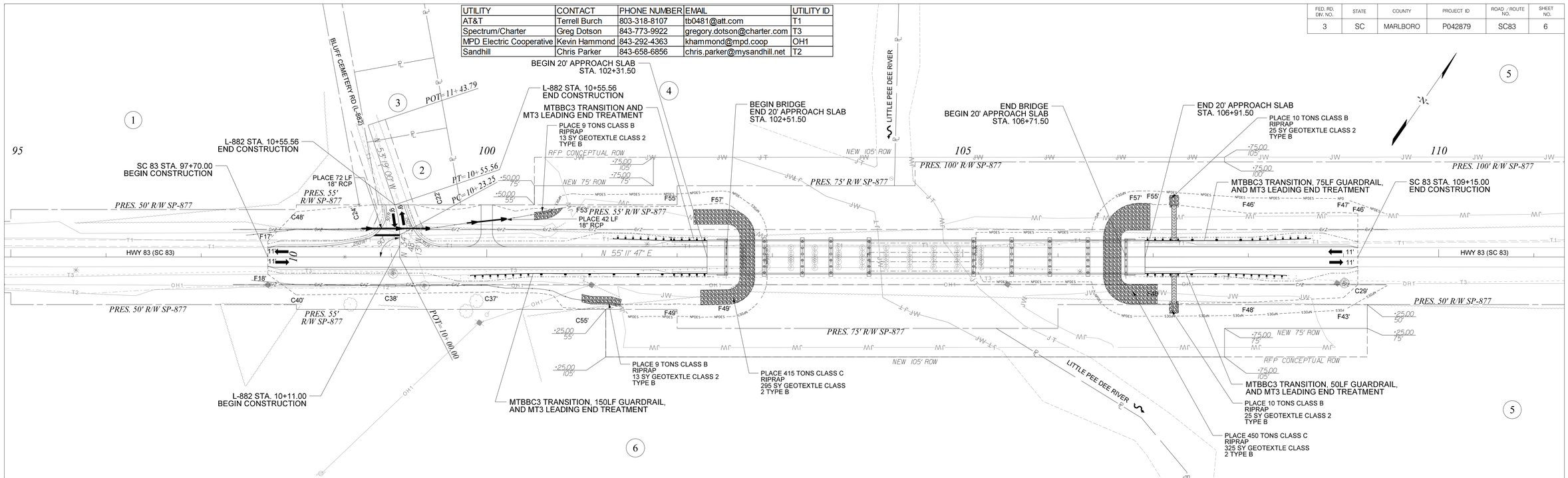
SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

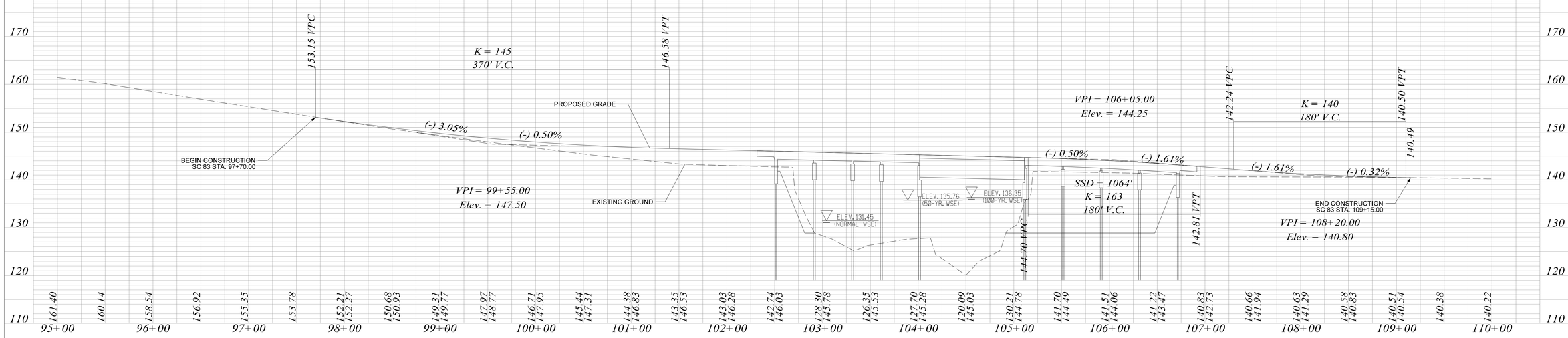
REFERENCE DATA SHEET

SC ROUTE 83
OVER LITTLE PEE DEE RIVER

UTILITY	CONTACT	PHONE NUMBER	EMAIL	UTILITY ID
AT&T	Terrell Burch	803-318-8107	tb0481@att.com	T1
Spectrum/Charter	Greg Dotson	843-773-9922	gregory.dotson@charter.com	T3
MPD Electric Cooperative	Kevin Hammond	843-292-4363	khammond@mpd.coop	OH1
Sandhill	Chris Parker	843-658-6856	chris.parker@mysandhill.net	T2

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	MARLBORO	P042879	SC83	6





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6/12/2025

CROWDER

0 25' 50' 100'
HORIZONTAL SCALE: 1" = 50'
VERTICAL SCALE: 1" = 10'

ALIGNMENT CONTROL CAN BE FOUND ON REFERENCE DATA SHEET 5A & 5B

INFRASTRUCTURE CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

REV. NO.	BY	DATE	DESCRIPTION OF REVISION
6			
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SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE SHEET

SC ROUTE 83
OVER LITTLE PEE DEE RIVER

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	MARLBORO	P042879	SC83	6A

CROWDER

0 25' 50' 100'
HORIZONTAL SCALE: 1" = 50'
VERTICAL SCALE: 1" = 10'

ALIGNMENT CONTROL CAN BE FOUND ON REFERENCE DATA SHEET 5A & 5B

INFRASTRUCTURE CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

REV. NO.	BY	DATE	DESCRIPTION OF REVISION
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SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE SHEET

SC ROUTE 83
OVER LITTLE PEE DEE RIVER

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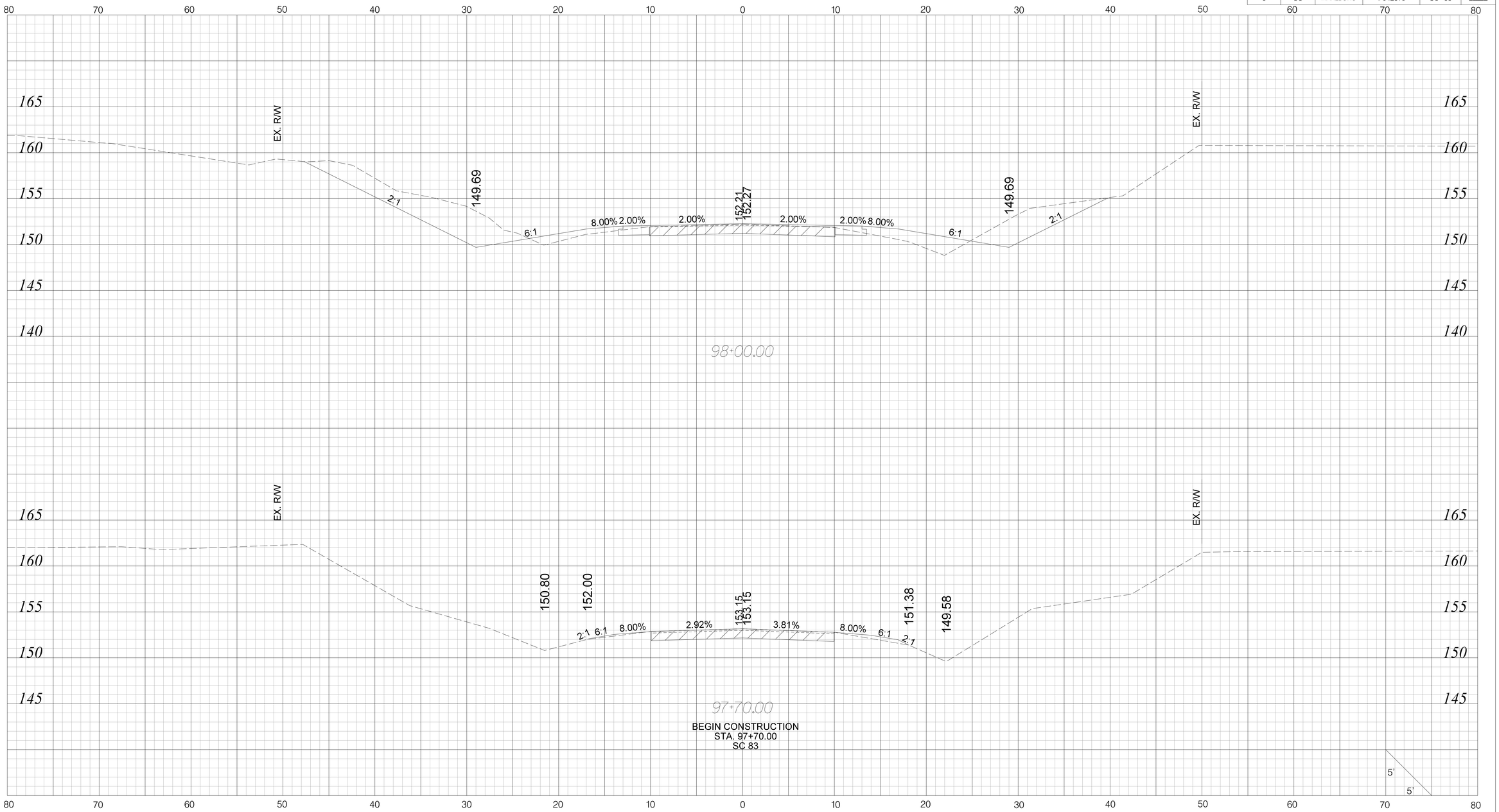
6/12/2025

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	MARLBORO	P042879	SC 83	X1



CROWDER

INFRASTRUCTURE CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

REV. NO.	BY	DATE	DESCRIPTION OF REVISION
4			
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SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

CROSS SECTION SHEET

SC 83 (HWY 83) OVER
LITTLE PEE DEE RIVER

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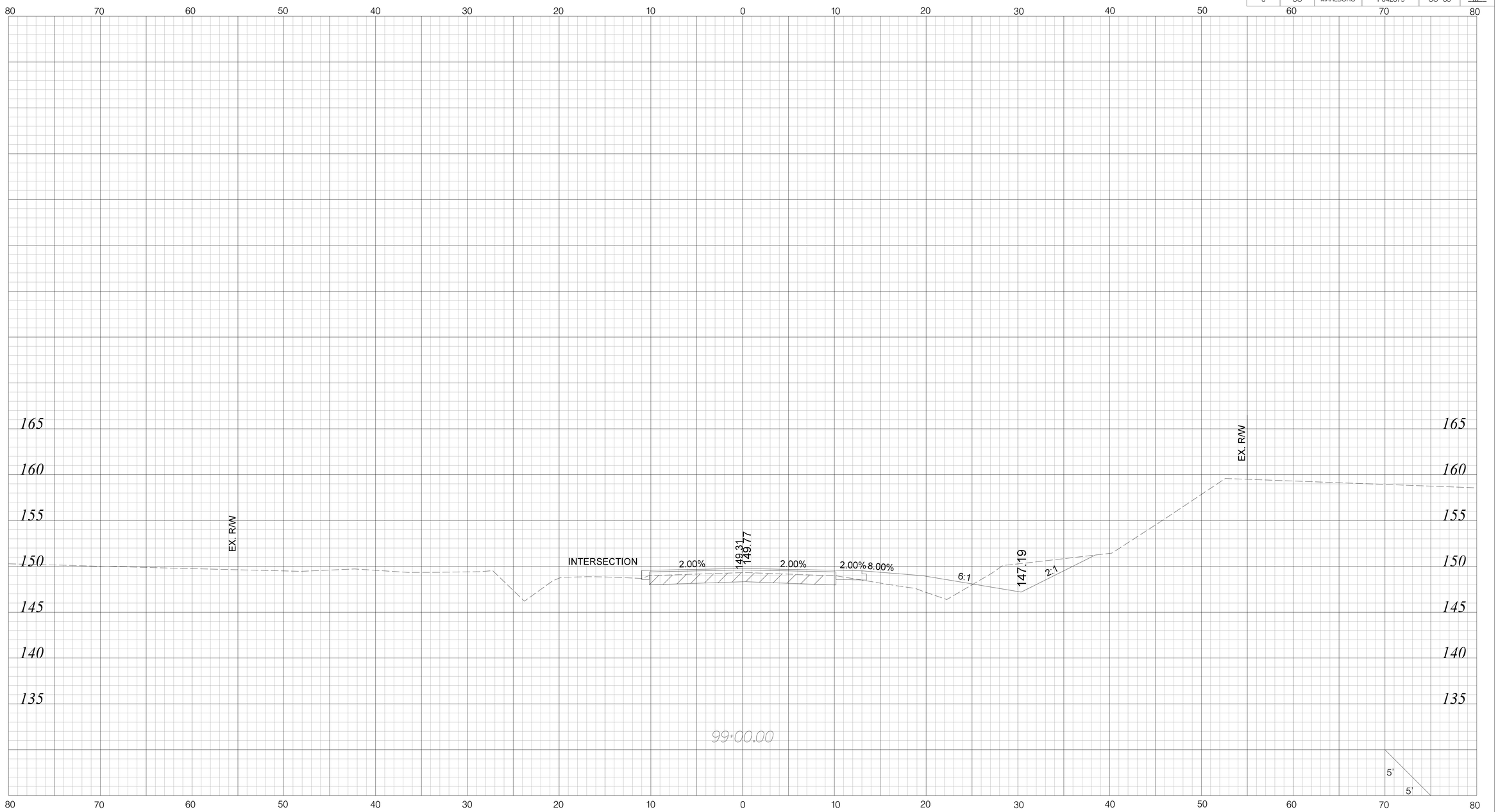
FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	MARLBORO	P042879	SC 83	X2



CROWDER

INFRASTRUCTURE CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

REV. NO.	BY	DATE	DESCRIPTION OF REVISION
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SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

CROSS SECTION SHEET

SC 83 (HWY 83) OVER
LITTLE PEE DEE RIVER

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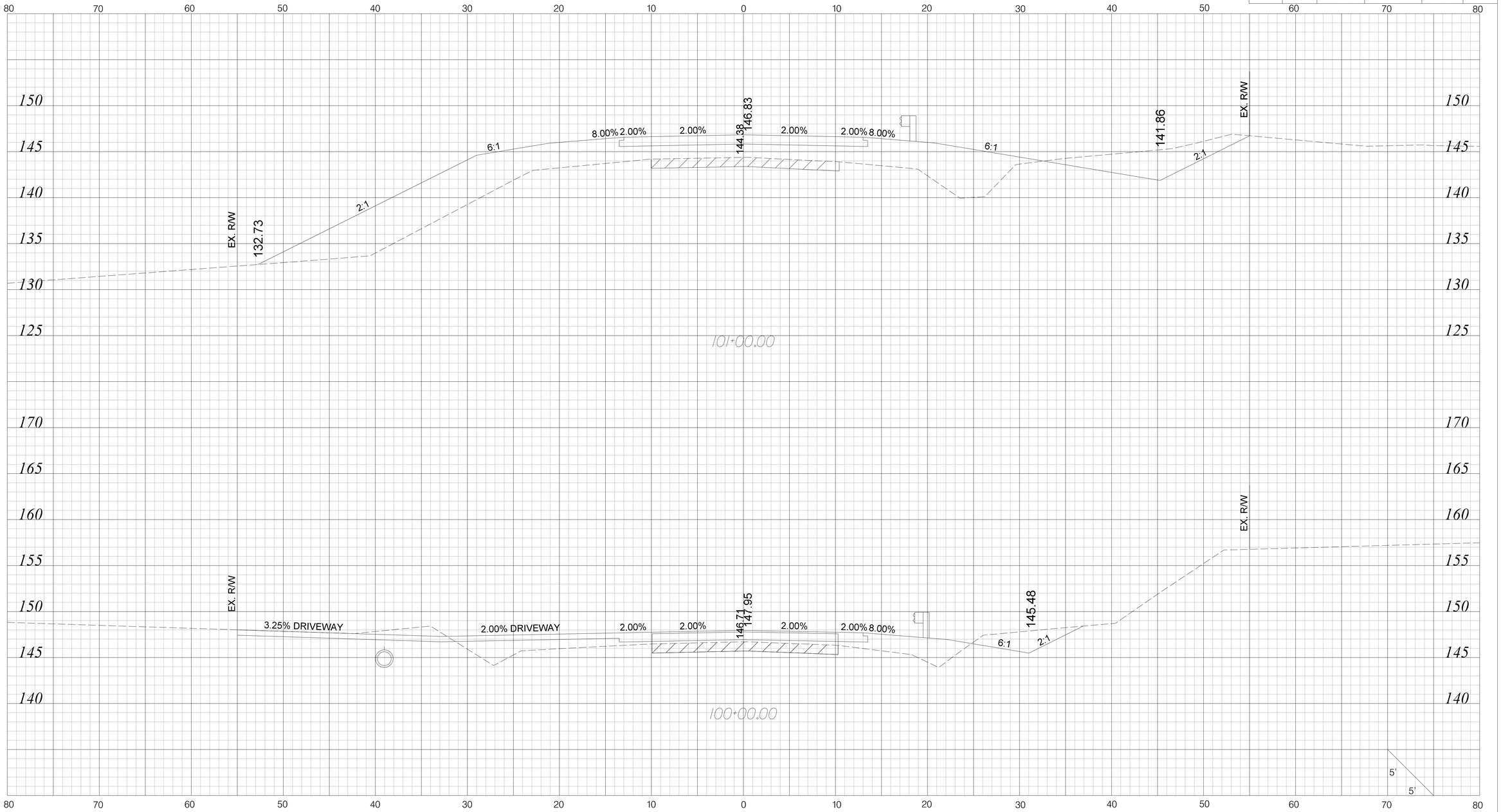
6/12/2025

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	MARLBORO	P042879	SC 83	X3



CROWDER

INFRASTRUCTURE CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

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SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

CROSS SECTION SHEET

SC 83 (HWY 83) OVER
LITTLE PEE DEE RIVER

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6/12/2025

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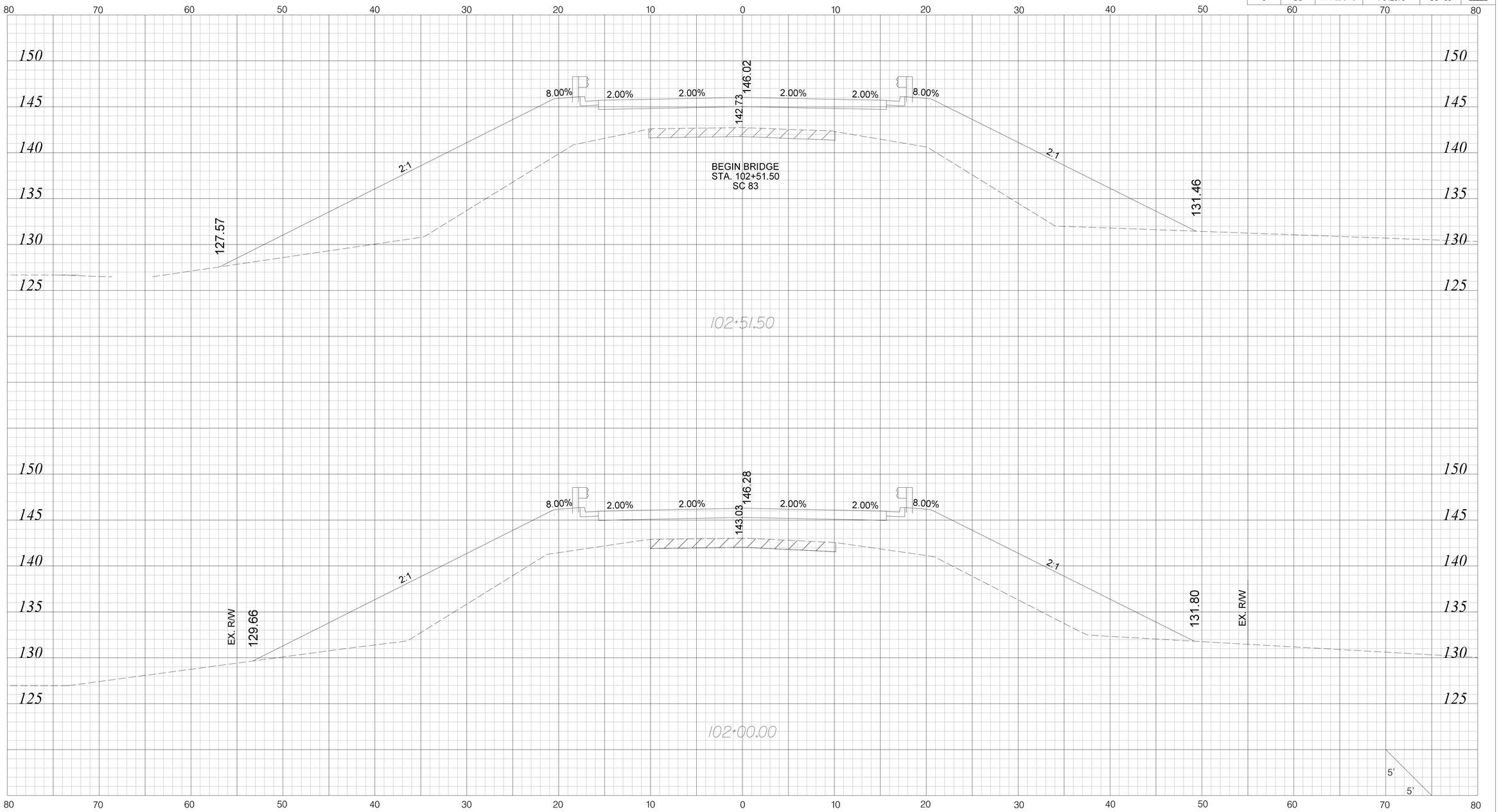
3	SC	MARLBORO	P042879	SC 83	X4



CROWDER

INFRASTRUCTURE CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

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SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

CROSS SECTION SHEET

SC 83 (HWY 83) OVER
LITTLE PEE DEE RIVER

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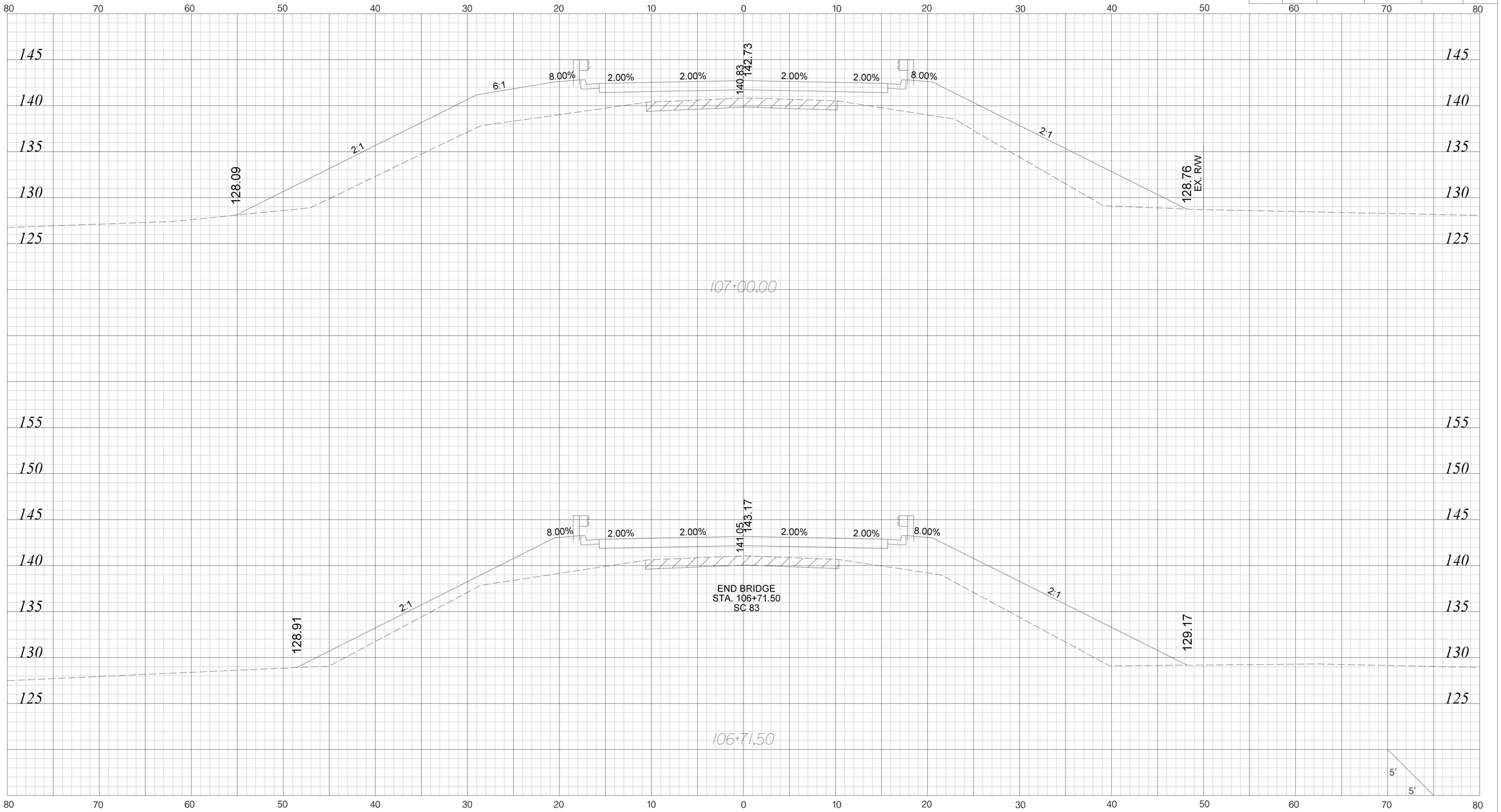
3	SC	MARLBORO	P042879	SC 83	X5



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CROWDER

INFRASTRUCTURE
CONSULTING & ENGINEERING

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NOT FOR CONSTRUCTION

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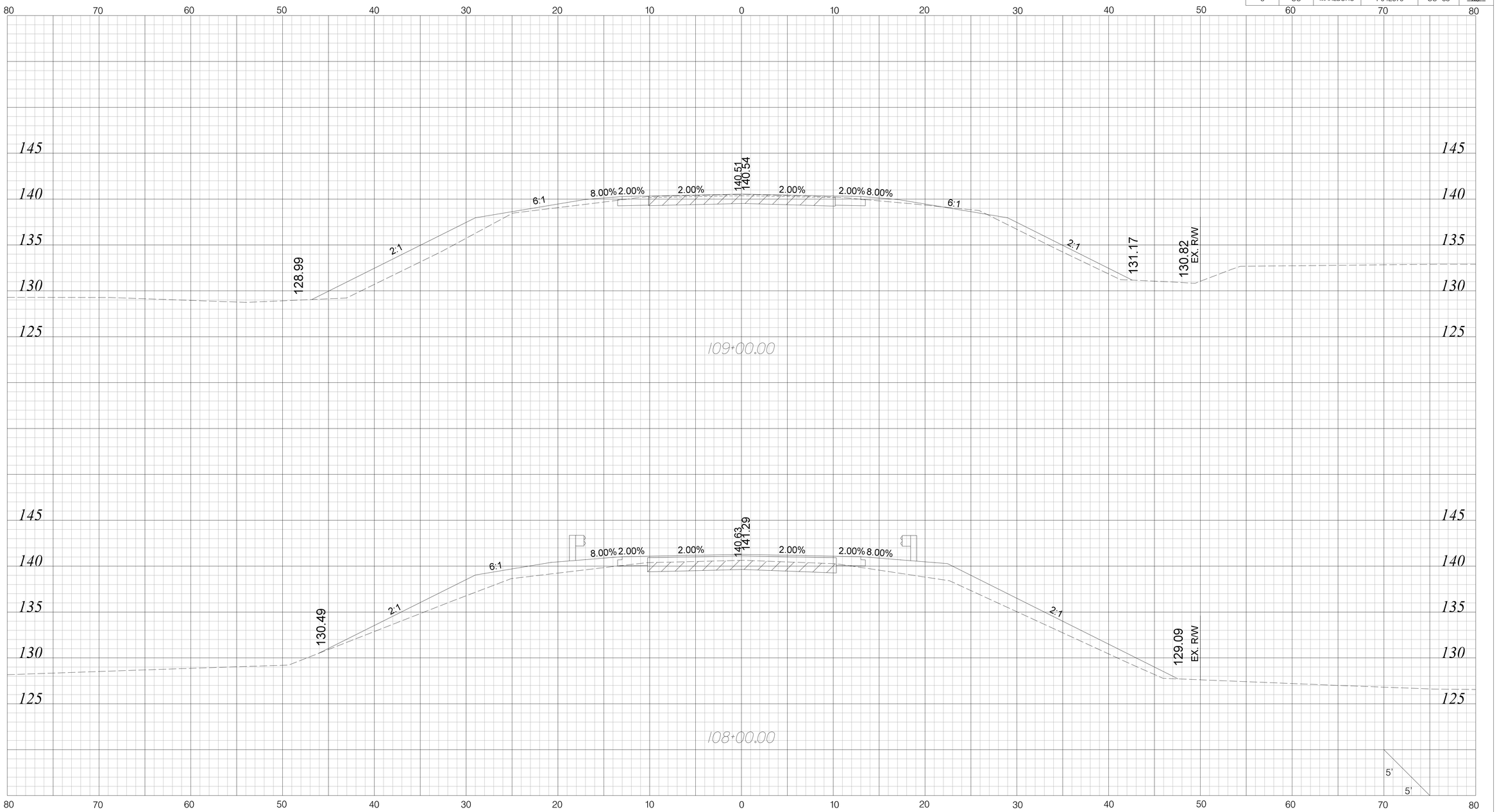
SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

CROSS SECTION SHEET

SC 83 (HWY 83) OVER
LITTLE PEE DEE RIVER

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	MARLBORO	P042879	SC 83	X6



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CROWDER

INFRASTRUCTURE CONSULTING & ENGINEERING

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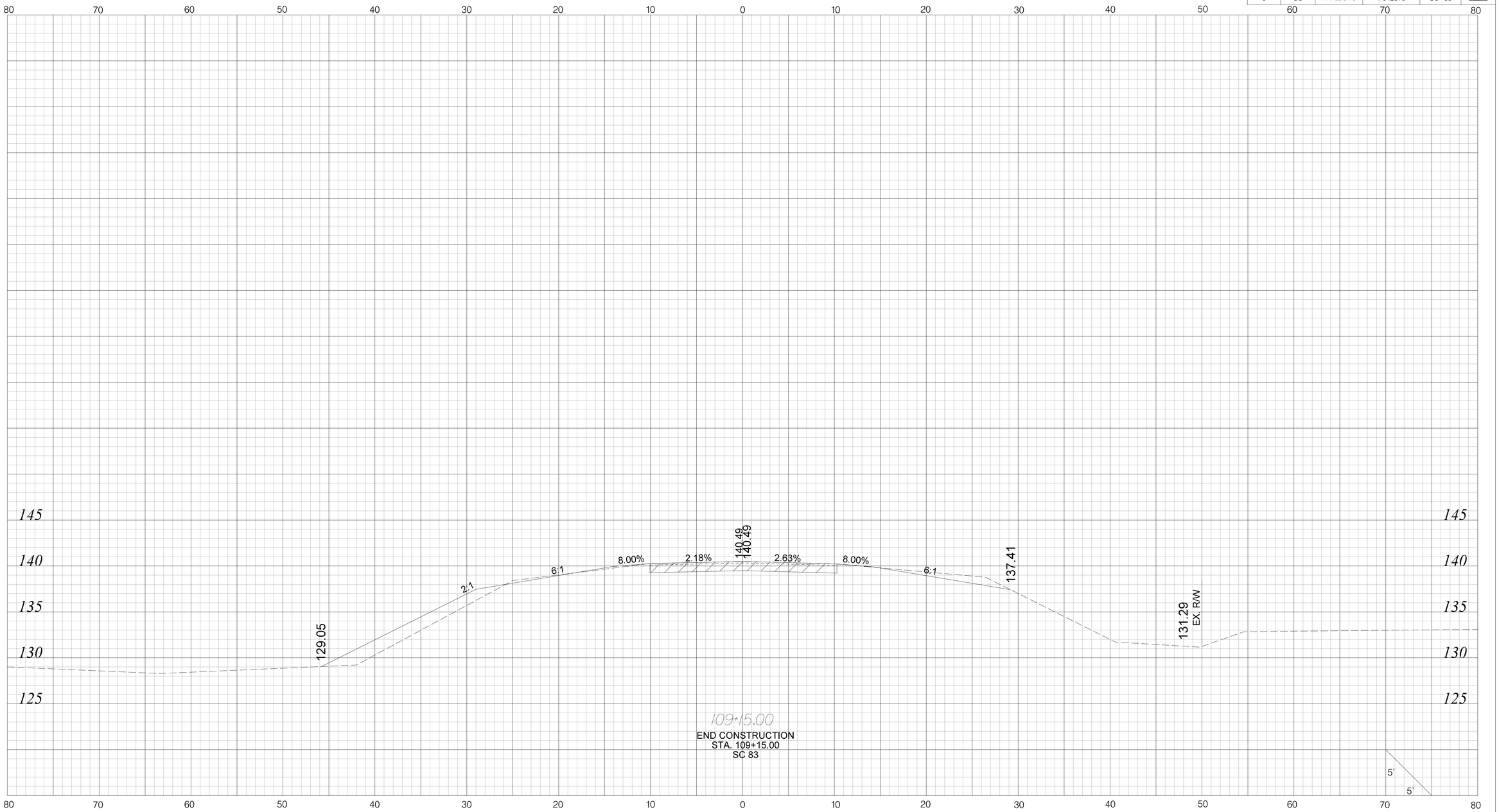
SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

CROSS SECTION SHEET

SC 83 (HWY 83) OVER
LITTLE PEE DEE RIVER

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	MARLBORO	P042879	SC 83	XT



CROWDER

INFRASTRUCTURE CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

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SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

CROSS SECTION SHEET

SC 83 (HWY 83) OVER
LITTLE PEE DEE RIVER

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6/12/2025

SHEET NO.	TOTAL SHEETS
1	8

INDEX OF SHEETS

SHEET NO.	DESCRIPTION	SHEET
1	TITLE SHEET	1
3	TYPICAL SECTIONS	1
4	RIGHT–OF–WAY DATA SHEET	1
4A	PROPERTY STRIP MAP	1
5	GENERAL CONSTRUCTION NOTES	1
5A–5B	REFERENCE DATA SHEETS	2
6	PLAN & PROFILE SHEETS	1
TC1	TRAFFIC CONTROL SHEETS	OMITTED
PM1	PAVEMENT MARKING & SIGNING SHEET	OMITTED
G1–G3	GEOTECHNICAL DETAIL SHEETS	OMITTED
EC1	EROSION CONTROL DATA SHEETS	OMITTED
U01	UTILITY COORDINATION SHEET	OMITTED
X1–X9	CROSS SECTIONS	OMITTED
		8

SCDOT

South Carolina Department of Transportation

PROPOSED PLANS FOR

DILLON COUNTY

PROJECT ID: P043715

S-58 (GADDYS MILL ROAD)

ROADWAY APPROACHES TO BRIDGE OVER BEAVERDAM CREEK

BRIDGE PLANS BOUND UNDER A SEPARATE COVER

Design Reference for these plans is the:

2021

SCDOT "Roadway Design Manual"

Design Reference for these plans is the:

LVB

Supplemental Design Criteria For Low Volume Bridge Replacement Projects

Hydraulic Design Reference for these plans is the:

2009

Edition of SCDOT's "Requirements for Hydraulic Design Studies"

ENVIRONMENTAL PERMIT INFORMATION

USACE PERMIT	X YES	NO		
NEPA DOCUMENT	X YES	NO		
401 CERTIFICATION	YES	X NO		
OCRM CAP	YES	X NO		
NAVIGABLE WATERS	SC	USCG	USACE	X N/A

3 DAYS BEFORE DIGGING IN SOUTH CAROLINA

CALL 811

SOUTH CAROLINA 811 (SC811)
WWW.SC811.COM
ALL UTILITIES MAY NOT BE A MEMBER OF SC811

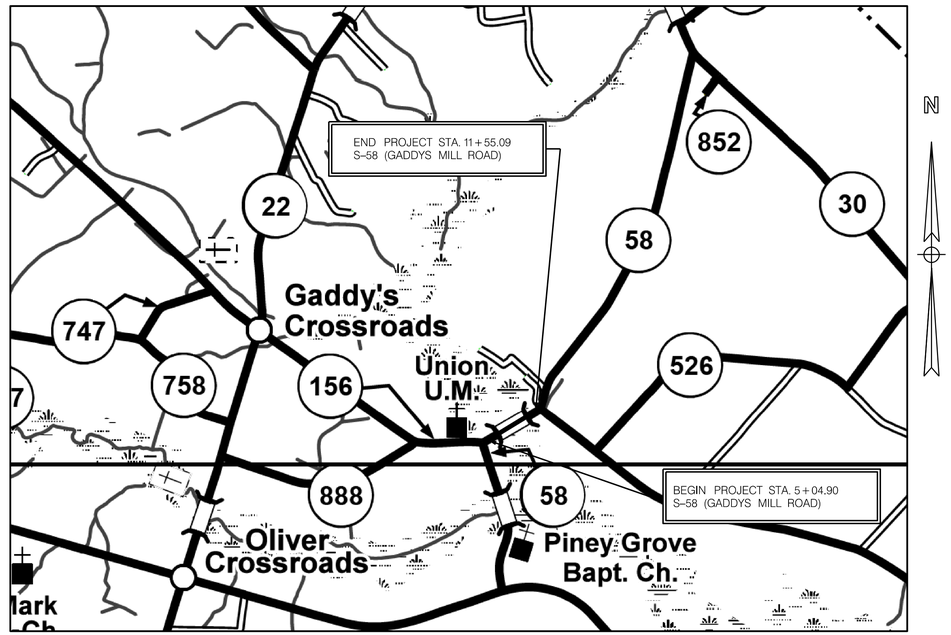
RAILROAD INVOLVEMENT?
YES / (NO)

TRAFFIC DATA

2026 ADT 80

2046 ADT 160

TRUCKS 9 %



LAYOUT
NOT TO SCALE

DILLON COUNTY

NPDES PERMIT INFORMATION

Disturbed Area = 0.89 Acre (s)

Project Area = 2.89 Acre(s)

Approximate Location of Roadway is

Begin
Latitude 34° 23' 33.71" N
Longitude 79° 14' 21.35" W

End
Latitude 34° 23' 36.94" N
Longitude 79° 14' 14.77" W

Hydraulic and NPDES Design provided by:

INFRASTRUCTURE CONSULTING & ENGINEERING

Designs may be obtained from the Design Build Group

CONTRACTOR FIRM

CROWDER

NET LENGTH OF ROADWAY	0.095	MILES
NET LENGTH OF BRIDGES	0.028	MILES
NET LENGTH OF PROJECT	0.123	MILES
LENGTH OF EXCEPTIONS	0.000	MILES
GROSS LENGTH OF PROJECT	0.123	MILES

EQUALITIES IN STATIONING
NONE

NOTE: EXCEPT AS MAY BE OTHERWISE SPECIFIED ON THE PLANS OR IN THE SPECIAL PROVISIONS, ALL MATERIALS AND WORKMANSHIP ON THIS PROJECT SHALL CONFORM TO THE SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (2025 EDITION) AND THE STANDARD DRAWINGS FOR ROAD CONSTRUCTION IN EFFECT AT THE TIME OF FINAL RFP.

CONSULTING ENGINEERING FIRM

INFRASTRUCTURE CONSULTING & ENGINEERING

ENGINEER OF RECORD

PRELIMINARY
NOT FOR CONSTRUCTION

FOR CONSTRUCTION : DATE

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6/12/2025

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	DILLON	P043715	S-58	3

TYPICAL SECTION OF IMPROVEMENT
SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION
COLUMBIA, S.C.

GENERAL NOTES:

SEE PLANS AND CROSS SECTIONS FOR LOCATIONS OF DITCH AND FILL SECTIONS

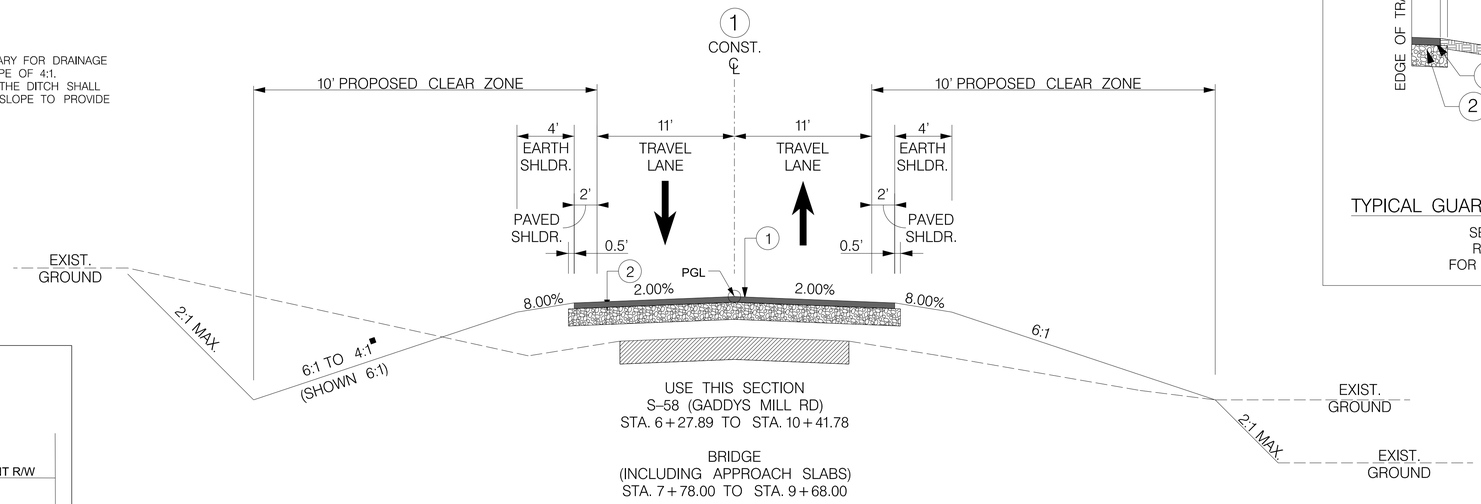
TIE TO EXISTING TRAFFIC LANE AND SHOULDER WIDTHS

SHOULDER WIDTH VARIES AT GUARDRAIL LOCATIONS, SEE "TYPICAL GUARDRAIL SECTION FOR OUTSIDE SHOULDER". SEE PLANS FOR GUARDRAIL LOCATIONS.

SECTION NOTES:

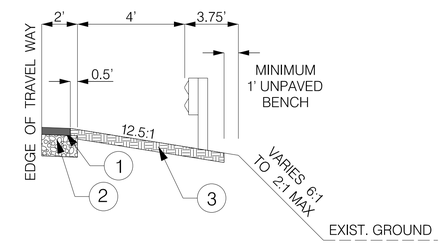
■ VARIABLE – SEE CROSS SECTIONS FOR REQUIRED SLOPE VALUES PROVIDE SMOOTH TRANSITIONS BETWEEN SLOPE CHANGES

6:1 SLOPE 0'–5' FILL HEIGHT
4:1 SLOPE 5'–10' FILL HEIGHT
2:1 MAX SLOPE >10' FILL HEIGHT AND AT BRIDGE ENDS
DITCH SLOPES MAY BE VARIED WHEN A DEEPER DITCH IS NECESSARY FOR DRAINAGE PURPOSES, USING A MINIMUM SLOPE OF 12:1 AND A MAXIMUM SLOPE OF 4:1. WHERE A DEEPER DITCH PROVIDED BY A 4:1 SLOPE IS NECESSARY, THE DITCH SHALL BE PLACED FURTHER FROM THE CENTERLINE CONTINUING THE 4:1 SLOPE TO PROVIDE THE NECESSARY DEPTH.



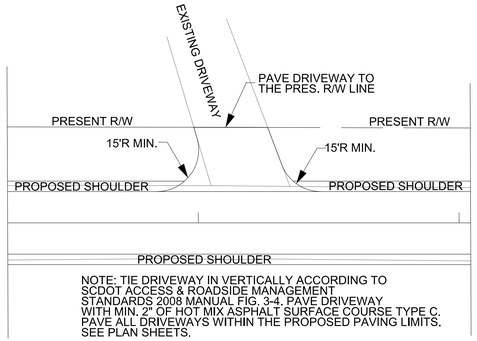
USE THIS SECTION
S-58 (GADDYS MILL RD)
STA. 6+27.89 TO STA. 10+41.78

BRIDGE
(INCLUDING APPROACH SLABS)
STA. 7+78.00 TO STA. 9+68.00



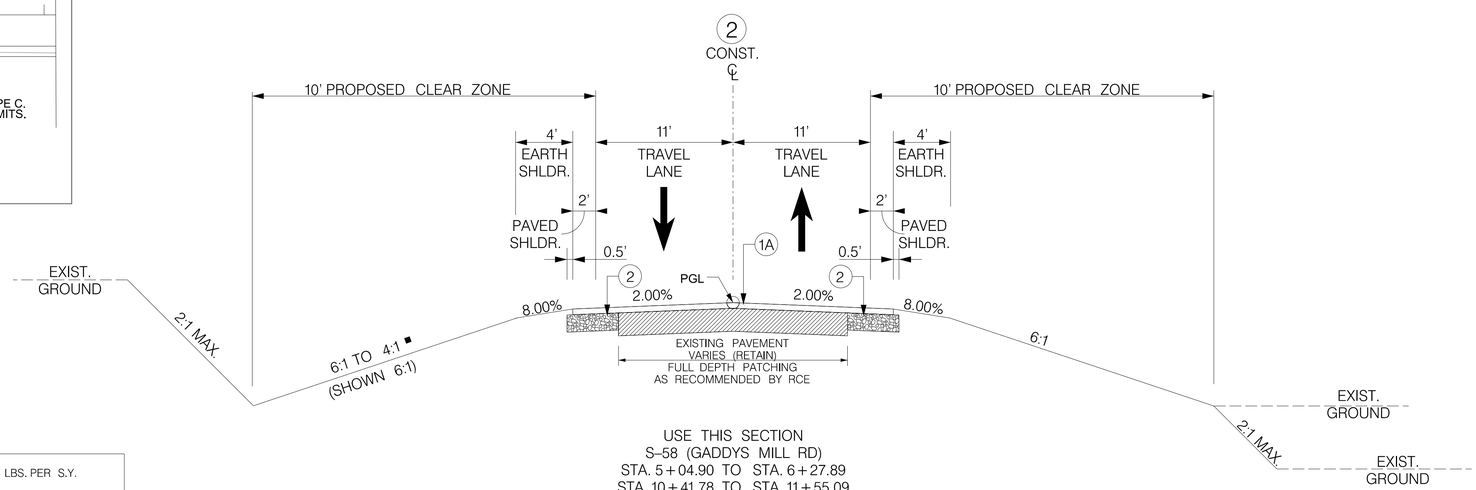
TYPICAL GUARDRAIL SECTION FOR OUTSIDE SHOULDER

SEE PLANS FOR GUARDRAIL LOCATIONS
REFER TO SCDOT STD. DWG. 805-115-10
FOR GRADING SCHEME AT END TREATMENTS



NOTE: TIE DRIVEWAY IN VERTICALLY ACCORDING TO SCDOT ACCESS & ROADSIDE MANAGEMENT STANDARDS 2008 MANUAL FIG. 3-4. PAVE DRIVEWAY WITH MIN. 2" OF HOT MIX ASPHALT SURFACE COURSE TYPE C. PAVE ALL DRIVEWAYS WITHIN THE PROPOSED PAVING LIMITS. SEE PLAN SHEETS.

DRIVEWAY DETAIL
N.T.S.



USE THIS SECTION
S-58 (GADDYS MILL RD)
STA. 5+04.90 TO STA. 6+27.89
STA. 10+41.78 TO STA. 11+55.09

1	HOT MIX ASPHALT SURFACE COURSE TYPE C – 200 LBS. PER S.Y.
1A	HOT MIX ASPHALT SURFACE COURSE TYPE C – 150 LBS. PER S.Y.
2	HOT MIX ASPHALT BASE COURSE TYPE B – 300 LBS. PER S.Y.
3	4" HOT MIX ASPHALT SURFACE COURSE (NON-MOWABLE STRIP)

CROWDER

INFRASTRUCTURE CONSULTING & ENGINEERING

FUNCTIONAL CLASSIFICATION

S-58 (GADDYS MILL RD)
RURAL – LOCAL GROUP 4

RTE. S-58 DESIGN SPEED

MPH	FROM STA.	TO STA.
35	5+04.90	11+55.09

EXCEPTIONS TO DESIGN SPEED

PRELIMINARY
NOT FOR CONSTRUCTION

N.T.S.

REV. NO.	BY	DATE	DESCRIPTION OF REVISION
6			
5			
4			
3			
2			
1			
0			

SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

TYPICAL SECTION

S-58 (GADDYS MILL ROAD)
OVER BEAVERDAM CREEK

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6/12/2025

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	DILLON	P043715	S-58	4

SCDOT
South Carolina Department of Transportation

RIGHT-OF-WAY DATA SHEET

TRACT NO.	PROPERTY OWNER	TAX MAP REFERENCE	TOTAL TRACT ACRES	OBTAIN: OUTFALL DITCH	OBTAIN: LEFT	OBTAIN: RIGHT	OBTAIN: TOTAL	REMAINDER LEFT ACRES A	REMAINDER RIGHT ACRES A	DATE ACQUIRED	TYPE OF INSTRUMENT	PERMISSION (YES): OUTFALL DITCH	PERMISSION (YES): SLOPE	PERMISSION (YES): DRAINAGE STRUCTURE	PERMISSION (YES): EROSION CONTROL	PERMISSION (YES): ENTRANCE CONSTRUCTION	REMARKS
1	COWARD, JOSEPH E ETAL	072-00-00-022	201.44														
2	BLUE MASIONIC AFM LODGE	072-00-00-028	0.00														
3	UNION METHODIST CHURCH	072-00-00-029	1.00														
4	JUSTICE, SANDY MICHELLE AND JOHN	072-00-00-024	352.87		4158 SF (0.0955 AC)	2058 SF (0.0472 AC)	6216 SF (0.1427 AC)	296.818	58.039								
5	TURNER, RICKY E	072-00-00-072	0.00														
6	JUSTICE, SANDY MICHELLE AND JOHN	072-00-00-056	5.00														
7	JUSTICE, SANDY MICHELLE AND JOHN	072-00-00-057	5.60														
8	MCKENZIE, JUSTIN R	072-00-00-065	60.93														
9	YARBOROUGH, FRANCES M	072-00-00-038	2.27														
10	MCKENZIE, JUSTIN R	072-00-00-036	23.00														
11	DUJIC, ARLEEN AMBER	072-00-00-030	1.00														
12	RENFROW, JAMES AND FLORINETTE	072-00-00-045	1.26														

DATE	TRACT NO.	REMARKS

R/W NOTE: THE DEPARTMENT WILL UTILIZE THE PRESENT RIGHT OF WAY AS SHOWN BELOW EXCEPT AS OTHERWISE SHOWN ON PLANS.

ROAD / ROUTE #	FILE #	R/W WIDTH	YEAR ACQ'D.

NOTES:

A. SHOW REMAINDER IN SQUARE FEET WHEN LESS THAN 0.25 ACRE.

6/5/2025
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6/12/2025

CROWDER

INFRASTRUCTURE
CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

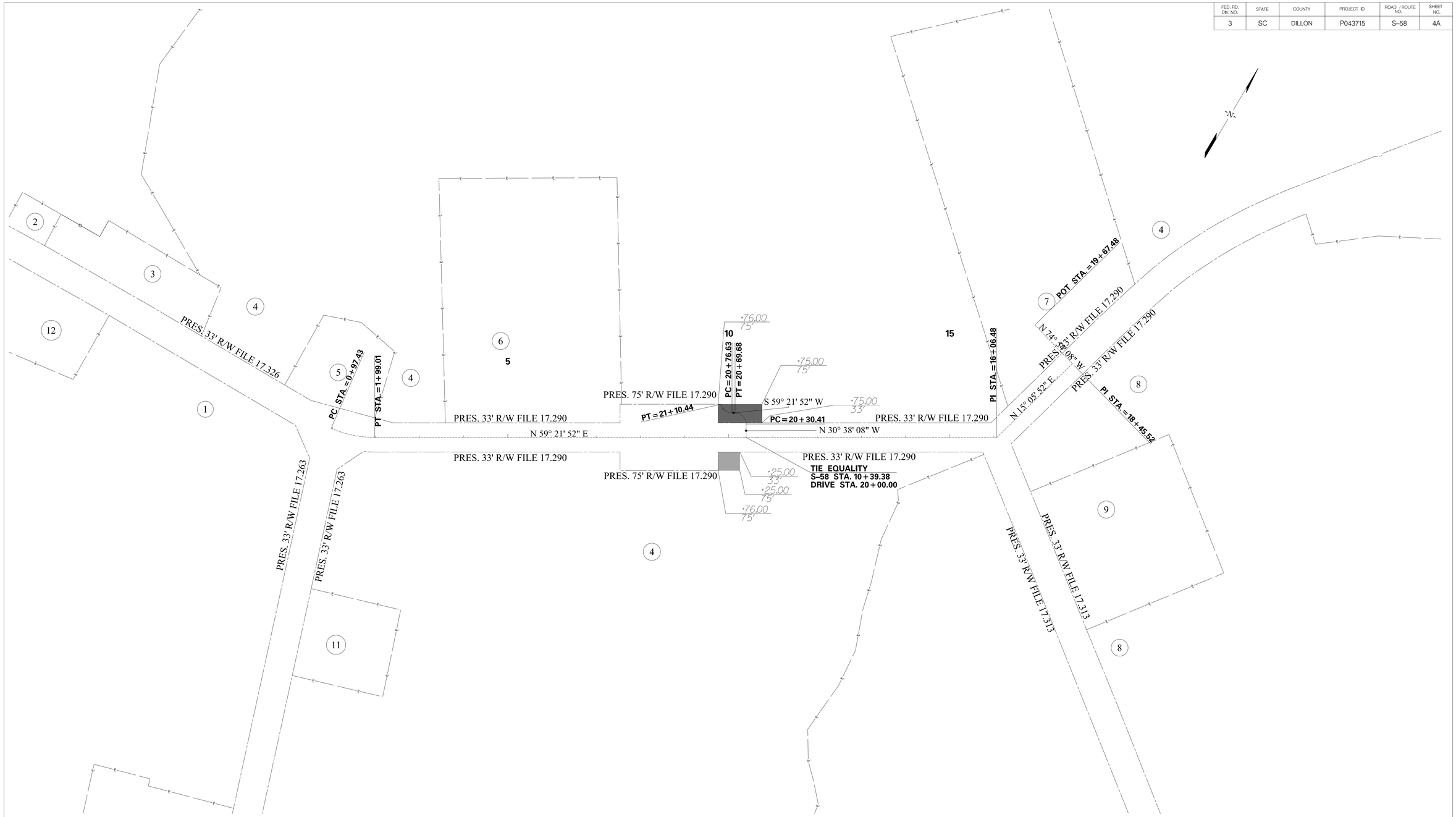
REV. NO.	BY	DATE	DESCRIPTION OF REVISION
6			
5			
4			
3			
2			
1			
0			

SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

RIGHT-OF-WAY DATA SHEET

S-58 (GADDYS MILL ROAD)
OVER BEAVERDAM CREEK

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	DILLON	P043715	S-58	4A



CROWDER

0 50' 100' 200'
SCALE : 1" = 100'

INFRASTRUCTURE
CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

REV. NO.	BY	DATE	DESCRIPTION OF REVISION
6			
5			
4			
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SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

PROPERTY STRIP MAP

S-58 (GADDYS MILL ROAD)
OVER BEAVERDAM CREEK

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6/12/2025

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	DILLON	P043715	S-58	5

SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION

GENERAL CONSTRUCTION NOTE

THE DEPUTY SECRETARY OF ENGINEERING MUST SPECIFICALLY AUTHORIZE CHANGES INVOLVING INCREASED COST OF PROJECT OR CHANGES IN ALIGNMENT. THE DISTRICT ENGINEERING ADMINISTRATOR IS PERMITTED UNDER THE DIRECTION OF THE DEPUTY SECRETARY OF ENGINEERING TO AUTHORIZE MINOR ALTERATIONS NOT IN CONFLICT WITH THE STANDARD PRACTICES OF THE DEPARTMENT. FORWARD INFORMATION ON ANY PROPOSED CHANGES IN ALIGNMENT TO THE COLUMBIA OFFICE AS SOON AS POSSIBLE.

SEE INDIVIDUAL CURVES ON REFERENCE DATA SHEET FOR SUPERELEVATION RATE AND DESIGN SPEED, AS APPLICABLE.

NOTES:

1. THIS PROJECT IS BASED ON A LUMP SUM BASIS
2. SEE FINAL RFP FOR ADDITIONAL REQUIREMENTS

CROWDER

INFRASTRUCTURE CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

REV. NO.	BY	DATE	DESCRIPTION OF REVISION
6			
5			
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SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

GENERAL CONSTRUCTION NOTES

S-58 (GADDYS MILL ROAD)
OVER BEAVERDAM CREEK

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6/12/2025

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD/ROUTE NO.	SHEET NO.
3	SC	DILLON	P043715	S-58	5A

PROPERTY MONUMENTS FOUND

ALIGNMENT	STATION	OFFSET	NORTHING	EASTING	DESCRIPTION
S-156 GADDYS MILL RD	OFF CHAIN		935550.39	2529599.51	IP 5/8"PIPEBENT
S-156 GADDYS MILL RD	OFF CHAIN		935556.62	2529899.50	IP 5/8"PIPEBENT
S-58 GADDYS MILL RD	01+95.94	-34.04	935653.29	2530537.41	IP #4REBAR
S-156 GADDYS MILL RD	198+16.10	-32.88	935623.35	2530321.30	IP #4REBAR BENT

SURVEY CONTROL POINTS

POINT ID	ALIGNMENT	STATION	OFFSET	NORTHING	EASTING	ELEV.	DESCRIPTION
1	-156 GADDYS MILL RD.	OFF CHAIN		935557.3738	2529521.1550	108.88	PSC RB/CAP
2	S-156 GADDYS MILL RD	199+42.71	45.15	935546.3198	2530451.1160	102.55	PSC RB/CAP
3	S-58 GADDYS MILL RD	09+47.80	-13.24	936018.3025	2531194.5800	91.13	CP #5REBAR
4	S-58 GADDYS MILL RD	16+58.50	16.70	936388.4331	2531797.7390	90.54	CP #5REBAR

PROJECT BENCHMARKS

POINT ID	ALIGNMENT	STATION	OFFSET	NORTHING	EASTING	ELEV.	DESCRIPTION
5	S-58 GADDYS MILL RD	08+04.56	-20.62	935951.6597	2531067.5688	92.20	BM 1

NOTES:

1. The alignment Station and Offset are referenced to the existing Survey Centerline.
2. Date of Survey: 12-11-2024

The Property Monuments Found listed on this sheet are assumed to be property corner monuments, field located during the course of this survey. The Department makes no claim that these located monuments are the true position of any property and takes no responsibility for this information being used as such. These monuments are tied to the control of this project in an effort to document and preserve their location in the event they are disturbed or destroyed during the construction of the project.

SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

SURVEY CONTROL DATA

PROJECT DESCRIPTION

S-58

DATUM DESCRIPTION

This GRID Coordinate System developed for this project is based on NAD83(2011)(RTN) South Carolina State Plane Coordinate System. A Combined Scale Factor (CSF) for each Survey Control Point must be computed and applied to horizontal ground distances. Elevations for this project are based on NAVD88 for PSC 2 with an Elevation of 102.55'

SOUTH CAROLINA PROFESSIONAL LAND SURVEYOR No. 26610 MICHAEL WESLEY TODD II

2024.12.16 09:48:17 -05'00'

J:\ToddMW\[P043715.xlsm]SCDS 5A

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	DILLON	P043715	S–58	5B

S–58 (GRADDYS MILL ROAD)

Beginning chain PR58 description
==========

Curve Data

Curve PR58-1
P.I. Station 1+48.94 N 935,599.0904 E 2,530,512.7236
Delta = 23° 29' 29.37" (LT)
Degree = 23° 07' 32.43"
Tangent = 51.5146
Length = 101.5819
Radius = 247.7583
External = 5.2989
Long Chord = 100.8718
Mid. Ord. = 5.1879
P.C. Station 0+97.43 N 935,592.6837 E 2,530,461.6089
P.T. Station 1+99.01 N 935,625.3411 E 2,530,557.0481
C.C. N 935,838.5185 E 2,530,430.7961
Back = N 82° 51' 21.01" E
Ahead = N 59° 21' 51.64" E
Chord Bear = N 71° 06' 36.32" E

Course from PT PR58-1 to 101 N 59° 21' 51.64" E Dist 1,407.4674

Point 101 N 936,342.5540 E 2,531,768.0683 Sta 16+06.48

Course from 101 to 102 N 15° 05' 52.11" E Dist 239.0413

Point 102 N 936,573.3442 E 2,531,830.3308 Sta 18+45.52

Course from 102 to 103 N 74° 54' 07.89" W Dist 121.9568

Point 103 N 936,605.1100 E 2,531,712.5836 Sta 19+67.48

==========
Ending chain PR58 description

DRIVEWAY

Beginning chain S58_DRIVEWAY description
==========

Point 28 N 936,053.5752 E 2,531,280.1251 Sta 0+00.00

Course from 28 to 29 N 30° 38' 07.75" W Dist 30.4117

Point 29 N 936,079.7422 E 2,531,264.6280 Sta 0+30.41

Curve Data

Curve C
P.I. Station 0+55.41 N 936,101.2528 E 2,531,251.8886
Delta = 90° 00' 00.00" (LT)
Degree = 229° 10' 59.22"
Tangent = 25.0000
Length = 39.2699
Radius = 25.0000
External = 10.3553
Long Chord = 35.3553
Mid. Ord. = 7.3223
P.C. Station 0+30.41 N 936,079.7422 E 2,531,264.6280
P.T. Station 0+69.68 N 936,088.5134 E 2,531,230.3780
C.C. N 936,067.0028 E 2,531,243.1174
Back = N 30° 38' 08.36" W
Ahead = S 59° 21' 51.64" W
Chord Bear = N 75° 38' 08.36" W

Point 32 N 936,088.5135 E 2,531,230.3779 Sta 0+69.68

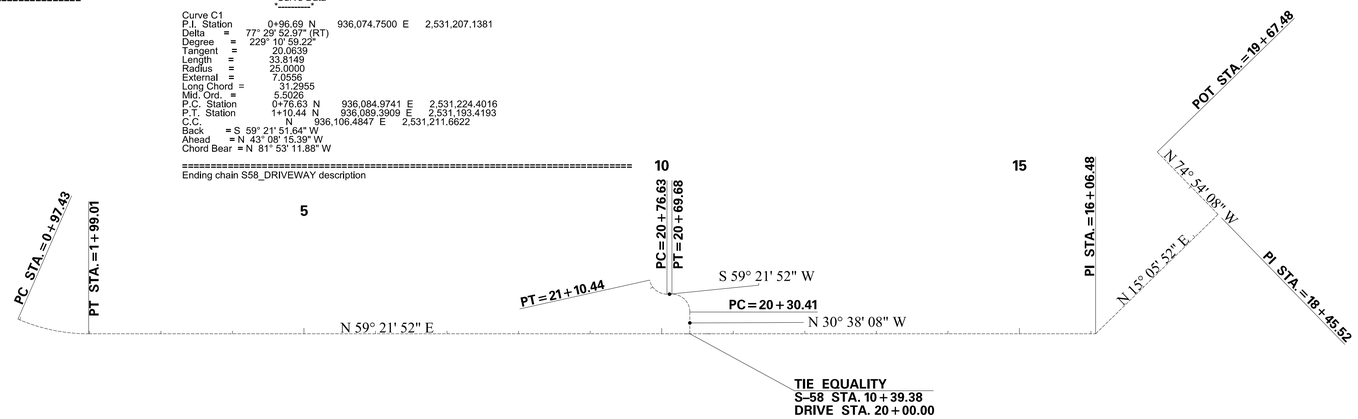
Course from 32 to 33 S 59° 21' 51.64" W Dist 6.9458

Point 33 N 936,084.9741 E 2,531,224.4016 Sta 0+76.63

Curve Data

Curve C1
P.I. Station 0+96.69 N 936,074.7500 E 2,531,207.1381
Delta = 77° 29' 52.97" (RT)
Degree = 229° 10' 59.22"
Tangent = 20.0639
Length = 33.8149
Radius = 25.0000
External = 7.0556
Long Chord = 31.2955
Mid. Ord. = 5.5026
P.C. Station 0+76.63 N 936,084.9741 E 2,531,224.4016
P.T. Station 1+10.44 N 936,089.3909 E 2,531,193.4193
C.C. N 936,106.4847 E 2,531,211.6622
Back = S 59° 21' 51.64" W
Ahead = N 43° 08' 15.39" W
Chord Bear = N 81° 53' 11.88" W

==========
Ending chain S58_DRIVEWAY description



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6/12/2025

CROWDER

0 50' 100' 200'
SCALE : 1" = 100'

INFRASTRUCTURE
CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

REV. NO.	BY	DATE	DESCRIPTION OF REVISION
6			
5			
4			
3			
2			
1			
0			

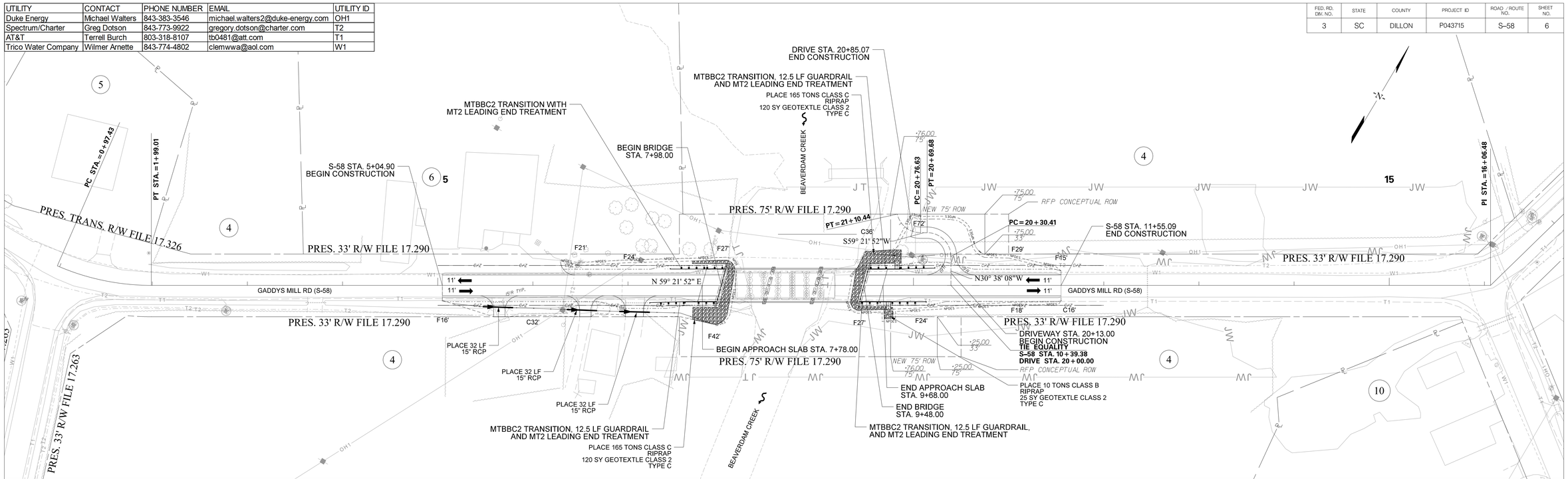
SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

REFERENCE DATA SHEET

S–58 (GADDYS MILL ROAD)
OVER BEAVERDAM CREEK

UTILITY	CONTACT	PHONE NUMBER	EMAIL	UTILITY ID
Duke Energy	Michael Walters	843-383-3546	michael.walters2@duke-energy.com	OH1
Spectrum/Charter	Greg Dotson	843-773-9922	gregory.dotson@charter.com	T2
AT&T	Terrell Burch	803-318-8107	tb0481@att.com	T1
Trico Water Company	Wilmer Arnette	843-774-4802	clemwwa@aol.com	W1

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD / ROUTE NO.	SHEET NO.
3	SC	DILLON	P043715	S-58	6



CROWDER

0 25' 50' 100'
HORIZONTAL SCALE: 1" = 50'
VERTICAL SCALE: 1" = 10'

ALIGNMENT CONTROL CAN BE FOUND ON REFERENCE DATA SHEET 5A & 5B

INFRASTRUCTURE CONSULTING & ENGINEERING

PRELIMINARY
NOT FOR CONSTRUCTION

REV. NO.	BY	DATE	DESCRIPTION OF REVISION
6			
5			
4			
3			
2			
1			
0			

SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE SHEET

S-58 (GADDYS MILL ROAD)
OVER BEAVERDAM CREEK

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6/12/2025

A.2 CONCEPTUAL BRIDGE PLANS

BRIDGE PLANS ID	SHEET
P5570770 - B01	1

INDEX OF SHEETS

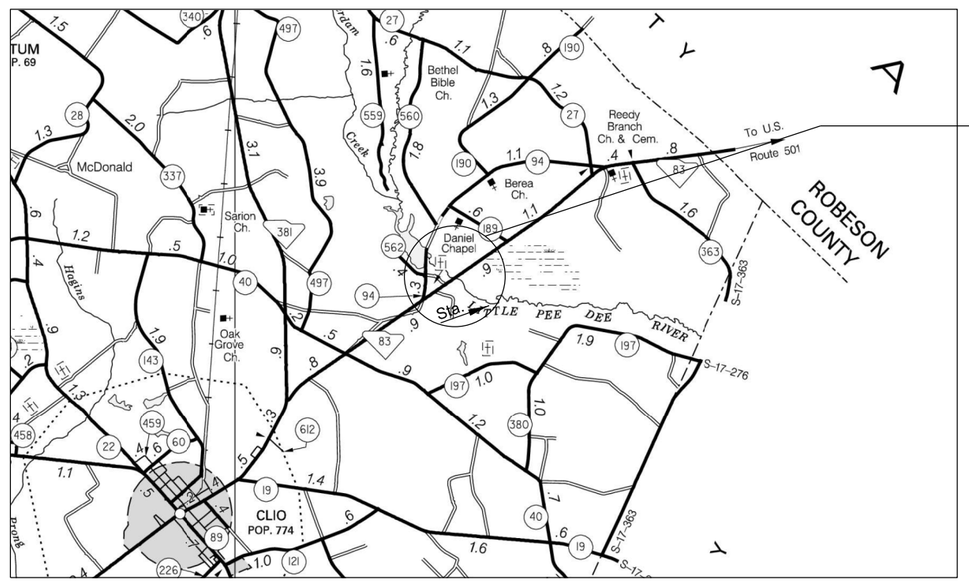
1. TITLE SHEET
2. BRIDGE PLAN AND PROFILE
3. TYPICAL SECTION (1)
4. TYPICAL SECTION (2)

SCDOT

South Carolina Department of Transportation

PROPOSED PLANS FOR MARLBORO COUNTY PROJECT ID P5570770 ROUTE SC 83 REPLACE BRIDGE OVER LITTLE PEE DEE RIVER

-N-



SITE LOCATION

Approximate Location of Bridge is	
Latitude	34°-36'-40" N
Longitude	79°-30'-04" W

LAYOUT

NET LENGTH OF ROADWAY	0.000	MILES
NET LENGTH OF BRIDGES	0.079	MILES
NET LENGTH OF PROJECT	0.079	MILES
LENGTH OF EXCEPTIONS	0.000	MILES
GROSS LENGTH OF PROJECT	0.079	MILES

NOTE: EXCEPT AS MAY OTHERWISE BE SPECIFIED ON THE PLANS OR IN THE SPECIAL PROVISIONS, ALL MATERIALS AND WORKMANSHIP ON THIS PROJECT SHALL CONFORM TO THE SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION AND THE STANDARD DRAWINGS FOR ROAD CONSTRUCTION IN EFFECT AT THE TIME OF THE RELEASE OF THE FINAL RFP.

3 DAYS BEFORE DIGGING IN SOUTH CAROLINA

CALL 811

SOUTH CAROLINA 811 (SC811)
WWW.SC811.COM
ALL UTILITIES MAY NOT BE A MEMBER OF SC811

ASSET ID TBD

TRAFFIC DATA

2026 ADT 570 V.P.D.

2046 ADT 700 V.P.D.

TRUCKS 12 %

CROWDER

PLANS PREPARED BY:

INFRASTRUCTURE CONSULTING & ENGINEERING

110 MIDLANDS COURT
WEST COLUMBIA, SC 29169
Telephone: (803) 822-0333

ENGINEER OF RECORD

BRIDGE 1
CONCEPTUAL PLANS

FOR CONSTRUCTION : _____ _____ DATE

REVIEWED P. COBLENTZ			
DR.	ADG	CSB	05-25
	BY	CHK.	DATE

$$PRINT_PROPERTIES$$

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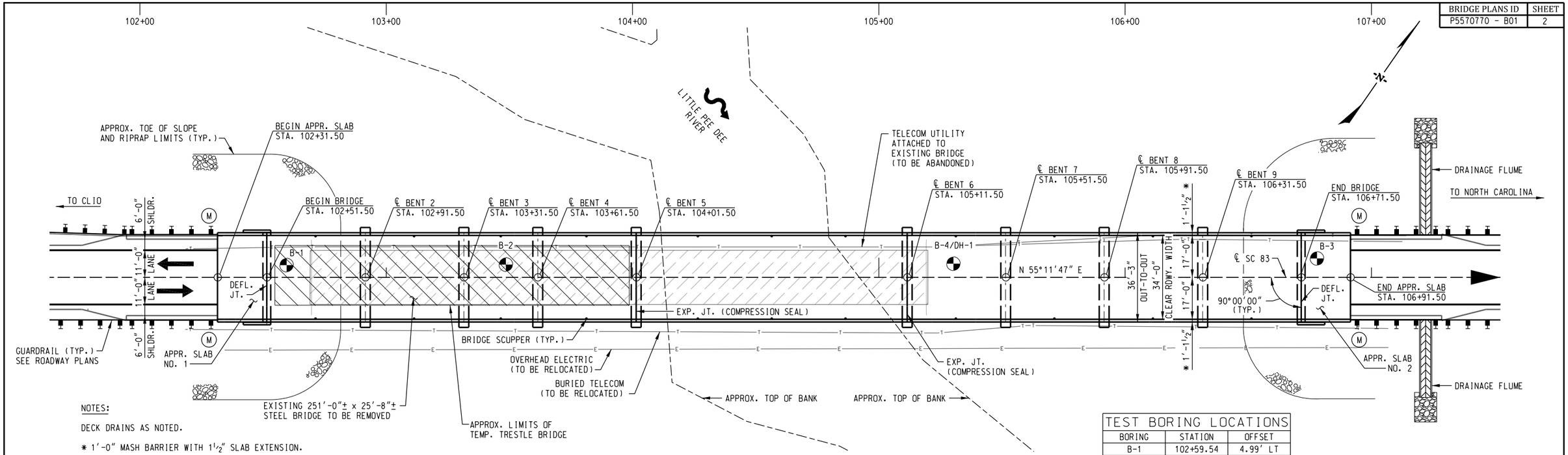
6/12/2025

BRIDGE PLANS ID	SHEET
P5570770 - B01	2



NOTES:

DECK DRAINS AS NOTED.

* 1'-0" MASH BARRIER WITH 1½" SLAB EXTENSION.

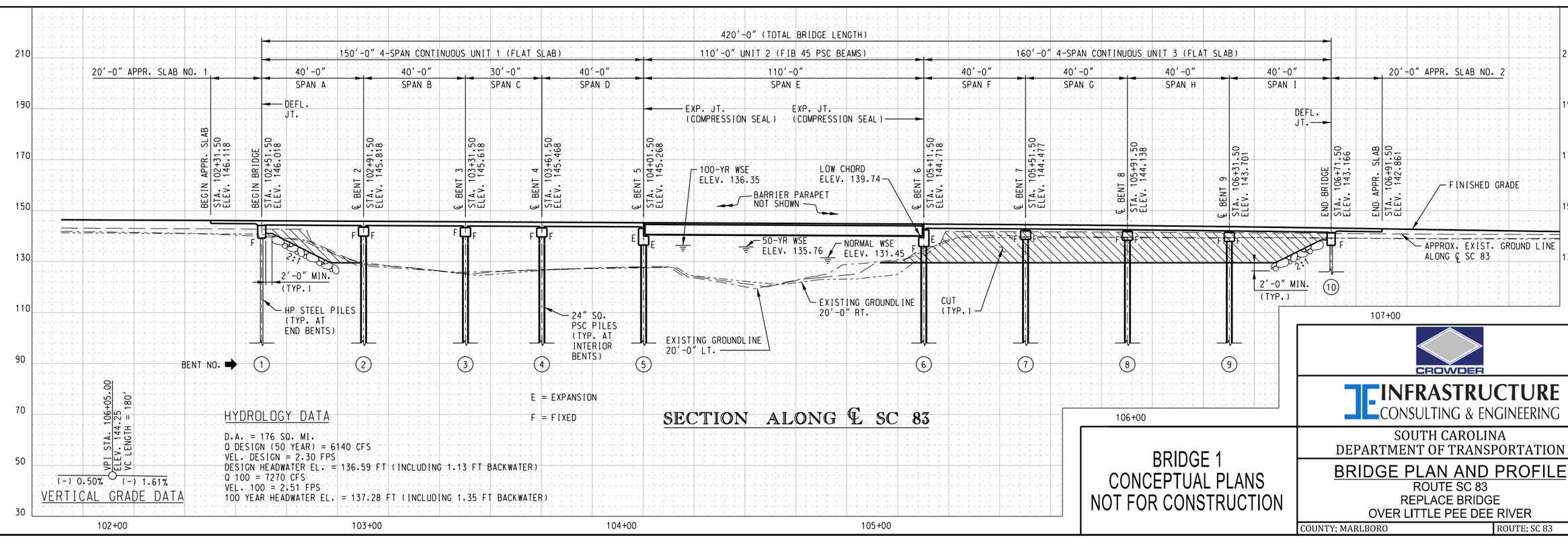
EXISTING BRIDGE TO BE REMOVED AND DISPOSED OF BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 202.3.2 OF THE STANDARD SPECIFICATIONS.

(M) DENOTES MTBBC3 GUARDRAIL ATTACHMENT.

PLAN

TEST BORING LOCATIONS		
BORING	STATION	OFFSET
B-1	102+59.54	4.99' LT
B-2	103+48.59	5.20' LT
B-3	106+77.91	7.67' LT
B-4/DH-1	105+30.29	5.55' LT

NOTE: BORING STATIONS AND OFFSETS ARE REFERENCED FROM ℄ SC 83.



SECTION ALONG ℄ SC 83

HYDROLOGY DATA

D.A. = 176 SQ. MI.
Q DESIGN (50 YEAR) = 6140 CFS
VEL. DESIGN = 2.30 FPS
DESIGN HEADWATER EL. = 136.59 FT (INCLUDING 1.13 FT BACKWATER)
Q 100 = 7270 CFS
VEL. 100 = 2.51 FPS
100 YEAR HEADWATER EL. = 137.28 FT (INCLUDING 1.35 FT BACKWATER)

BRIDGE 1
CONCEPTUAL PLANS
NOT FOR CONSTRUCTION

CROWDER
INFRASTRUCTURE CONSULTING & ENGINEERING

SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION

BRIDGE PLAN AND PROFILE
ROUTE SC 83
REPLACE BRIDGE
OVER LITTLE PEE DEE RIVER

COUNTY: MARLBORO | ROUTE: SC 83

REVIEWED P.COBLENTZ			
QUAN.			
DR.	ADG	CSB	05-25
DES.	CSB	RWS	05-25
	BY	CHK.	DATE

$$PRINT_PROPERTIES$$

REV.				
REV.				
REV.				
	BY	CHK.	DATE	DESCRIPTION OF REVISION
			PREVIOUS REVISION	
			LATEST REVISION	

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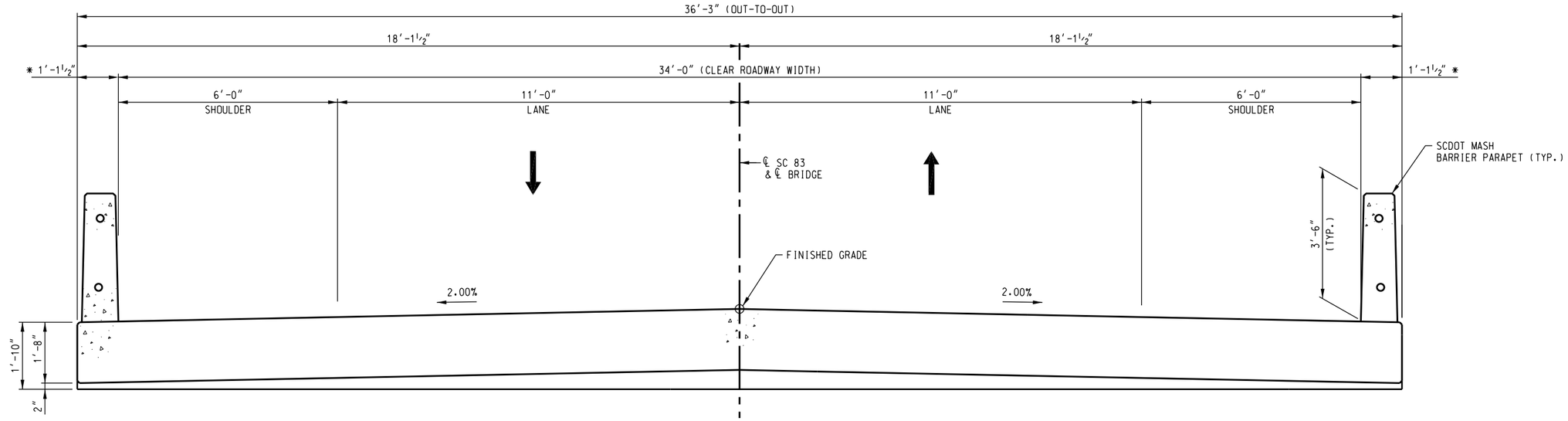
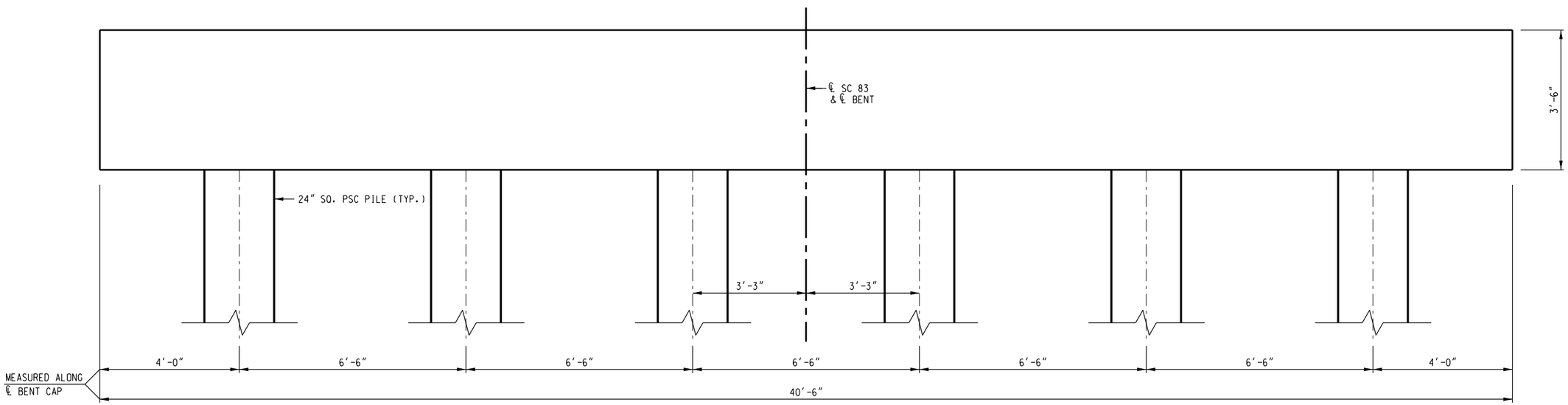
6/12/2025

BRIDGE PLANS ID	SHEET
P5570770 - B01	3





NOTES:

* 1'-0" MASH BARRIER PARAPET W/ 1½" SLAB EXTENSION

DECK DRAINS NOT SHOWN FOR CLARITY. SEE BRIDGE PLAN AND PROFILE SHEET.

TYPICAL SECTION

(UNITS 1 AND 3)
LOOKING IN DIRECTION OF STATIONING

BRIDGE 1
CONCEPTUAL PLANS
NOT FOR CONSTRUCTION

CROWDER
INFRASTRUCTURE CONSULTING & ENGINEERING

SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTION (1)
ROUTE SC 83
REPLACE BRIDGE
OVER LITTLE PEE DEE RIVER

COUNTY: MARLBORO | ROUTE: SC 83

REVIEWED P. COBLENTZ			
QUAN.	—	—	—
DR.	ADG	CSB	05-25
DES.	—	—	—
	BY	CHK.	DATE

$$PRINT_PROPERTIES$$

REV.	BY	CHK.	DATE	DESCRIPTION OF REVISION
REV.				
REV.				

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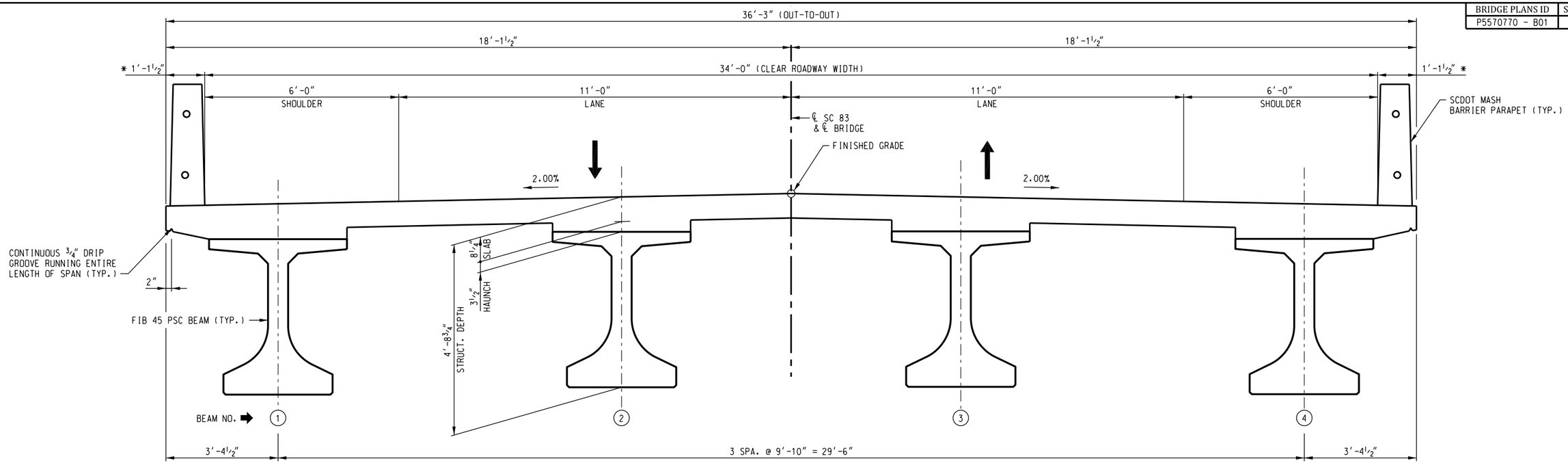
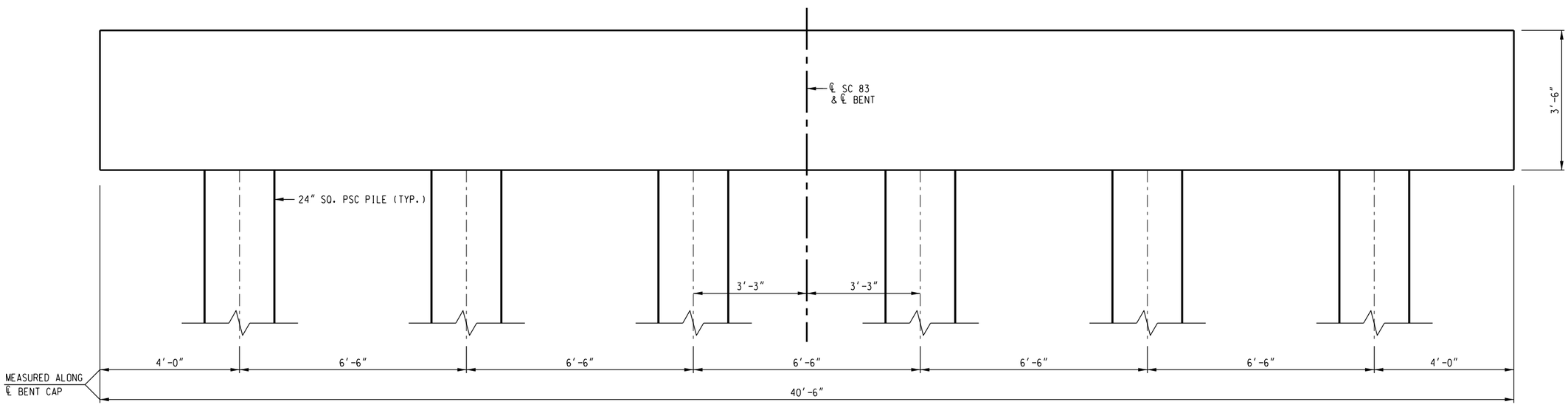
6/12/2025

BRIDGE PLANS ID	SHEET
P5570770 - B01	4





TYPICAL SECTION

(UNIT 2)
LOOKING IN DIRECTION OF STATIONING

NOTES:

* 1'-0" MASH BARRIER PARAPET W/ 1½" SLAB EXTENSION

DECK DRAINS NOT SHOWN FOR CLARITY. SEE BRIDGE PLAN AND PROFILE SHEET.

SHEAR KEYS SHALL BE PROVIDED (NOT SHOWN FOR CLARITY).

BRIDGE 1
CONCEPTUAL PLANS
NOT FOR CONSTRUCTION

CROWDER
INFRASTRUCTURE CONSULTING & ENGINEERING

SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTION (2)
ROUTE SC 83
REPLACE BRIDGE
OVER LITTLE PEE DEE RIVER

COUNTY: MARLBORO | ROUTE: SC 83

REVIEWED	P. COBLENTZ	
QUAN.	—	—
DR.	ADG	CSB
DES.	—	—
BY	CHK.	DATE

$$PRINT_PROPERTIES$$

REV.				
REV.				
REV.				
	BY	CHK.	DATE	DESCRIPTION OF REVISION

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6/12/2025

BRIDGE PLANS ID	SHEET
P5570770 - B02	1

INDEX OF SHEETS

1. TITLE SHEET
2. BRIDGE PLAN AND PROFILE
3. TYPICAL SECTION

SCDOT
South Carolina Department of Transportation

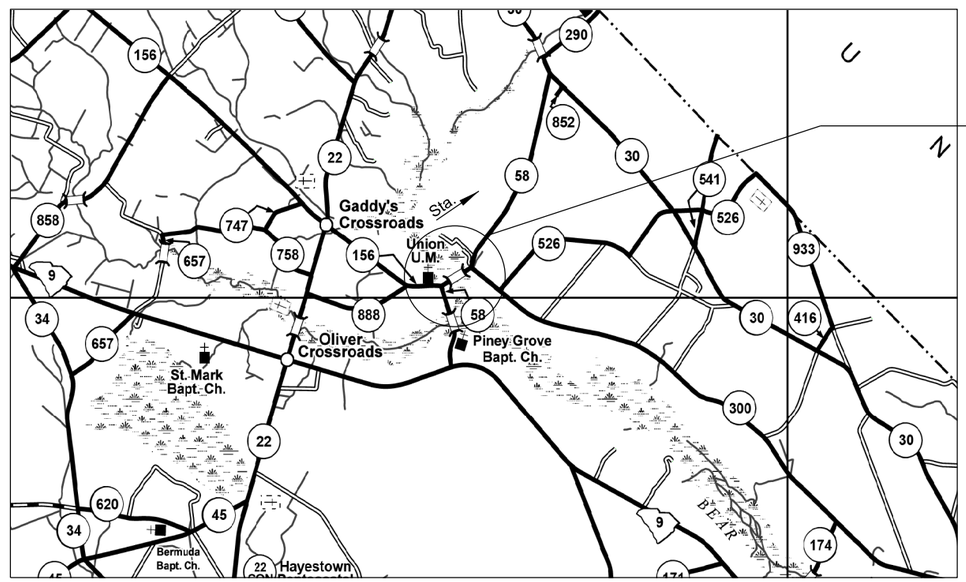
DESIGN REFERENCE FOR THESE PLANS IS THE:

LVB

SUPPLEMENTAL DESIGN CRITERIA FOR
LOW VOLUME BRIDGE REPLACEMENT PROJECTS

PROPOSED PLANS
FOR
DILLON COUNTY
PROJECT ID P5570770
ROUTE S-58 (GADDYS MILL ROAD)
REPLACE BRIDGE OVER BEAVERDAM CREEK

-N-



SITE LOCATION

Approximate Location of Bridge is	
Latitude	34°-23'-36" N
Longitude	79°-14'-21" W

LAYOUT

NET LENGTH OF ROADWAY	0.000	MILES
NET LENGTH OF BRIDGES	0.028	MILES
NET LENGTH OF PROJECT	0.028	MILES
LENGTH OF EXCEPTIONS	0.000	MILES
GROSS LENGTH OF PROJECT	0.028	MILES

NOTE: EXCEPT AS MAY OTHERWISE BE SPECIFIED ON THE PLANS OR IN THE SPECIAL PROVISIONS, ALL MATERIALS AND WORKMANSHIP ON THIS PROJECT SHALL CONFORM TO THE SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION AND THE STANDARD DRAWINGS FOR ROAD CONSTRUCTION IN EFFECT AT THE TIME OF THE RELEASE OF THE FINAL RFP.

3 DAYS BEFORE DIGGING IN
SOUTH CAROLINA

CALL 811

SOUTH CAROLINA 811 (SC811)
WWW.SC811.COM
ALL UTILITIES MAY NOT BE A MEMBER OF SC811

ASSET ID TBD

TRAFFIC DATA

2026 ADT 80 V.P.D.

2046 ADT 160 V.P.D.

TRUCKS 9 %

CROWDER

PLANS PREPARED BY:

INFRASTRUCTURE CONSULTING & ENGINEERING

110 MIDLANDS COURT
WEST COLUMBIA, SC 29169
Telephone: (803) 822-0333

ENGINEER OF RECORD

BRIDGE 2
CONCEPTUAL PLANS

FOR CONSTRUCTION : _____ _____ DATE

REVIEWED P. COBLENTZ			
DR.	ADG	CSB	06-25
	BY	CHK.	DATE

$$PRINT_PROPERTIES$$

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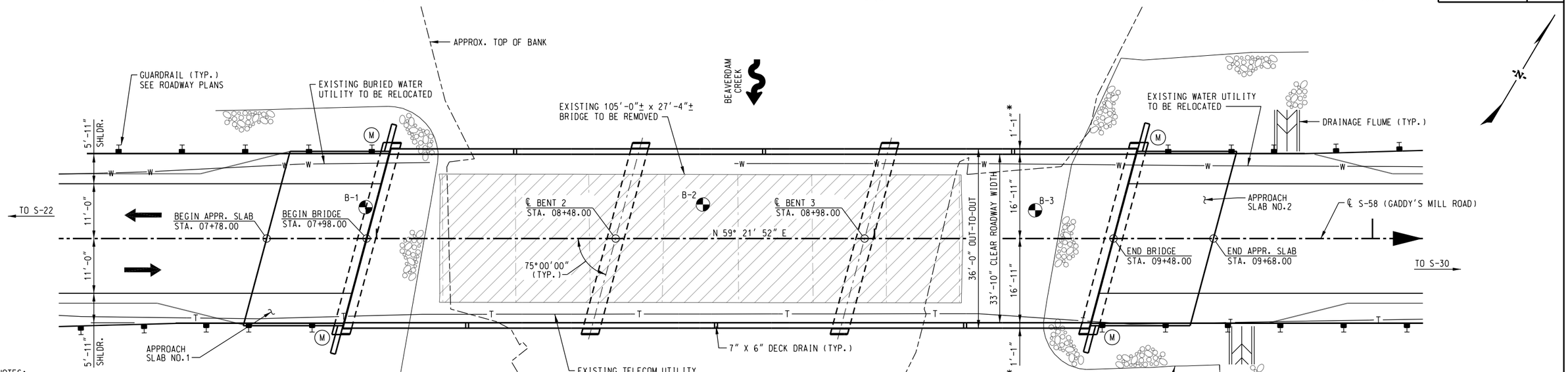
6/12/2025

BRIDGE PLANS ID	SHEET
P5570770 - B02	2



NOTES:

DECK DRAINS AS NOTED.

* 1'-0" MASH BARRIER WITH 1" SLAB EXTENSION.

** PERPENDICULAR TO BENT.

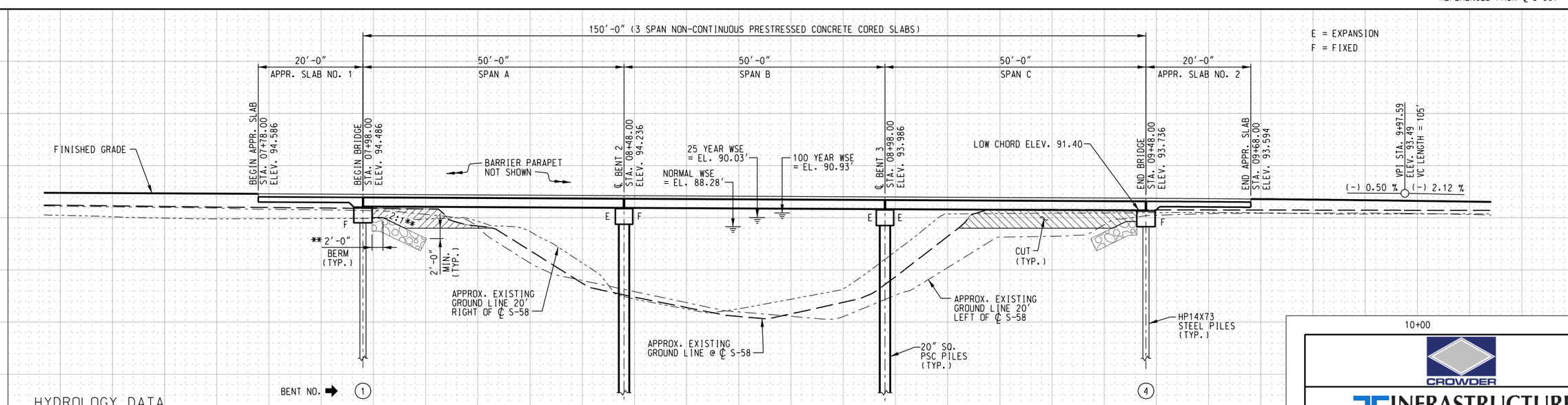
EXISTING BRIDGE TO BE REMOVED AND DISPOSED OF BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 202.3.2 OF THE STANDARD SPECIFICATIONS.

(M) DENOTES MTBBC2 GUARDRAIL ATTACHMENT.

PLAN

TEST BORING LOCATIONS		
BORING	STATION	OFFSET
B-1	07+97.80	6.28' LT
B-2	08+65.54	6.84' LT
B-3	09+32.26	5.68' LT

NOTE: BORING STATIONS AND OFFSETS ARE REFERENCED FROM ℄ S-58.



HYDROLOGY DATA

D.A. = 16.2 SQ. MI.
Q DESIGN (25 YEAR) = 1,530 CFS
VEL. DESIGN = 2.23 FPS
DESIGN W.S.E. = 90.03 FT
DESIGN HEADWATER EL. = 90.14 FT (INCLUDING 0.32 FT BACKWATER)
Q 100 = 2,350 CFS
VEL. 100 = 2.89FPS
100 YEAR W.S.E. = 90.93 FT
100 YEAR HEADWATER EL. = 91.11 FT (INCLUDING 0.53 FT BACKWATER)

SECTION ALONG ℄ S-58

BRIDGE 2
CONCEPTUAL PLANS
NOT FOR CONSTRUCTION

CROWDER
INFRASTRUCTURE
CONSULTING & ENGINEERING

SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION

BRIDGE PLAN AND PROFILE

ROUTE S-58 (GADDYS MILL ROAD)
REPLACE BRIDGE OVER BEAVERDAM CREEK

COUNTY: DILLON | ROUTE: S-58

REVIEWED	P. COBLENTZ		
QUAN.			
DR.	WLH	CSB	06-25
DES.	DPC	ALP	05-25
	BY	CHK.	DATE

REV.			
REV.			
REV.			
	BY	CHK.	DATE	DESCRIPTION OF REVISION
				LATEST REVISION
				PREVIOUS REVISION

$$PRINT_PROPERTIES$$

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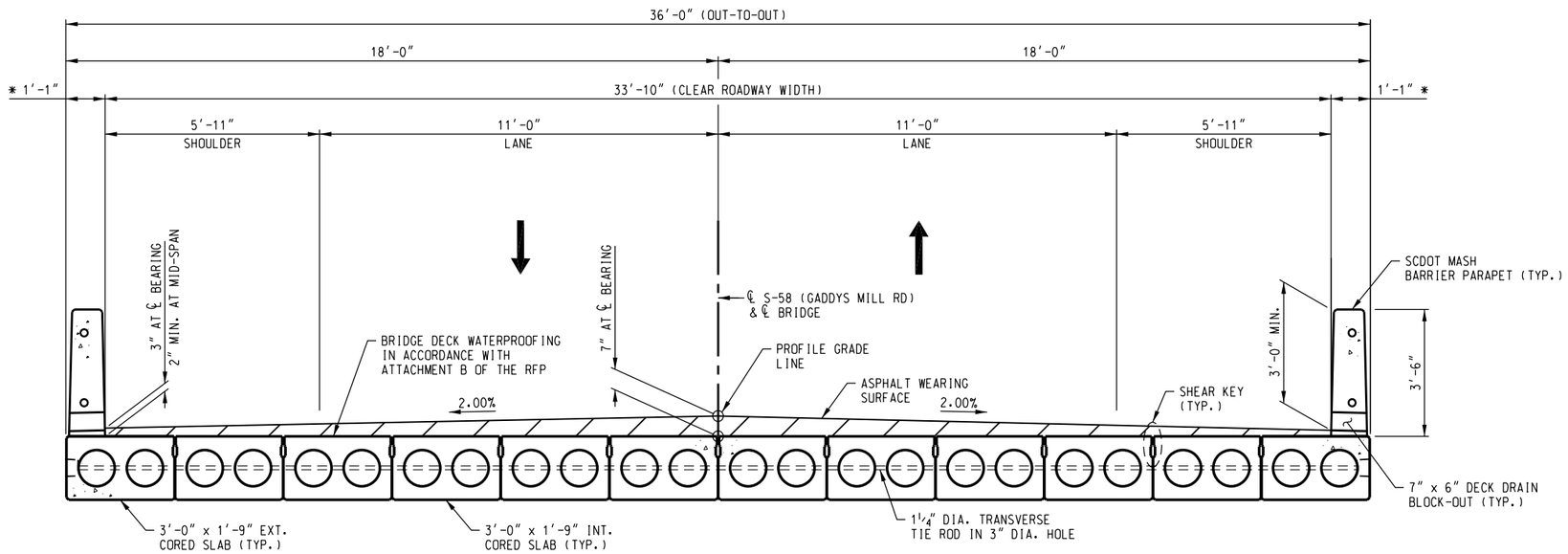
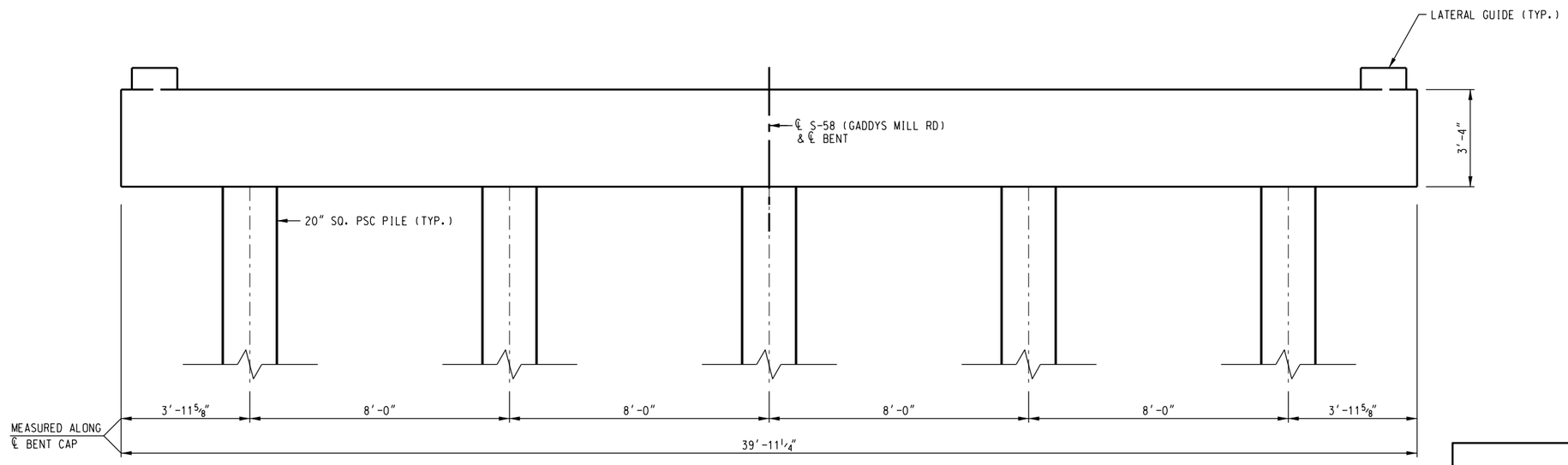
6/12/2025

BRIDGE PLANS ID	SHEET
P5570770 - B02	3





NOTES:

* 1'-0" MASH BARRIER PARAPET W/ 1" SLAB EXTENSION

DECK DRAINS NOT SHOWN FOR CLARITY. SEE BRIDGE PLAN AND PROFILE SHEET.

TYPICAL SECTION

(LOOKING IN DIRECTION OF STATIONING)

BRIDGE 2
CONCEPTUAL PLANS
NOT FOR CONSTRUCTION

CROWDER
INFRASTRUCTURE CONSULTING & ENGINEERING

SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTION

ROUTE S-58 (GADDYS MILL ROAD)
REPLACE BRIDGE OVER BEAVERDAM CREEK

COUNTY: DILLON | ROUTE: S-58

REVIEWED P. COBLENTZ			
QUAN.	—	—	—
DR.	ADG	CSB	06-25
DES.	DPC	CSB	05-25
	BY	CHK.	DATE

$$PRINT_PROPERTIES$$

REV.	BY	CHK.	DATE	DESCRIPTION OF REVISION
REV.				
REV.				

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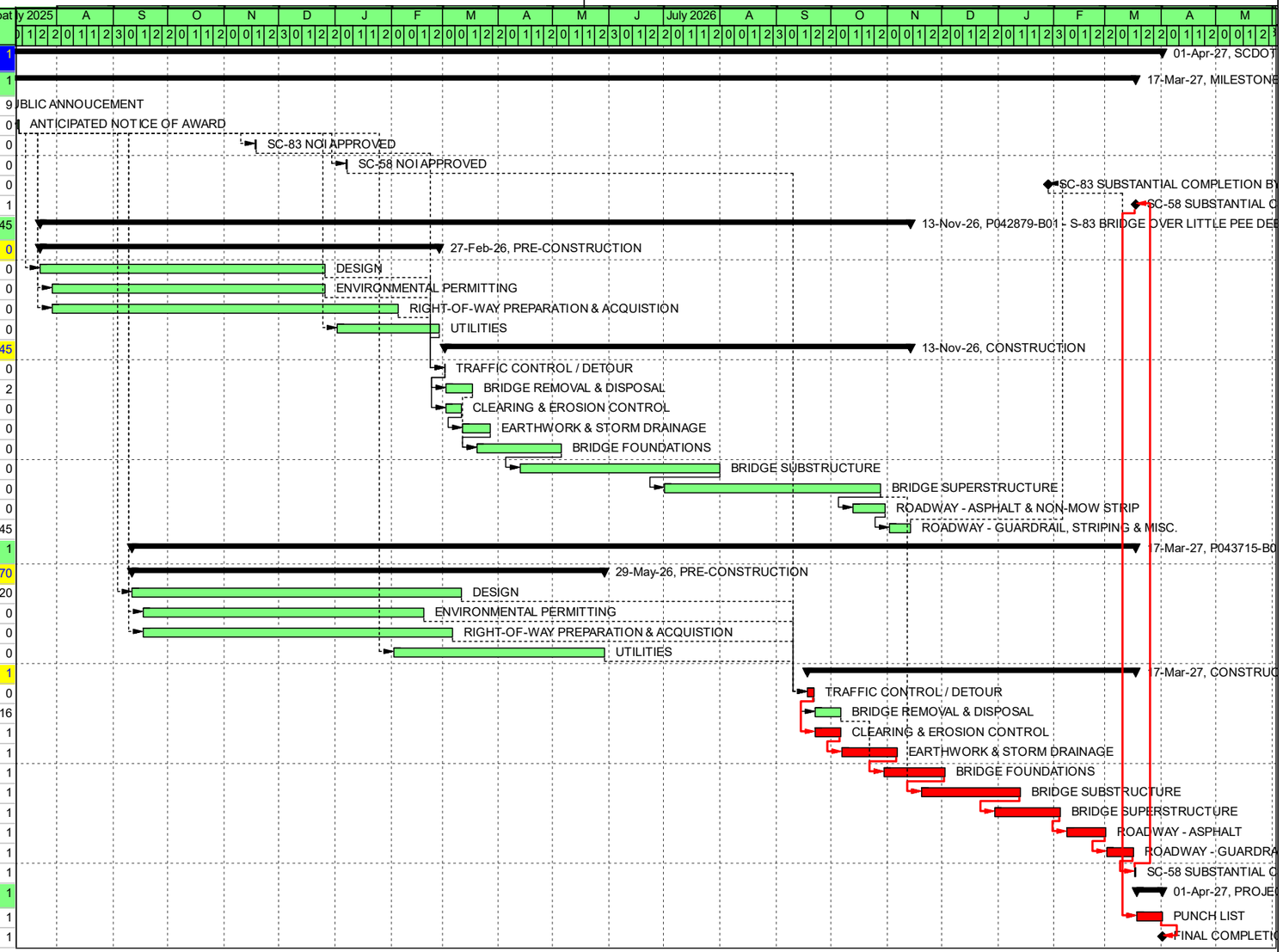
6/12/2025

A.3 CPM SCHEDULE

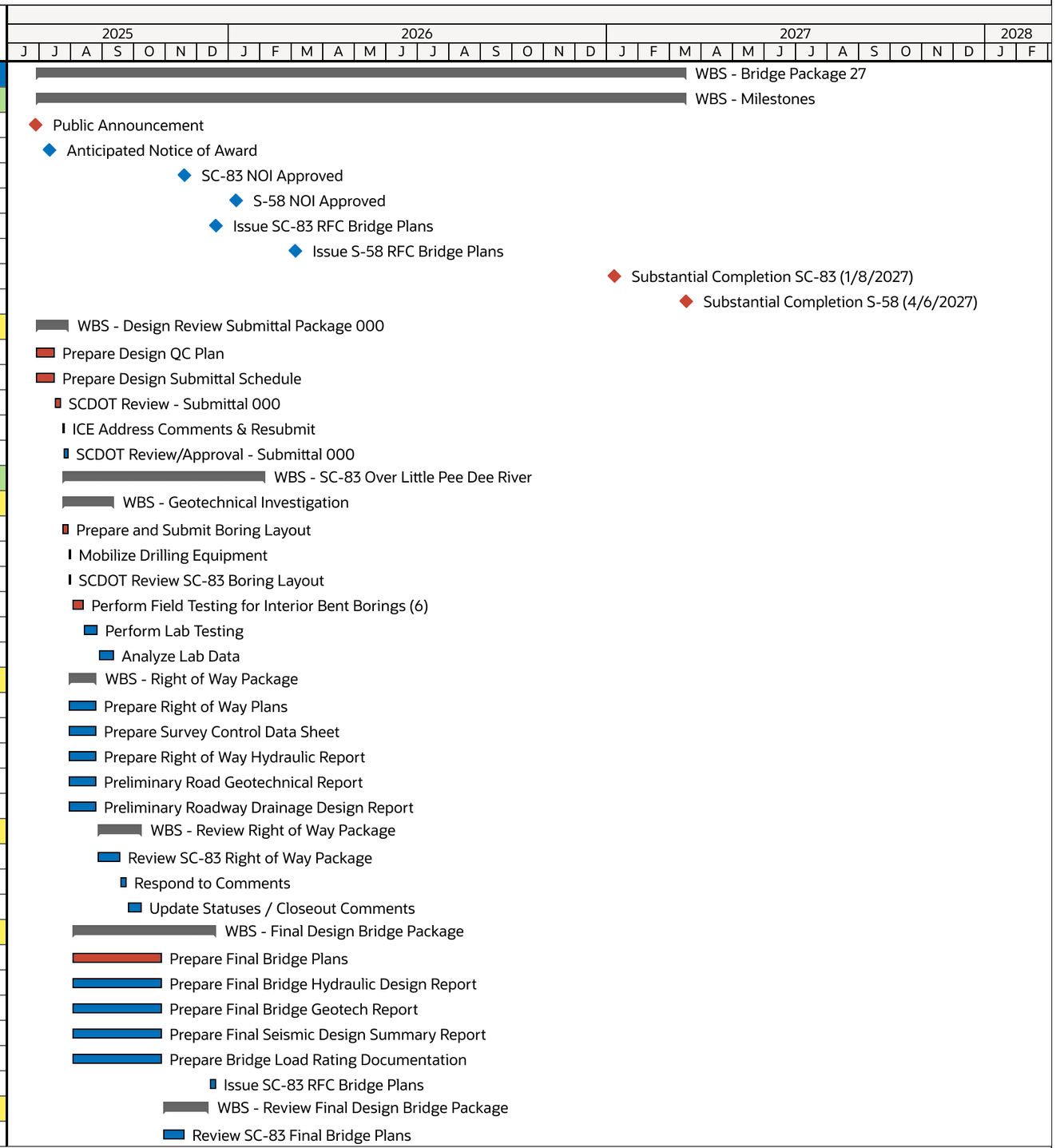
Activity ID	Activity Name	Original Duration	Start	Finish	Total Float
SCDOT Primary and Secondary Route Design-Build P		404	26-Jun-25	01-Apr-27	1
MILESTONES		394	26-Jun-25	17-Mar-27	1
A1000	PUBLIC ANNOUCEMENT	0	26-Jun-25		9
A1010	ANTICIPATED NOTICE OF AWARD	1	10-Jul-25*	10-Jul-25	0
A1020	SC-83 NOI APPROVED	1	18-Nov-25*	18-Nov-25	0
A1030	SC-58 NOI APPROVED	1	07-Jan-26*	07-Jan-26	0
A1260	SC-83 SUBSTANTIAL COMPLETION BY 1/28/2027	0		28-Jan-27*	0
A1270	SC-58 SUBSTANTIAL COMPLETION	0		17-Mar-27	1
P042879-B01 - S-83 BRIDGE OVER LITTLE PEE DEE RIVE		304	22-Jul-25	13-Nov-26	45
PRE-CONSTRUCTION		139	22-Jul-25	27-Feb-26	0
A1080	DESIGN	100	22-Jul-25*	26-Dec-25	0
A1090	ENVIRONMENTAL PERMITTING	95	29-Jul-25*	26-Dec-25	0
A1100	RIGHT-OF-WAY PREPARATION & ACQUISTION	119	29-Jul-25*	04-Feb-26	0
A1110	UTILITIES	37	02-Jan-26*	27-Feb-26	0
CONSTRUCTION		165	02-Mar-26	13-Nov-26	45
A1300	TRAFFIC CONTROL / DETOUR	1	02-Mar-26	02-Mar-26	0
A1170	BRIDGE REMOVAL & DISPOSAL	10	03-Mar-26	17-Mar-26	2
A1320	CLEARING & EROSION CONTROL	7	03-Mar-26	11-Mar-26	0
A1330	EARTHWORK & STORM DRAINAGE	10	12-Mar-26	27-Mar-26	0
A1160	BRIDGE FOUNDATIONS	30	20-Mar-26	05-May-26	0
A1180	BRIDGE SUBSTRUCTURE	70	13-Apr-26	31-Jul-26	0
A1190	BRIDGE SUPERSTRUCTURE	76	01-Jul-26	28-Oct-26	0
A1200	ROADWAY - ASPHALT & NON-MOW STRIP	12	13-Oct-26	30-Oct-26	0
A1280	ROADWAY - GUARDRAIL, STRIPING & MISC.	9	02-Nov-26	13-Nov-26	45
P043715-B01 - S-58 BRIDGE OVER BEAVERDAM CREEK		345	11-Sep-25	17-Mar-27	1
PRE-CONSTRUCTION		163	11-Sep-25	29-May-26	70
A1120	DESIGN	113	11-Sep-25*	11-Mar-26	120
A1130	ENVIRONMENTAL PERMITTING	96	17-Sep-25*	18-Feb-26	0
A1140	RIGHT-OF-WAY PREPARATION & ACQUISTION	107	17-Sep-25*	06-Mar-26	0
A1150	UTILITIES	76	02-Feb-26*	29-May-26	0
CONSTRUCTION - 180 DAYS FOR CONSTRUCTION TIME		112	18-Sep-26	17-Mar-27	1
A1310	TRAFFIC CONTROL / DETOUR	2	18-Sep-26*	21-Sep-26	0
A1220	BRIDGE REMOVAL & DISPOSAL	10	22-Sep-26	06-Oct-26	16
A1340	CLEARING & EROSION CONTROL	10	22-Sep-26	06-Oct-26	1
A1350	EARTHWORK & STORM DRAINAGE	20	07-Oct-26	06-Nov-26	1
A1210	BRIDGE FOUNDATIONS	20	30-Oct-26	02-Dec-26	1
A1230	BRIDGE SUBSTRUCTURE	31	20-Nov-26	13-Jan-27	1
A1240	BRIDGE SUPERSTRUCTURE	24	30-Dec-26	04-Feb-27	1
A1250	ROADWAY - ASPHALT	15	08-Feb-27	01-Mar-27	1
A1290	ROADWAY - GUARDRAIL, NON-MOW STRIP, STRIPING & MIS	10	02-Mar-27	16-Mar-27	1
A1050	SC-58 SUBSTANTIAL COMPLETION	1	17-Mar-27	17-Mar-27	1
PROJECT COMPLETION		10	18-Mar-27	01-Apr-27	1
A1060	PUNCH LIST	10	18-Mar-27	01-Apr-27	1
A1070	FINAL COMPLETION	0		01-Apr-27	1



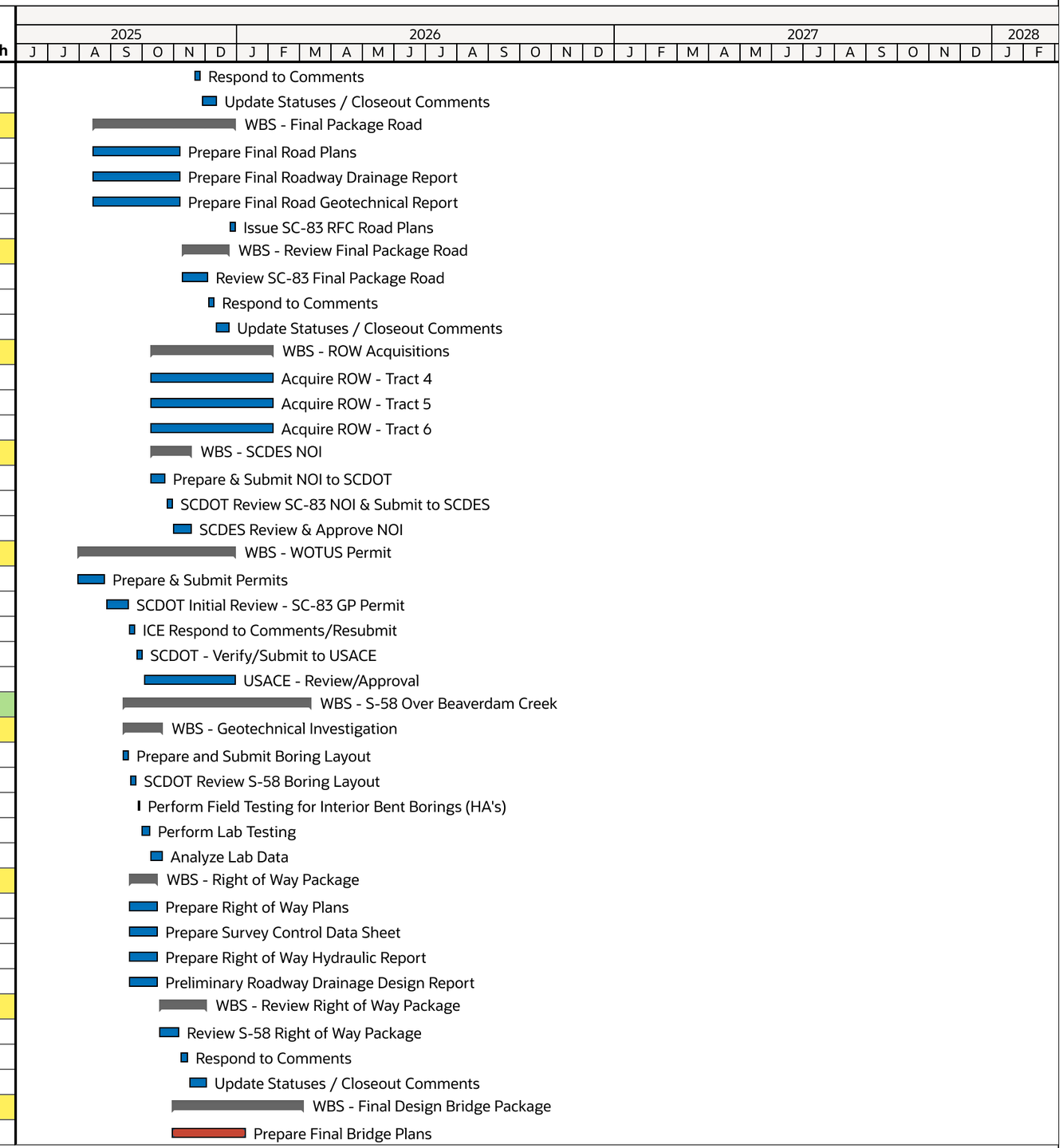
Note:
Detailed pre-construction activities are shown on the following pages.

© Oracle Corporation

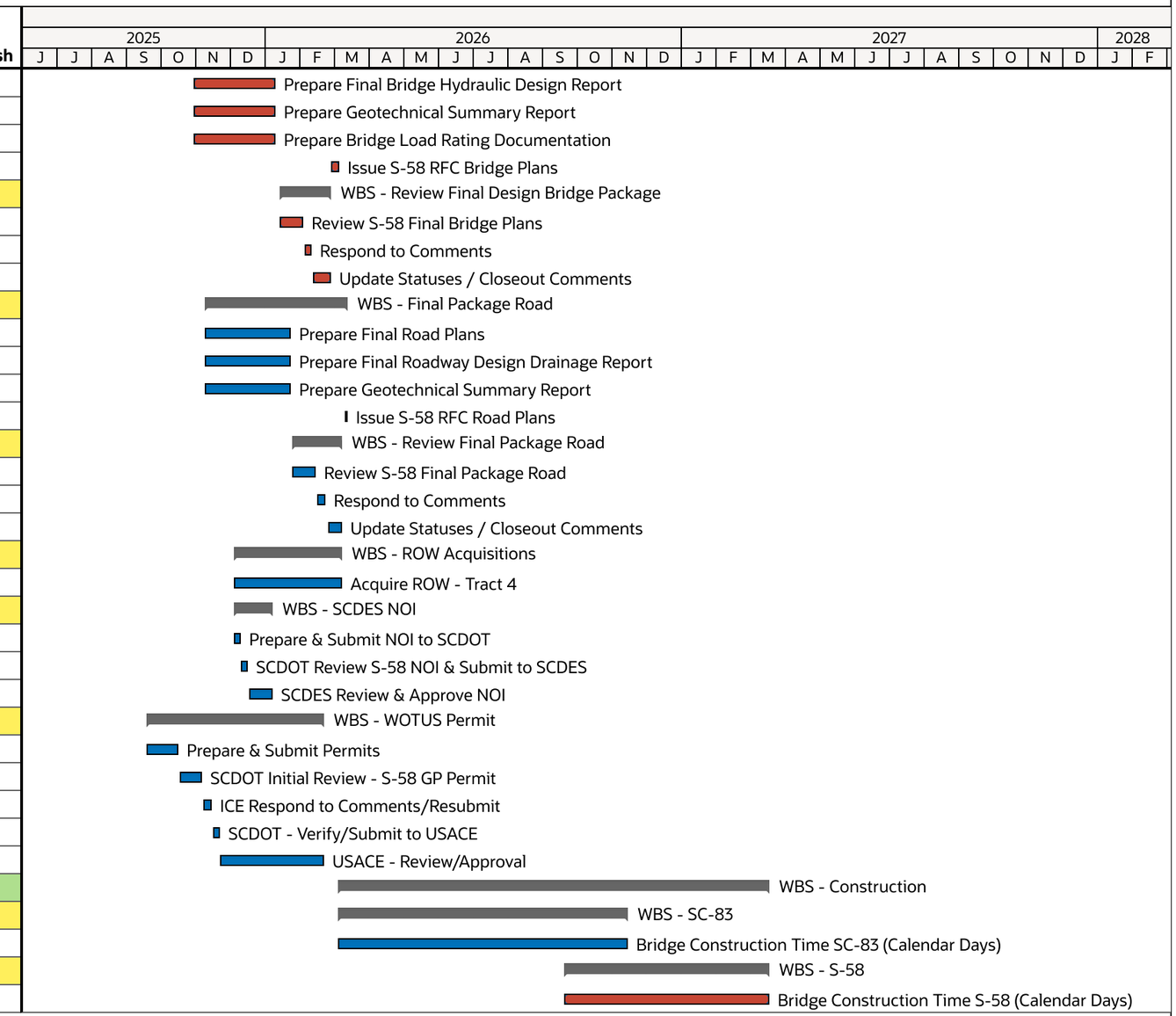
ID	Name	Planned Duration	Start	Finish
WBS - Bridge Package 27		431d	26-Jun-25	16-Mar-27
WBS - Milestones		431d	26-Jun-25	16-Mar-27
A1000	Public Announcement	0d		26-Jun-25
A1010	Anticipated Notice of Award	0d		10-Jul-25
A1020	SC-83 NOI Approved	0d	18-Nov-25	
A1030	S-58 NOI Approved	0d	07-Jan-26	
A1040	Issue SC-83 RFC Bridge Plans	0d	19-Dec-25	
A1050	Issue S-58 RFC Bridge Plans	0d	04-Mar-26	
A1060	Substantial Completion SC-83 (1/8/2027)	0d		08-Jan-27 C
A1070	Substantial Completion S-58 (4/6/2027)	0d		16-Mar-27
WBS - Design Review Submittal Package 000		22d	26-Jun-25	28-Jul-25
A1080	Prepare Design QC Plan	12d	26-Jun-25	14-Jul-25
A1090	Prepare Design Submittal Schedule	12d	26-Jun-25	14-Jul-25
A1100	SCDOT Review - Submittal 000	5d	15-Jul-25	21-Jul-25
A1110	ICE Address Comments & Resubmit	2d	22-Jul-25	23-Jul-25
A1120	SCDOT Review/Approval - Submittal 000	3d	24-Jul-25	28-Jul-25
WBS - SC-83 Over Little Pee Dee River		135d	22-Jul-25	04-Feb-26
WBS - Geotechnical Investigation		36d	22-Jul-25	10-Sep-25
A1130	Prepare and Submit Boring Layout	5d	22-Jul-25	28-Jul-25
A1140	Mobilize Drilling Equipment	2d	29-Jul-25	30-Jul-25
A1150	SCDOT Review SC-83 Boring Layout	3d	29-Jul-25	31-Jul-25
A1160	Perform Field Testing for Interior Bent Borings (6)	8d	01-Aug-25	12-Aug-25
A1170	Perform Lab Testing	10d	13-Aug-25	26-Aug-25
A1180	Analyze Lab Data	10d	27-Aug-25	10-Sep-25
WBS - Right of Way Package		20d	29-Jul-25	25-Aug-25
A1190	Prepare Right of Way Plans	20d	29-Jul-25	25-Aug-25
A1200	Prepare Survey Control Data Sheet	20d	29-Jul-25	25-Aug-25
A1210	Prepare Right of Way Hydraulic Report	20d	29-Jul-25	25-Aug-25
A1220	Preliminary Road Geotechnical Report	20d	29-Jul-25	25-Aug-25
A1230	Preliminary Roadway Drainage Design Report	20d	29-Jul-25	25-Aug-25
WBS - Review Right of Way Package		30d	26-Aug-25	07-Oct-25
A1240	Review SC-83 Right of Way Package	15d	26-Aug-25	16-Sep-25
A1250	Respond to Comments	5d	17-Sep-25	23-Sep-25
A1260	Update Statuses / Closeout Comments	10d	24-Sep-25	07-Oct-25
WBS - Final Design Bridge Package		96d	01-Aug-25	18-Dec-25
A1270	Prepare Final Bridge Plans	60d	01-Aug-25	27-Oct-25
A1280	Prepare Final Bridge Hydraulic Design Report	60d	01-Aug-25	27-Oct-25
A1290	Prepare Final Bridge Geotech Report	60d	01-Aug-25	27-Oct-25
A1300	Prepare Final Seismic Design Summary Report	60d	01-Aug-25	27-Oct-25
A1310	Prepare Bridge Load Rating Documentation	60d	01-Aug-25	27-Oct-25
A1320	Issue SC-83 RFC Bridge Plans	5d	12-Dec-25	18-Dec-25
WBS - Review Final Design Bridge Package		31d	28-Oct-25	11-Dec-25
A1330	Review SC-83 Final Bridge Plans	15d	28-Oct-25	18-Nov-25



ID	Name	Planned Duration	Start	Finish
A1340	Respond to Comments	5d	19-Nov-25	25-Nov-25
A1350	Update Statuses / Closeout Comments	10d	26-Nov-25	11-Dec-25
WBS - Final Package Road		94d	13-Aug-25	29-Dec-25
A1360	Prepare Final Road Plans	60d	13-Aug-25	06-Nov-25
A1370	Prepare Final Roadway Drainage Report	60d	13-Aug-25	06-Nov-25
A1380	Prepare Final Road Geotechnical Report	60d	13-Aug-25	06-Nov-25
A1390	Issue SC-83 RFC Road Plans	3d	24-Dec-25	29-Dec-25
WBS - Review Final Package Road		31d	07-Nov-25	23-Dec-25
A1400	Review SC-83 Final Package Road	15d	07-Nov-25	02-Dec-25
A1410	Respond to Comments	5d	03-Dec-25	09-Dec-25
A1420	Update Statuses / Closeout Comments	10d	10-Dec-25	23-Dec-25
WBS - ROW Acquisitions		80d	08-Oct-25	04-Feb-26
A1430	Acquire ROW - Tract 4	80d	08-Oct-25	04-Feb-26
A1440	Acquire ROW - Tract 5	80d	08-Oct-25	04-Feb-26
A1450	Acquire ROW - Tract 6	80d	08-Oct-25	04-Feb-26
WBS - SCDES NOI		27d	08-Oct-25	17-Nov-25
A1460	Prepare & Submit NOI to SCDOT	10d	08-Oct-25	22-Oct-25
A1470	SCDOT Review SC-83 NOI & Submit to SCDES	5d	23-Oct-25	29-Oct-25
A1480	SCDES Review & Approve NOI	12d	30-Oct-25	17-Nov-25
WBS - WOTUS Permit		105d	29-Jul-25	29-Dec-25
A1490	Prepare & Submit Permits	20d	29-Jul-25	25-Aug-25
A1500	SCDOT Initial Review - SC-83 GP Permit	15d	26-Aug-25	16-Sep-25
A1510	ICE Respond to Comments/Resubmit	5d	17-Sep-25	23-Sep-25
A1520	SCDOT - Verify/Submit to USACE	5d	24-Sep-25	30-Sep-25
A1530	USACE - Review/Approval	60d	01-Oct-25	29-Dec-25
WBS - S-58 Over Beaverdam Creek		123d	11-Sep-25	11-Mar-26
WBS - Geotechnical Investigation		27d	11-Sep-25	20-Oct-25
A1540	Prepare and Submit Boring Layout	5d	11-Sep-25	17-Sep-25
A1550	SCDOT Review S-58 Boring Layout	5d	18-Sep-25	24-Sep-25
A1560	Perform Field Testing for Interior Bent Borings (HA's)	2d	25-Sep-25	26-Sep-25
A1570	Perform Lab Testing	7d	29-Sep-25	07-Oct-25
A1580	Analyze Lab Data	8d	08-Oct-25	20-Oct-25
WBS - Right of Way Package		20d	17-Sep-25	15-Oct-25
A1590	Prepare Right of Way Plans	20d	17-Sep-25	15-Oct-25
A1600	Prepare Survey Control Data Sheet	20d	17-Sep-25	15-Oct-25
A1610	Prepare Right of Way Hydraulic Report	20d	17-Sep-25	15-Oct-25
A1620	Preliminary Roadway Drainage Design Report	20d	17-Sep-25	15-Oct-25
WBS - Review Right of Way Package		31d	16-Oct-25	01-Dec-25
A1630	Review S-58 Right of Way Package	15d	16-Oct-25	05-Nov-25
A1640	Respond to Comments	5d	06-Nov-25	13-Nov-25
A1650	Update Statuses / Closeout Comments	10d	14-Nov-25	01-Dec-25
WBS - Final Design Bridge Package		85d	28-Oct-25	03-Mar-26
A1660	Prepare Final Bridge Plans	50d	28-Oct-25	09-Jan-26



ID	Name	Planned Duration	Start	Finish
A1670	Prepare Final Bridge Hydraulic Design Report	50d	28-Oct-25	09-Jan-26
A1680	Prepare Geotechnical Summary Report	50d	28-Oct-25	09-Jan-26
A1690	Prepare Bridge Load Rating Documentation	50d	28-Oct-25	09-Jan-26
A1700	Issue S-58 RFC Bridge Plans	5d	25-Feb-26	03-Mar-26
WBS - Review Final Design Bridge Package		30d	12-Jan-26	24-Feb-26
A1710	Review S-58 Final Bridge Plans	15d	12-Jan-26	02-Feb-26
A1720	Respond to Comments	5d	03-Feb-26	09-Feb-26
A1730	Update Statuses / Closeout Comments	10d	10-Feb-26	24-Feb-26
WBS - Final Package Road		83d	07-Nov-25	11-Mar-26
A1740	Prepare Final Road Plans	50d	07-Nov-25	22-Jan-26
A1750	Prepare Final Roadway Design Drainage Report	50d	07-Nov-25	22-Jan-26
A1760	Prepare Geotechnical Summary Report	50d	07-Nov-25	22-Jan-26
A1770	Issue S-58 RFC Road Plans	3d	09-Mar-26	11-Mar-26
WBS - Review Final Package Road		30d	23-Jan-26	06-Mar-26
A1780	Review S-58 Final Package Road	15d	23-Jan-26	12-Feb-26
A1790	Respond to Comments	5d	13-Feb-26	20-Feb-26
A1800	Update Statuses / Closeout Comments	10d	23-Feb-26	06-Mar-26
WBS - ROW Acquisitions		65d	02-Dec-25	06-Mar-26
A1810	Acquire ROW - Tract 4	65d	02-Dec-25	06-Mar-26
WBS - SCDES NOI		24d	02-Dec-25	06-Jan-26
A1820	Prepare & Submit NOI to SCDOT	5d	02-Dec-25	08-Dec-25
A1830	SCDOT Review S-58 NOI & Submit to SCDES	5d	09-Dec-25	15-Dec-25
A1840	SCDES Review & Approve NOI	12d	16-Dec-25	06-Jan-26
WBS - WOTUS Permit		105d	17-Sep-25	19-Feb-26
A1850	Prepare & Submit Permits	20d	17-Sep-25	15-Oct-25
A1860	SCDOT Initial Review - S-58 GP Permit	15d	16-Oct-25	05-Nov-25
A1870	ICE Respond to Comments/Resubmit	5d	06-Nov-25	13-Nov-25
A1880	SCDOT - Verify/Submit to USACE	5d	14-Nov-25	20-Nov-25
A1890	USACE - Review/Approval	60d	21-Nov-25	19-Feb-26
WBS - Construction		379d	03-Mar-26	16-Mar-27
WBS - SC-83		256d	03-Mar-26	13-Nov-26
A1900	Bridge Construction Time SC-83 (Calendar Days)	256d	03-Mar-26	13-Nov-26
WBS - S-58		180d	18-Sep-26	16-Mar-27
A1910	Bridge Construction Time S-58 (Calendar Days)	180d	18-Sep-26	16-Mar-27



APPENDIX B

REQUIRED FORMS AND CONFIDENTIAL AND PROPRIETARY INFORMATION

A. STIPEND ACKNOWLEDGMENT FORM

12. STIPEND ACKNOWLEDGEMENT FORM

Stipend Acknowledgement Form

Bridge Package 27
Dillon and Marlboro Counties

Proposer: Crowder Construction Company / Infrastructure Consulting & Engineering

ADDRESS: PO Box 30007, Charlotte, NC 28230

The undersigned Proposer, hereby:

☐ Waives the stipend for this Project.

☑ Accepts the stipend for this Project.

By accepting the stipend for this Project, Proposer agrees:

1) to execute and include the Stipend Agreement in Article XIII of the RFP with its RFP response;

2) to submit an invoice with FEIN number for the stipend amount to the SCDOT POC after SCDOT's posting of the Notice of Award on SCDOT's Design-Build Website.;

3) to transfer all rights to its Work Product used to develop the Proposal as of the date of this acknowledgement. "Work Product" " means all submittals, including ideas, innovations, solutions, methods, processes, design concepts, materials, electronic files, marked up drawings, cross sections, quantity lists and intellectual property, made by Proposer during the RFP process, including the Proposal, exchange of information during the pre-Proposal and post-Proposal period.

SCDOT will pay the stipend to each eligible unsuccessful Proposer, who has signed a Stipend Agreement, within ninety (90) days after execution of the Contract or the decision to not award a contract.

June 13, 2025
Date

Proposer

George F Ellis, Executive Vice President
Print Name

B. STIPEND AGREEMENT

13. STIPEND AGREEMENT

STIPEND AGREEMENT
Project ID: 5570770
Bridge Package 27
Dillon and Marlboro Counties

THIS STIPEND AGREEMENT (the "Agreement") is made and entered into as of the 13th day of June, 2025 by and between the SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION (hereinafter "SCDOT"), and Crowder Construction Company ("Proposer"), with reference to the following facts:

SCDOT issued a Request for Proposal ("RFP") for design and construction of the above-referenced Design-Build Project ("Project"), pursuant to procurement authority granted in Section 57-5-1625 of the S.C. Code of Laws, 1976, as amended. The RFP provided for payment of stipends as provided herein. Capitalized terms used, but not defined, have the meanings ascribed in the RFP.

NOW, THEREFORE, Proposer hereby agrees as follows:

1. Work Product.

1.1 Proposer shall prepare and submit a responsible and responsive Technical Proposal and Cost Proposal that conforms in all material respects to the requirements and provisions of the RFP, as determined by SCDOT, and are timely received by SCDOT in accordance with the RFP Milestone Schedule.

1.2 By signing this Stipend Agreement, Proposer agrees to transfer full and complete ownership to SCDOT of all Work Product. The Work Product (as defined below) shall become the property of SCDOT without restriction or limitation on its use, without further compensation or consideration, and can be used in connection with this Project or any future projects by SCDOT. Neither Proposer nor any of its team members shall copyright any of the material developed under this Agreement.

1.3 The term "Work Product" shall mean the Proposal and all material, electronic files, marked up drawings, cross sections, quantity lists, submittals, ideas, innovations, solutions, methods, processes, design concepts, Trade Secrets or confidential information, and intellectual property, made by or produced for Proposer in the development and submission of the Technical and Cost Proposal, including exchanges of information during the pre-Proposal and post-Proposal period.

2. Compensation and Payment.

2.1 A stipend to Proposer for the Work Product described herein shall be $20,000 and is payable to Proposer that was determined to be responsible and (1) submitted a responsive Technical Proposal and responsive Cost Proposal to the RFP which is not selected for award of this Project, or (2) was awarded the Contract but the Contract was terminated by SCDOT for convenience after the Submittal of Proposal Due Date (See Final RFP Milestone schedule) but prior to the Notice to Proceed #1. Responsibility of Proposers and responsiveness of the Technical Proposal and Cost Proposal will be determined by SCDOT as a condition of payment.

2.2 SCDOT will pay the stipend to Proposer as follows, subject (as applicable) to the following conditions:

(a) Proposer has submitted this signed Stipend Agreement, unchanged with its response to the RFP.

(b) After posting of the Notice of Award on SCDOT's Design-Build Website, Proposer has submitted to SCDOT an invoice, with FEIN Number, for the Stipend amount.

(c) After execution of the Contract or the decision not to award a contract, SCDOT will pay the invoice for the stipend amount to the unsuccessful Proposer meeting the criteria of Section 2.1 within 90 calendar days of receipt of the invoice from Proposer.

(d) If the procurement is suspended or cancelled prior to the Proposal Due Date (see FINAL RFP Milestone schedule), no stipend will be paid to Proposer.

(e) After the submittal of Proposals, but prior to award, if the procurement is cancelled, all Proposers that provide a responsive Technical Proposal and Cost Proposal to the final RFP and submitted a signed Stipend Agreement with their RFP shall receive the stipend

(f) In the event of a Best and Final Offer, only one stipend will be paid to each Proposer that executed a Stipend Agreement and met the other criteria and conditions herein.

(g) No stipends will be paid for submitting RFQ responses.

(h) No stipends will be paid to a Proposer who withdraws at any time from this procurement.

2.3 Acceptance by the Proposer of payment of the stipend amount from SCDOT shall constitute a waiver by Proposer of any and all right, equitable or otherwise, to bring any claim in connection with this procurement, procurement process, award of the Contract, or cancellation of this procurement.

2.4 The Proposer awarded the contract shall be not eligible to receive a stipend.

2.5 If Proposer elects to waive payment of the stipend, SCDOT will not use the ideas or information contained in that Proposer's Proposal for this Project. However, the Proposer's Proposal will be subject to the South Carolina Freedom of Information Act.

3. Indemnities.

3.1 Subject to the limitations contained in Section 3.2, Proposer shall indemnify, protect and hold harmless SCDOT and its directors, officers, employees and contractors from, and Proposer shall defend at its own expense, all claims, costs, expenses, liabilities, demands, or suits at law or equity arising, in whole or in part, from the negligence or willful misconduct of Proposer or any of its agents, officers, employees, representatives or subcontractors or breach of any of Proposer's obligations under this Agreement.

3.2 This indemnity shall not apply with respect to any claims, demands or suits arising from use of the Work Product by SCDOT.

4. Compliance With Laws.

4.1 Proposer shall comply with all federal, state, and local laws, ordinances, rules, and regulations applicable to the work performed or paid for under this Agreement and covenants and agrees that it and its employees shall be bound by the standards of conduct provided in applicable laws, ordinances, rules, and regulations as they relate to work performed under this Agreement. Proposer agrees to incorporate the provisions of this paragraph in any subcontract into which it might enter with reference to the work performed pursuant to this Agreement.

4.2 The Proposer agrees (a) not to discriminate in any manner against an employee or applicant for employment because of race, color, religion, creed, age, sex, marital status, national origin, ancestry or disability of a qualified individual with a disability; (b) to include a provision similar to that contained in subsection (a) in any subcontract; and (c) to post and to cause subcontractors to post in conspicuous places available to employees and applicants for employment, notices setting forth the substance of this clause.

5. Assignment.

Proposer shall not assign this Agreement without SCDOT's prior written consent. Any assignment of this Agreement without such consent shall be null and void.

6. Miscellaneous.

6.1 Proposer and SCDOT agree that Proposer, its team members, and their respective employees are not agents of SCDOT as a result of this Agreement.

6.2 This Agreement, together with the RFP, as amended from time to time, the provisions of which are incorporated herein by reference, embodies the entire agreement of the parties. There are no promises, terms, conditions, or obligations other than those contained herein or in the RFP, and this Agreement shall supersede all previous communications, representation, or agreements, either oral or written, between the parties hereto.

6.3 It is understood and agreed by the parties hereto that if any part, term, or provision of this Agreement is by the courts held to be illegal or in conflict with any law of the State of South Carolina, the validity of the remaining portions or provisions shall not be affected, and the rights and obligations of the parties shall be construed and enforced as if the Agreement did not contain the particular part, term, or provisions to be invalid.

6.4 This Agreement shall be governed by and construed in accordance with the laws of the State of South Carolina.

IN WITNESS WHEREOF, the parties have executed this Agreement as of the date first written above.

Witness:

_____

Recommended:

_____
{INSERT NAME}

SOUTH CAROLINA DEPARTMENT
OF TRANSPORATION

By: _____
{INSERT NAME}
Preconstruction Alternative Delivery Engineer

Proposer

_____
Gerorge F Ellis, Executive Vice President
Crowder Construction Company

C. EQUAL EMPLOYMENT OPPORTUNITY CERTIFICATE

11. EQUAL EMPLOYMENT OPPORTUNITY CERTIFICATION

(COMPLETE THIS SECTION FOR FEDERAL PROJECTS ONLY)
EQUAL EMPLOYMENT OPPORTUNITY PERFORMANCE

Select the Certification that applies to the PROPOSER:

Certification (1) [X] or Certification (2) [ ]

Select the appropriate responses in the applicable Certification:

Certification (1): Pursuant to 41 C.F.R. §60-1.7(b)(1), Previous Equal Employment Opportunity Performance Certification, as the Prospective Prime Contractor, I HEREBY CERTIFY THAT I:

(a) (HAVE / **HAVE NOT)** developed and filed an Affirmative Action Program pursuant to 41C.F.R. §60-2 and/or 60-4;

(b) (HAVE / **HAVE NOT)** participated in a previous contract or subcontract subject to the equal opportunity clause;

(c) (HAVE / **HAVE NOT)** filed with the Joint Reporting Committee, the Director of Office of Federal Contract Compliance, or the Equal Employment Opportunity Commission, all reports due under the applicable filing requirements,

OR

Certification (2): I, HEREBY CERTIFY that as the Prospective Prime Contractor submitting this Proposal, **(CLAIM / DO NOT CLAIM)** exemption from the submission of the Standard Form 100 (EEO-1) due to the fact that it employs a total of less than fifty (50) employees under C.F.R. §60-1.7, or qualifies for an exempted status under 41 C.F.R. §60-1.5.

I FURTHER CERTIFY that the above Certification will be made part of any Subcontract Agreement, or other agreement involved with this project.

Executed on 06/13, 20 25 .

Signed: _____
(Officer/PROPOSER)

Title: Executive Vice President

Company: Crowder Construction Company

Address: PO Box 30007, Charlotte, NC 28230

Note: The above certification is required by the Equal Employment Opportunity Regulations of the Secretary of Labor (41 CFR 60-1.7(b)(1)), and must be submitted by PROPOSERS only in connection with contracts which are subject to the equal opportunity clause. Contracts that are exempt from the equal opportunity clause are set forth in 41 CFR 60-1.5. (Generally, only contracts of $10,000 or under are exempt.)

Currently, Standard Form 100 (EEO-1) is the only report required by Executive Orders or their implementing regulations.

Proposers, Primary Members, or proposed Subcontractors (any tier) and Consultants who have participated in a previous contract subject to the Executive Orders and have not filed the required reports shall note that 41 CFR 60-1.7(b)(1) prevents the award of contracts and subcontracts unless such contractor submits a report covering the delinquent period or such other period specified by the Federal Highway Administration or by the Director, Office of Federal Contract Compliance, U.S. Department of Labor.

D. NON-COLLUSION CERTIFICATE

10. NON-COLLUSION CERTIFICATION

NON-COLLUSION CERTIFICATION

Project ID: 5570770

IN ACCORDANCE WITH THE PROVISIONS OF S.C. CODE ANN. §§ 39-3-10 ET.SEQ., 39-5-10 ET. SEQ., 15 U.S.C. §45; 23 C.F.R. §635.112(F); AND 28 U.S.C. §1746, I HEREBY ACKNOWLEDGE THAT I AM AN OFFICER OF THE PROPOSER FIRM AND, UNDER PENALTY OF PERJURY UNDER THE LAWS OF THE UNITED STATES AND SOUTH CAROLINA, DECLARE, BY MY CERTIFICATION BELOW, THAT THE FOLLOWING IS TRUE AND CORRECT, AND FURTHER, THAT THIS JOINT-VENTURE, FIRM, PARTNERSHIP, ASSOCIATION OR CORPORATION, OR ANY OTHER LEGAL ENTITY HAS NOT, EITHER DIRECTLY OR INDIRECTLY, ENTERED INTO ANY AGREEMENT, PARTICIPATED IN ANY COLLUSION, OR OTHERWISE TAKEN ANY ACTION IN RESTRAINT OF FREE COMPETITIVE BIDDING IN CONNECTION WITH THE SUBMISSION OF A BID PROPOSAL ON THE ABOVE REFERENCED PROJECT.

BY CHECKING THIS BOX ☒, I CERTIFY THAT I HAVE READ, UNDERSTAND, ACCEPT, AND ACKNOWLEDGE ALL OF THE ABOVE STATEMENTS.

Executed on 06/13/2025
(Date)

Signed: _____
(Officer/Proposer)
George F Ellis, Crowder Construction Company

Executive Vice President
(Title)

PO Box 30007, Charlotte, NC 28230
(Address)

E. NOTICE OF RECEIPT OF ADDENDUM

South Carolina
Department of Transportation

NOTICE OF RECEIPT

Bridge Package 27
Design-Build – Project ID 5570770
Dillon and Marlboro County

Addendum 1

The information in this addendum shall be made part of the contract documents. PROPOSERS are instructed to incorporate the information into the previously provided RFP documents.

PROPOSERS are required to sign this document and enclose it with their Technical Proposal. Receipt of this signed document by The South Carolina Department of Transportation serves as confirmation that the PROPOSER has received and incorporated this Addendum into the contract documents.

Confirmation Statement:

I, the PROPOSER confirm that I have received this addendum package and have incorporated the information provided in the addendum into the contract documents.

PROPOSER's Signature

6-5-2025
Date

George F Ellis, Executive Vice President
Printed Name

For: Crowder / ICE
Design-Build Team Name

South Carolina
Department of Transportation

NOTICE OF RECEIPT

Bridge Package 27
Design-Build – Contract ID 5570770
Dillon and Marlboro Counties

Addendum 2

The information in this addendum shall be made part of the contract documents. PROPOSERS are instructed to incorporate the information into the previously provided RFP documents.

PROPOSERS are required to sign this document and enclose it with their Technical Proposal. Receipt of this signed document by The South Carolina Department of Transportation serves as confirmation that the PROPOSER has received and incorporated this Addendum into the contract documents.

Confirmation Statement:

I, the PROPOSER confirm that I have received this addendum package and have incorporated the information provided in the addendum into the contract documents.

PROPOSER's Signature

6/11/2025
Date

George F Ellis, Executive Vice President
Printed Name

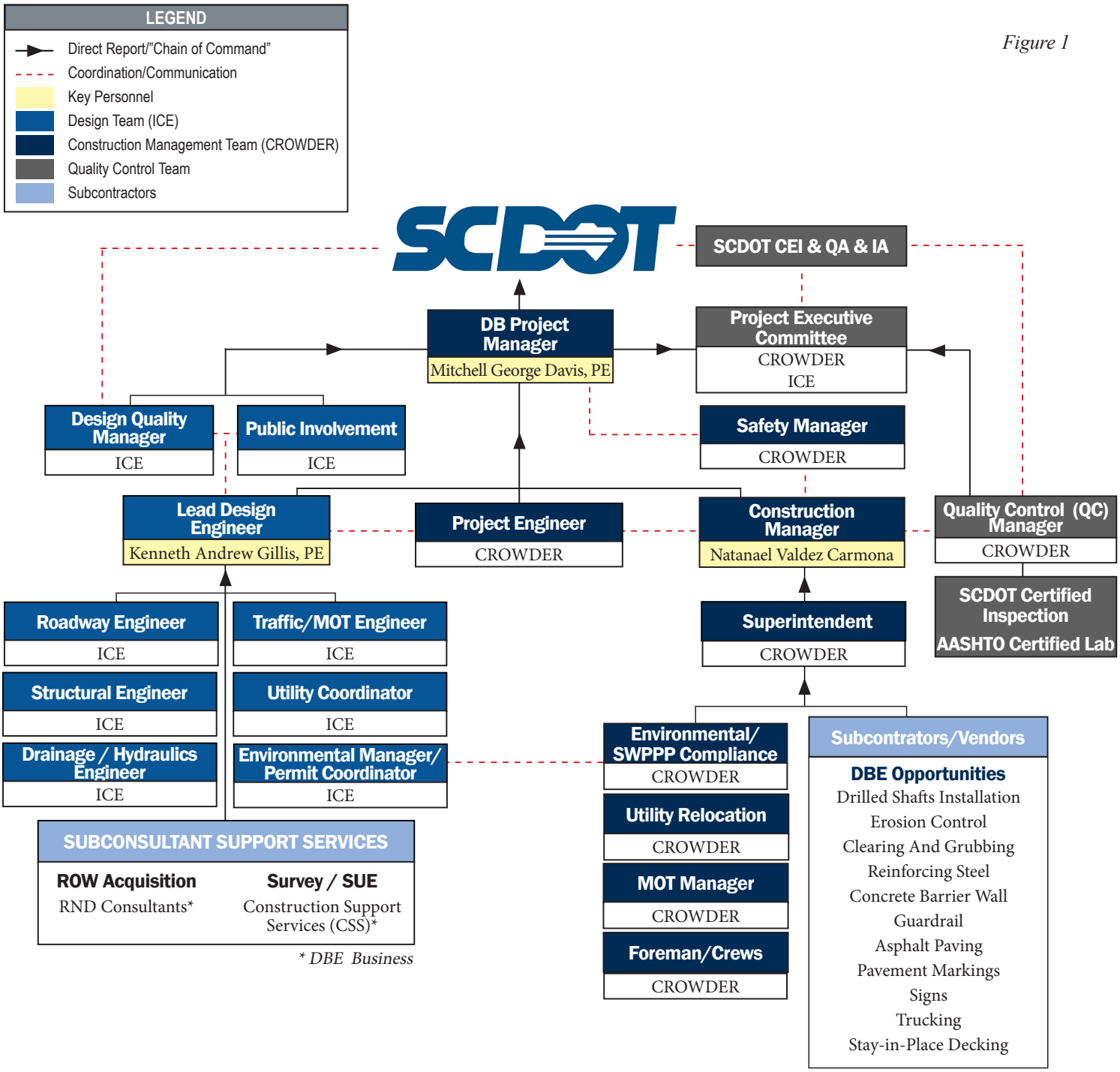
For: Crowder / ICE
Design-Build Team Name

F. UPDATED ORGANIZATION CHART AND NOTARIZED STATEMENT OF AVAILABILITY OF KEY INDIVIDUALS

Organizational Chart

The organizational chart highlights the Crowder+ICE team's key personnel who are committed to facilitating transparent communication and partnering with SCDOT to deliver this project. All key personnel identified meet requirements of the RFQ and the SCDOT's quality and schedule expectations. Crowder Construction Company and Infrastructure Consulting & Engineering (ICE) confirm availability of key staff for the duration of the project.

Figure 1



The Crowder/ICE Team acknowledges there are no key personnel changes that have been made to the original organizational chart submitted in the Statement of Qualifications.

PO Box 30007 (28230-0007)
6409 Brookshire Boulevard (28216)
Charlotte, NC
Telephone: 704.332.8184 Fax 704.372.9946

CROWDER INFRASTRUCTURE CONSULTING & ENGINEERING

PROPOSER'S STATEMENT

SCDOT BRIDGE PACKAGE 27

DESIGN BUILD PROJECT | PROJECT ID: 5570770 | COUNTY: DILLON AND MARLBORO

Proposed Key Individual, Mitchell Davis, Project Manager, identified in the original organizational chart in the Crowder Construction Company / Infrastructure Consulting & Engineering Statement of Qualifications, will be available barring any unforeseen circumstances at the earliest of the times and durations identified in the RFQ and RFP, until expiration of the Warranty Period, or such earlier date as the Contract is terminated or SCDOT releases, in writing, such Key Individual from this requirement.

George F. Ellis, Executive Vice President — Date: 5/20/25

Mitchell Davis, Project Manager — Date: 5/20/25

State: North Carolina
County: Mecklenburg

I, Stephanie Coppola, a Notary Pubic for said County and State, do hereby certify that George F Ellis personally appeared before me this day and acknowledged the due execution of the foregoing statement.

Witness my hand and official seal this 20th day of May, 2025

Notary Public

My Commission Expires: 04/24/2029

STEPHANIE COPPOLA NOTARY PUBLIC GASTON COUNTY, NC My Commission Expires April 24, 2029

State: North Carolina
County: Mecklenburg

I, Stephanie Coppola, a Notary Pubic for said County and State, do hereby certify that Mitchell Davis personally appeared before me this day and acknowledged the due execution of the foregoing statement.

Witness my hand and official seal this 20th day of May, 2025

Notary Public

My Commission Expires: 04/24/2029

STEPHANIE COPPOLA NOTARY PUBLIC GASTON COUNTY, NC My Commission Expires April 24, 2029

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6409 Brookshire Boulevard (28216)
Charlotte, NC
Telephone: 704.332.8184 Fax 704.372.9946

CROWDER INFRASTRUCTURE CONSULTING & ENGINEERING

PROPOSER'S STATEMENT

SCDOT BRIDGE PACKAGE 27

DESIGN BUILD PROJECT | PROJECT ID: 5570770 | COUNTY: DILLON AND MARLBORO

Proposed Key Individual, Andy Gillis, Lead Design Engineer, identified in the original organizational chart in the Crowder Construction Company / Infrastructure Consulting & Engineering Statement of Qualifications, will be available barring any unforeseen circumstances at the earliest of the times and durations identified in the RFQ and RFP, until expiration of the Warranty Period, or such earlier date as the Contract is terminated or SCDOT releases, in writing, such Key Individual from this requirement.

George F. Ellis, Executive Vice President — Date: 5/20/25

Mitchell Davis, Project Manager — Date: 5/20/25

State: North Carolina
County: Mecklenburg

I, Stephanie Coppola, a Notary Pubic for said County and State, do hereby certify that George F Ellis personally appeared before me this day and acknowledged the due execution of the foregoing statement.

Witness my hand and official seal this 20th day of May, 2025

Notary Public

My Commission Expires: 04/24/2029

STEPHANIE COPPOLA NOTARY PUBLIC GASTON COUNTY, NC My Commission Expires April 24, 2029

State: North Carolina
County: Mecklenburg

I, Stephanie Coppola, a Notary Pubic for said County and State, do hereby certify that Mitchell Davis personally appeared before me this day and acknowledged the due execution of the foregoing statement.

Witness my hand and official seal this 20th day of May, 2025

Notary Public

My Commission Expires: 04/24/2029

STEPHANIE COPPOLA NOTARY PUBLIC GASTON COUNTY, NC My Commission Expires April 24, 2029

PO Box 30007 (28230-0007)
6409 Brookshire Boulevard (28216)
Charlotte, NC
Telephone: 704.332.8184 Fax 704.372.9946

CROWDER INFRASTRUCTURE CONSULTING & ENGINEERING

PROPOSER'S STATEMENT

SCDOT BRIDGE PACKAGE 27

DESIGN BUILD PROJECT | PROJECT ID: 5570770 | COUNTY: DILLON AND MARLBORO

Proposed Key Individual, Nate Carmona, Construction Manager, identified in the original organizational chart in the Crowder Construction Company / Infrastructure Consulting & Engineering Statement of Qualifications, will be available barring any unforeseen circumstances at the earliest of the times and durations identified in the RFQ and RFP, until expiration of the Warranty Period, or such earlier date as the Contract is terminated or SCDOT releases, in writing, such Key Individual from this requirement.

George F. Ellis, Executive Vice President — Date: 5/20/25

Mitchell Davis, Project Manager — Date: 5/20/25

State: North Carolina
County: Mecklenburg

I, Stephanie Coppola, a Notary Pubic for said County and State, do hereby certify that George F Ellis personally appeared before me this day and acknowledged the due execution of the foregoing statement.

Witness my hand and official seal this 20th day of May, 2025

Notary Public

My Commission Expires: 04/24/2029

STEPHANIE COPPOLA NOTARY PUBLIC GASTON COUNTY, NC My Commission Expires April 24, 2029

State: North Carolina
County: Mecklenburg

I, Stephanie Coppola, a Notary Pubic for said County and State, do hereby certify that Mitchell Davis personally appeared before me this day and acknowledged the due execution of the foregoing statement.

Witness my hand and official seal this 20th day of May, 2025

Notary Public

My Commission Expires: 04/24/2029

STEPHANIE COPPOLA NOTARY PUBLIC GASTON COUNTY, NC My Commission Expires April 24, 2029

G. CONFIDENTIAL AND PROPRIETARY INFORMATION PAGE LIST

No page of this submitted technical proposal contains confidential or proprietary information.

H. PREQUALIFICATION CERTIFICATE FOR PROPOSER

South Carolina Department of Transportation

Columbia, South Carolina

South Carolina Department

Of

Transportation

Prime Contractor

Prequalification Certificate

This Certifies that your company has complied with the rules and regulations of the Department and the State of South Carolina, and subject to the rules and regulations for a prime contractor, is declared eligible to submit a bid and be awarded any construction contract issued by the Department, subject to obtaining proper bonds and insurance acceptable to the Department and complying with all other statutory and contract requirements.

All BIDS SUBMITTED TO THE DEPARTMENT MUST BE IN THE NAME AS SHOWN BELOW.

VENDOR NAME

CROWDER CONSTRUCTION COMPANY

Vendor ID:	**1CR007**
Date Issued:	**April 15, 2025**
Expiration Date:	**May 31, 2026**

Approved By: _____

Prequalification and Contracts Coordinator

H. JOINT VENTURE ORGANIZATIONAL AGREEMENT

Not applicable.

H. QUALITY CREDIT MATRIX

QUALITY CREDIT MATRIX

Number	Description	Added Value/Benefits	Cost/Schedule Impacts	Self-imposed Assurance
1	Right of Way Reduction	The Crowder+ICE Team was able to reduce new Right of Way on Package 27 by over 10,000 SF. Accomplished through using a FIB 45 girders and relocating a proposed driveway.	Within our proposed design we have reduced the required ROW impacts. Conservatively assuming $1.00/SF, our team's ROW reductions result in a $10,000 direct savings to the SCDOT.	The design build prep plans identified 58,566 SF of combined new Right of Way on Package 27. Our team identified 10,350 SF in Right of Way reduction. Crowder will reimburse the Department for the cost of new Right of Way above our estimated total area of 48,216 SF if it is determined that more Right of Way is required.
2	Minimizing wetland impacts with FIB 45 girders	Reduced superstructure depth means a lower roadway profile. The result is lesser side slope impacts which reduce environmental wetland/stream impacts and mitigation credit needs.	No direct costs to Contractor, since SCDOT is paying for wetland mitigation credits; however cost savings for SCDOT for reduced environmental mitigation credit needs.	Not applicable

COST/SCHEDULE IMPACTS – Describe the Cost in dollars and/or Schedule impacts in days associated with the feature (positive or negative).

SELF-IMPOSED ASSURANCE – Discuss any penalties or liquidated damages that will apply in the event the CONTRACTOR cannot implement the feature as described.