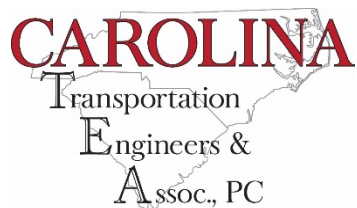


Technical Proposal
SCDOT Design Build Project
Bridge Package 14 DB Project
Contract ID 1162220



PROJECT DELIVERY AND APPROACH

Palmetto Infrastructure Inc. (PII) and Carolina Transportation Engineers & Associates, PC

(CTEA) have assembled a highly qualified and motivated team to deliver this DB Project. We have the design capabilities and the contracting resources to **beat the required schedule by 25%**. CTEA's bridge and roadway design staff is supported with geotechnical, utility coordination, permitting/ public involvement and right of way specialists to complete the preconstruction efforts for each site. The Design Manager, Derek Staton, works daily with PII as well as our prestressed concrete fabricator (Florence Concrete Products) to resolve construction issues and develop plans to facilitate construction.

Our fundamental design and construction approach to these bridge replacements is "Keep It Simple." Three of the five sites will be constructed with single span structures, and the remaining two sites have only a single interior bent. Cored slab and box beam bridges are very efficient structure types and are exactly the structures PII is organized to build. All bridges will utilize SCDOT Standard plans for cored slab bridges, and the SCDOT supplied plans for box beams.

Minimizing the number of spans provides SCDOT reduced construction costs, reduced future maintenance, and a faster construction schedule (benefit to the public). Elimination of interior bents reduces debris buildup in the channel and eliminates joint maintenance issues in the deck.

Roadway horizontal alignments are not altered – even at S-86 where allowed by the RFP. Roadway profiles provide:

- 2' freeboard to the design highwater (and/or clear the 500 year HWEL if below the existing low chord),
- a low point of the roadway set off the bridge, and
- vertical curve requirements per PCDM-11 criteria as further explained in the RFP.

Assurance for On-Time Completion: There is no greater assurance for on-time delivery of projects than past performance. PII and CTEA are committed to providing quality design and construction deliverables within the required schedule. In fact, **we are self-imposing LDs on our team if we don't beat the overall schedule by 150 days.** We have a proven successful history of accelerated delivery (EBP 2018-2A DB and EBP 2020-1 DB). CTEA has completed every project undertaken to date within the schedule established (or early) and PII has *never* been assessed LDs on a DB Project. Both PII and CTEA are eager to start this work as early as possible!

SCDOT has allotted 640 days for substantial completion of these five bridges. The overlap between design, permitting and construction is substantially more than was allotted on the Emergency Bridge Packages previously completed by this team. We plan to design, fabricate slabs and box beams, acquire ROW, relocate utilities and perform construction simultaneously at multiple sites, giving us more than ample time for each activity. PII anticipates Substantial Completion 150 days early, nearly 25% faster than requested by SCDOT.

PII has a plan to accomplish early delivery of this project, coordinating two bridge crews with earthwork and paving subcontractors. We will obtain approval of our detour plans prior to NTP and set new detour signage within 45 days of NTP as required. We will initiate clearing/grubbing activities and utility relocations prior to closing roadways, while awaiting plan approvals. Erosion control will be installed, and each site will be cleared and/or grubbed to the ROW limits (or nearest water feature) as per RFP requirements.

S-138 will be prioritized as required by the RFP. We also recognize the required waterline relocation at this site may delay the start of construction. We intend to reopen the roadways currently closed as early as possible. Simultaneous to S-138, we will develop plans for S-56, a single span structure with limited utility conflicts and no ROW required for construction, allowing PII to go to work almost immediately. S-106 and S-86 will follow, each being able to be constructed with limited additional ROW. S-226 will

be completed last as it is currently open to traffic. This schedule allows PII to maximize the available use of the roadways to the public – as noted in our Quality Matrix Form – and we are standing behind early delivery of every site with self-imposed LDs.

For each site, existing bridges will be removed, and rough grading completed. Bents will be constructed, and rip rap protection will be placed on the finished grade of the slopes. The bridge, approach slabs, concrete flumes and barrier rails will be constructed, and the pavement and guardrails will

Typical Single Span Construction Schedule	
After NTP	Install EC, Clearing, Utility Relocation, Detour Signage
Week 1	Mobilization / Existing Structure Demo
Week 2	Grade EB1/ Install EB1 Piling
Week 3	Grade EB2 / Install EB2 Piling / Pour EB1
Week 4	Pour EB2 / Set Slabs
Week 5	Grout Slabs / Pour Wing Walls
Week 6	Grade Approaches
Week 7	Pour Approach Slabs
Week 8	Place Barriers / Fine Grade / Shoulders
Week 9	Place Concrete Flumes & Install guardrail
Week 10	Final Paving
Week 11	Seeding & Stripping

be installed. We will complete each bridge early, as committed to in the Quality Matrix Form and as shown in our CPM schedule in Appendix C.

To address the challenges of utility relocations and ROW acquisitions, bridge sites with known conflicts are pushed to later in the schedule (except S-138). This allows additional time to work with the utility owners to design and implement relocations as necessary and time to acquire ROW. We started utility coordination at each site during the pursuit. We know which power lines can be dropped during construction, shielded from construction activities, placed on temporary poles, or relocated, and how much time each process will take. We are working with water providers to determine if conflicts exist and communicating with cable and phone providers to develop relocation schedules. With our schedule as shown, we anticipate the only delays to construction due to utilities may be at S-138. This is further evidenced in our guarantees for delivery dates in the quality matrix in Appendix B.

The PII construction schedule is based on completing work at each site before moving the bridge crews to the next site. We recognize our ability to work on concurrent sites and are therefore confident in our assurances to completing this project ahead of schedule – even beating our own guarantees. Many construction activities will take place simultaneously, and when necessary, we can utilize multiple crews at a site to accelerate the schedule. Adding time for weather delays, equipment issues, subcontractor performance and other delays, each site will be completed in less time than the minimum allotted Intermediate Contract Time per the RFP.

PII will remain continuously active at S-226 once it is closed and until it is reopened. We understand the inconvenience to the local residences to have roadways out of service and will work aggressively to restore the roadway network quickly.

The PII Team will self-perform major construction items on the critical path including project management, DB coordination, construction oversight, bridge demolition, bridge foundations, bridge substructures, bridge superstructures and traffic control / MOT. Specialty subcontractors, including DBEs, will be utilized for items including earthwork, paving, guardrail installation and striping. PII has the equipment, resources and availability to provide two crews to the project as soon as possible after NTP (once designs are approved for construction).

Design Approach: CTEA and PII approached this project with a goal for simplicity. Single-span structures, completely spanning the channel with required setbacks, and appropriately sized to provide hydraulic requirements as defined by the RFP, are utilized to the maximum extent practical. We focus on cored slab and box beam bridges as they are the most economical option for construction. Single spans eliminate joints and bearings, reduce or eliminate debris buildup under the bridge, eliminate pier scour, and are the fastest to construct; minimize impacts to the local communities.

CTEA set the bridges based on the minimum geometrical constraints including setbacks from the top of channel, RFP bridge length requirements, and minimum / maximum abutment heights. Hydraulic models

were provided (and verified) and the low chord set based on the results (lowering the low chord when not hydraulically controlled, setting the low chord height based on the 25-yr HWEL plus freeboard, and lengthening the bridge if backwater is increased). Roadway profiles are based on maintaining the minimum required low chord across the bridge, matching maximum grades within project limits, providing vertical curves to meet the required design speed, and keeping the low point off the bridge.

Paving limits are developed for the area where the roadway profile is reconstructed, and construction limits are extended when guardrail extends beyond the profile adjustment limits. CTEA evaluated where extending guardrail, steepening slopes, or utilizing retaining walls results in a reduction to the ROW.

Where applicable these techniques were implemented utilizing engineering judgment to determine where and when to incorporate each. We will review these decisions with SCDOT post award and may adjust our plans pending the outcome of those conversations.

Each site maintains the existing horizontal alignment.

CTEA utilizes end bents with up to the maximum allowable 5 ft exposed height from top of berm.

ROW: ROW follows the guidelines in the RFP. We will acquire 75 ft of ROW on each side of centerline of the existing roadway for a length of 75 ft from each end of the bridge. ROW will not be acquired, per the RFP, if the 75 ft wide ROW is already provided for a minimum of 45 ft from the bridge ends. New ROW is anticipated as follows:

- S-56: ROW is required from Parcels 1, 2, 3 and 5 to meet SCDOT requirements. ROW is not required for construction.
- S-86: ROW is required at Parcel 2 only. This is required to provide the requested ditch behind approach guardrail. We will extend the present 75 ft ROW approximately 50 ft to cover the construction limits. *(Note: Steepened slopes will be investigated post award to potentially reduce ROW acquisition.)*

- S-106: ROW is required from sites 1, 2, 9 and 10 to meet SCDOT requirements above. ROW limits will be adjusted to avoid driveways and/or to eliminate takes in septic drainage fields as necessary. ROW is not required for construction.
- S-138: ROW is required from Parcels 1 and 2 to meet SCDOT requirements above. ROW is not required for construction.
- S-226: ROW is required from Parcels 2 and 3 due to construction limits. Steepened slopes with erosion control measures will be investigated with SCDOT post award to reduce these impacts.

We propose to use compressed shoulders and steepened slopes leading up to the bridge at S-226 to limit ROW acquisition. The same can be used at S-138 to be able to go to work early (if there are any issues with obtaining ROW and to avoid condemnation). S-86 will utilize a short retaining wall to avoid ROW acquisition in the National Park, while providing the requested special ditch behind the guardrail. This wall could be eliminated if the trailing end guardrail from the bridge is removed. Geotechnical analysis will be performed to ensure slope stability on multiple steepened slopes.

Proposed Submittal Process: CTEA will produce each design submittal as a final design package for review by SCDOT. Our team is familiar with SCDOT policies, file naming conventions, report formats, and design plan requirements. We will produce quality deliverables that meet the requirements of the RFP as well as the intent of SCDOT in a format SCDOT expects, which will facilitate quick design reviews and expedite approvals. This same team successfully worked in a collaborative environment utilizing Bluebeam on EBP 2018-2A DB and improved on that success with EBP 2020-1 DB, where we received few comments and were able to achieve RFC plans very quickly. **Our goal on this project will be to have no comments on the plan submittals.**

CTEA will submit detour signage plans for SCDOT approval prior to NTP. We will provide a design QC plan, draft design deliverable schedule and Community and Public Relations Plan to SCDOT at the Preconstruction Meeting. To have PII under construction as soon as possible and meet the requirements of

the RFP, CTEA will submit both S-138 and S-56 at NTP. We believe we can go to construction at S-56 earlier than S-138 due to utility delays. We will however continue to push S-138 as required by the RFP. CTEA will follow the schedule shown for S-138 for all bridges. The initial submission date for each

bridge is shown, with RFC drawings expected 30 days after.

The first submission (Final Submittal) will include the Final Roadway and Bridge Plans, Final Hydraulic Report, Final Geotech Report and any additional information necessary for SCDOT to perform a thorough review.

NTP	Final Submittal S-138
NTP + 15 days	SCDOT Review
NTP + 20 days	Response & Corrections
NTP + 25 days	5 Day SCDOT Review
NTP + 30 days	RFC Plan Set
NTP	Final Submittal S-56
NTP + 30 days	Final Submittal S-106
NTP + 60 days	Final Submittal S-86
NTP + 90 days	Final Submittal S-226

Post RFC, CTEA will review and approve shop drawings, and submit construction submittals to SCDOT for pile installation plans and approved shop drawings. CTEA will assemble this information as well as any plan markups during construction, and ROW markers and deliver As-Built Plans to SCDOT prior to Final Completion of the Project.

Our design submittal schedule allows PII to go to construction at the first site (S-56) approximately 2 months after NTP and provides over a year to complete the final structure with RFC Plans in hand. Early plan approval and shop drawing approval facilitates fabrication of precast elements and minimizes issues with material delivery. The design submittals are ordered above how PII anticipates building each site. Our priorities are to complete S-138 as soon as possible, replace currently closed bridges second and close and replace existing open bridges last, as well as complete the project ahead of schedule. This maximizes public benefit of actually using these roadways.

4.1.5 INNOVATION AND ADDED VALUE

The PII – CTEA Team recognizes these bridge replacements are simple, and therefore do not offer significant innovation opportunity. However, we have incorporated into our plans ideas to reduce construction timeframes and temporary works (*Ability to meet project schedule goals including milestone schedule dates*), reduce ROW acquisition (*Minimize impacts to SCDOT right of way acquisition costs*), avoid utility relocations (*Avoid or minimize impacts to utilities*), and provide SCDOT with a better, safer product. Additionally, we believe quality can be added to the project through commitments by the contractor to simply be a good neighbor. A complete list of our quality added ideas is included in Appendix B (Item J.) and summarized below:

- PII is committed to completing these bridge replacements ahead of schedule. We will prioritize S-138 as requested by SCDOT in the Project Goals but recognize the utility relocation at this site could delay our start. We will therefore deliver two sites to SCDOT for review immediately upon NTP: S-138 to comply with the RFP and to get this work moving, and S-56 to get construction started as soon as possible.
- Each site will be delivered aggressively, beating the schedule for ICT as well as obtaining substantial completion early.
- Utilities attached to the bridges will be relocated, temporarily or permanently. With the exception of the waterline at S-138, all utilities will relocate at their own expense. We should not impact any additional utilities (including the waterline at S-56) that SCDOT will need to compensate.
- Acquired ROW at S-56, S-106 and S-138 is the minimum required by SCDOT. We will discuss slight modifications to these limits with SCDOT post award to avoid the driveway and potential septic drainage fields at S-106. There is no difference in cost or schedule to the PII Team to make these betterments.

- ROW at S-86 and S-226 may be reduced by steepening cut slopes. CTEA does not believe the steepened slopes allowed in the RFP were considered for cut slopes, and therefore showed a 2:1 standard cut in our plans. We will investigate the stability of these slopes (currently standing at steeper than 2:1) and reduce ROW at these sites as allowed by SCDOT post award.
- The CTEA design at each site minimizes the profile, reduces earthwork, reduces hauling, and efficiently replaces each bridge. Our design reduces the scope of work, reduces the construction costs, reduces the environmental impacts, and accelerates the projects. We also keep a critical eye on the surrounding parcels at each site and facilitate plans to minimize impacts to the area.
- The PII – CTEA Team will exceed the DBE requirements of this contract. We will provide 5 times the required professional services goal and may double the stated goal of 11.6% for the contract. **We will exceed the total goal by 50% or more.**

Appendix A.1 Roadway Plans
SCDOT Design Build Project
Bridge Package 14 DB Project
Contract ID 1162220



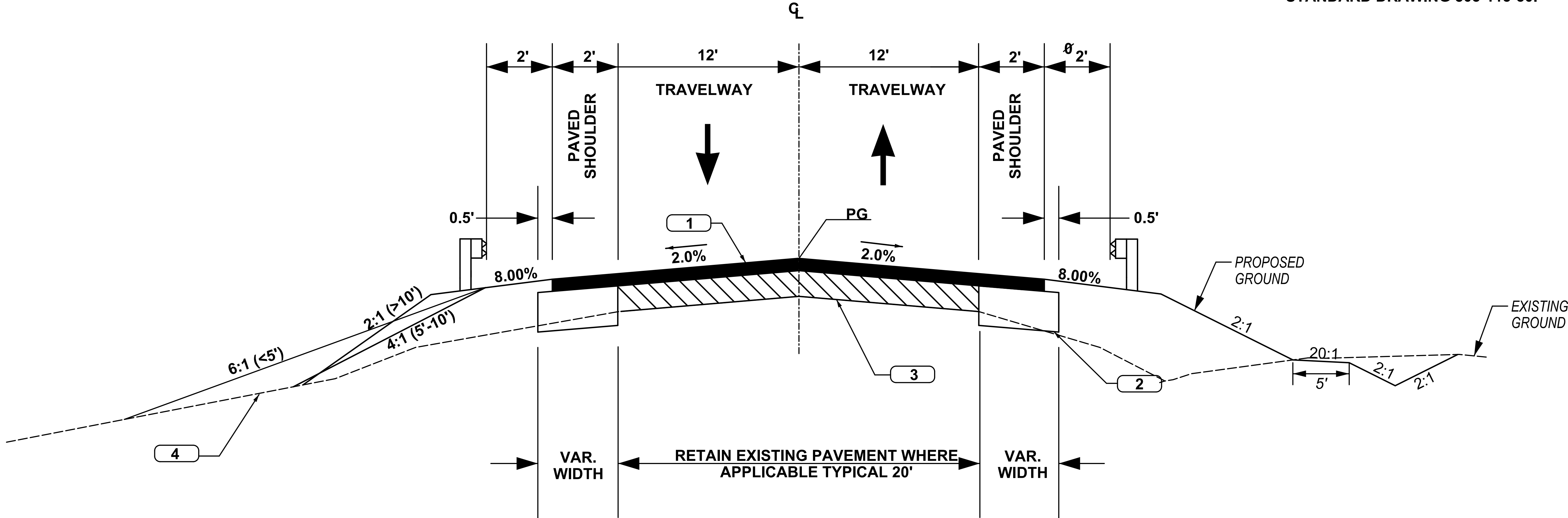
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TYPICAL SECTION OF IMPROVEMENT
SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION
COLUMBIA, S.C.

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROADROUTE NO.	SHEET NO.
3	S.C.	CHEROKEE	1162220	S-56	3

MONTGOMERY DRIVE
BRIDGE OVER HORSE CREEK


NOTES:
Ø WHERE CLEARZONE IS UNATTAINABLE OR END TREATMENT IS REQUIRED FOR BRIDGE APPROACH, ADD 3.75' TO SHOULDER FOR GUARDRAIL AND 2:1 FORESLOPE. ADDITIONAL SHOULDER WIDTH REQUIRED FOR END TREATMENT TYPE "TL2" SEE SCDOT STANDARD DRAWING 805-115-50.



USE THIS SECTION RD. S-56 (MONTGOMERY DRIVE)
BRIDGE OVER HORSE CREEK
STA. 32+60.00 TO STA. 35+50.00

EXCEPTION 50'X36' BRIDGE
FROM STA. 34+03.00 TO 34+53.00

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2	HMA BASE TYPE A 450 #/S.Y.
3	RETAIN EXISTING PAVEMENT
4	EXISTING GROUND

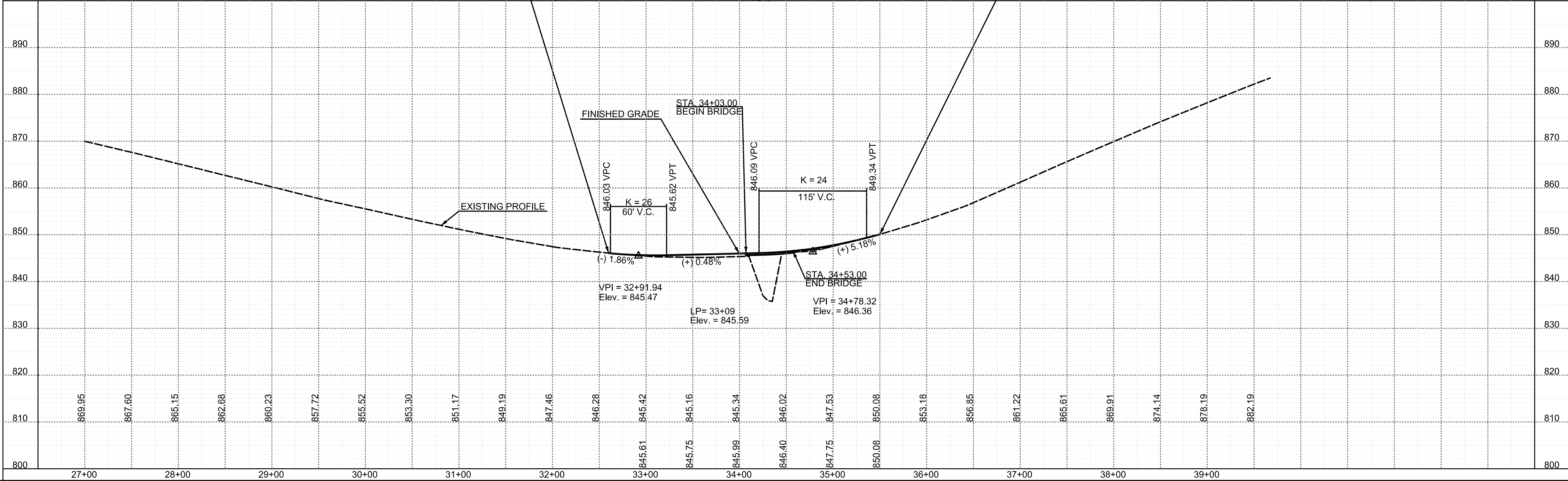
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			EXCEPTIONS TO DESIGN SPEED						TYPICAL SECTION			
									SCALE 1"V= SCALE 1"H= RTE./RD.			

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD NO.	SHEET NO.
3	S.C.	CHEROKEE	1162220	S-56	6

$P.I. = 34 + 78.01$
 $\Delta = 2^\circ 03' 56'' (RT)$
 $D = 1^\circ 00' 00''$
 $T = 103.30'$
 $L = 206.57'$
 $E = 0.93'$
 $R = 5,729.64'$
 $D.S. = 35 \text{ MPH}$
 $eMAX = 6\%$
 $e = NC$
 $P.C. - LG\% = 0.62\%$
 $P.T. - LG\% = 0.62\%$

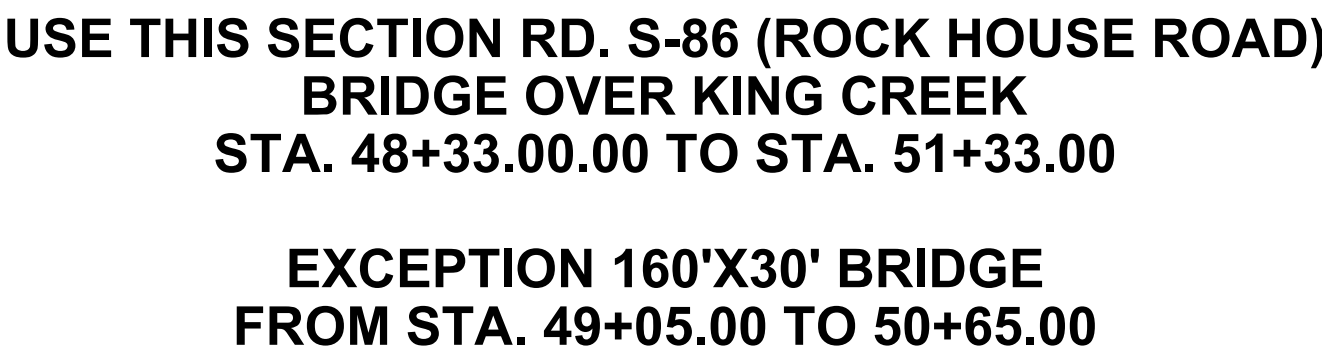
NOT FOR
CONSTRUCTION






CAROLINA
Transportation
Engineers &
Assoc., PC



FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD/ROUTE NO.	SHEET NO.
3	S.C.	CHEROKEE	1162220	S-86	3

NOTES:
 0 WHERE CLEARZONE IS UNATTAINABLE OR END
 TREATMENT IS REQUIRED FOR BRIDGE APPROACH,
 ADD 3.75' TO SHOULDER FOR GUARDRAIL AND 2:1
 FORESLOPE. ADDITIONAL SHOULDER WIDTH REQUIRED
 FOR END TREATMENT TYPE "TL3" SEE SCDOT
 STANDARD DRAWING 805-115-50.



1		HMA SURFACE COURSE TYPE C 150 #/S.Y.
2		HMA SURFACE COURSE TYPE C 175 #/S.Y.
3		HMA BASE TYPE A 600 #/S.Y.
4		RETAIN EXISTING PAVEMENT
5		EXISTING GROUND

CAROLINA
Transportation
Engineers &
Assoc., PC

PAVEMENT DESIGN

RURAL LOCAL GROUP 4		
RTE. S-86	DESIGN SPEED	
MPH	FROM STA.	TO STA.
50	48+33.00	51+33.00
EXCEPTIONS TO DESIGN SPEED		

SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION
ROAD DESIGN COLUMBIA, S.C

TYPICAL SECTION

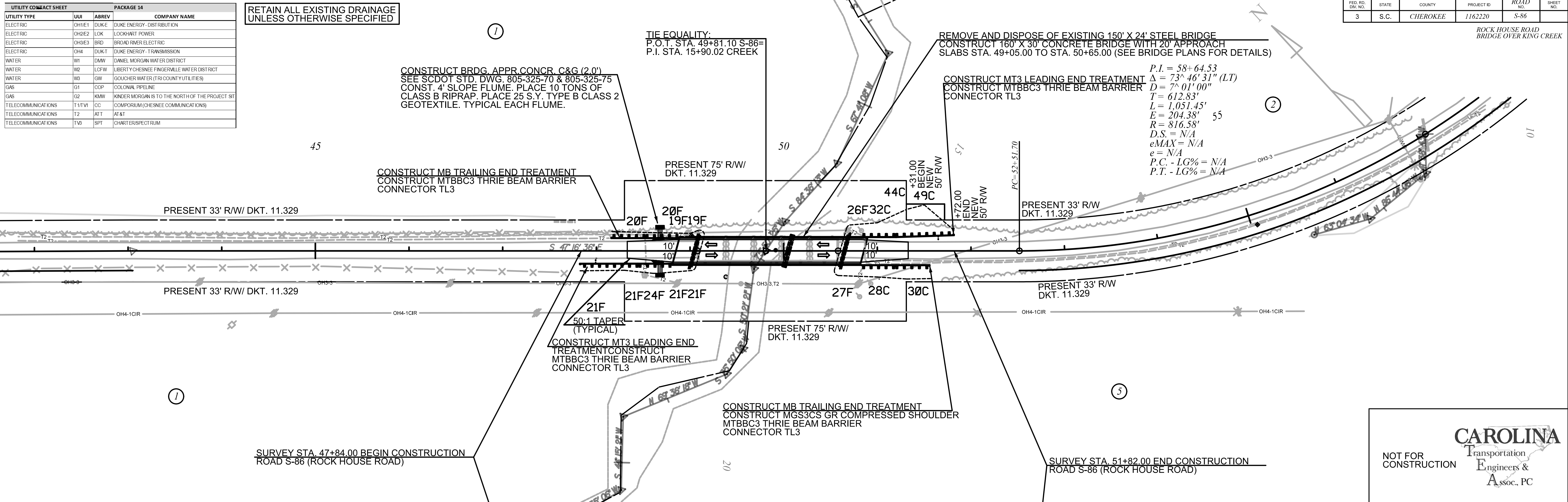
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100	100	100

UTILITY CONTACT SHEET			PACKAGE 14		
UTILITY TYPE	UUI	ABREV	COMPANY NAME		
ELECTRIC	OH1E1	DUKE	DUKE ENERGY- DISTRIBUTION		
ELECTRIC	OH2E2	LOK	LOCKHART POWER		
ELECTRIC	OH3E3	BRD	BROAD RIVER ELECTRIC		
ELECTRIC	OH4	DUK-T	DUKE ENERGY- TRANSMISSION		
WATER	W1	DMW	DANIEL MORGAN WATER DISTRICT		
WATER	W2	LCFW	LIBERTY CHESNEE FINGERVILLE WATER DISTRICT		
WATER	W3	GW	GOUCHER WATER (TRI COUNTY UTILITIES)		
GAS	G1	COP	COLONIAL PIPELINE		
GAS	G2	KMW	KINDER MORGAN IS TO THE NORTH OF THE PROJECT SITE		
TELECOMMUNICATIONS	T1/TV1	CC	COMPUORIUM (CHESNEE COMMUNICATIONS)		
TELECOMMUNICATIONS	T2	AT T	AT & T		
TELECOMMUNICATIONS	TV3	SPT	CHARTER SPECTRUM		

RETAIN ALL EXISTING DRAINAGE
UNLESS OTHERWISE SPECIFIED

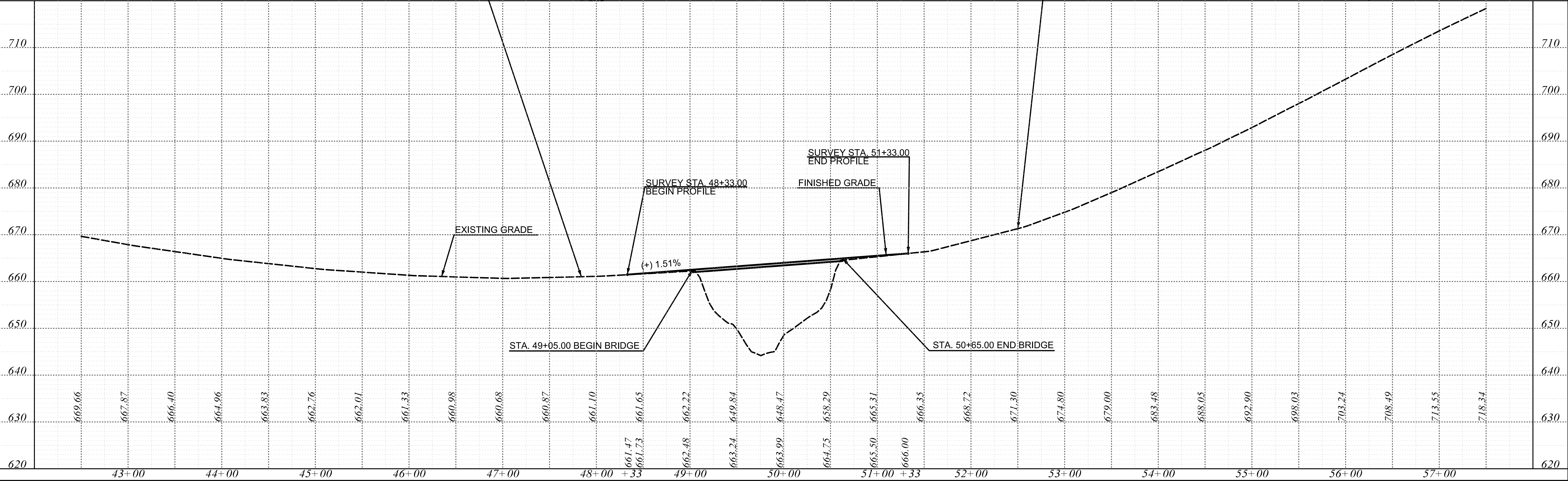
FED. RD. DW. NO.	STATE	COUNTY	PROJECT ID	ROAD NO.	SHEET NO.
3	S.C.	CHEROKEE	1162220	S-86	

ROCK HOUSE ROAD
BRIDGE OVER KING CREEK



NOT FOR
CONSTRUCTION

CAROLINA
Transportation
Engineers &
Assoc., PC



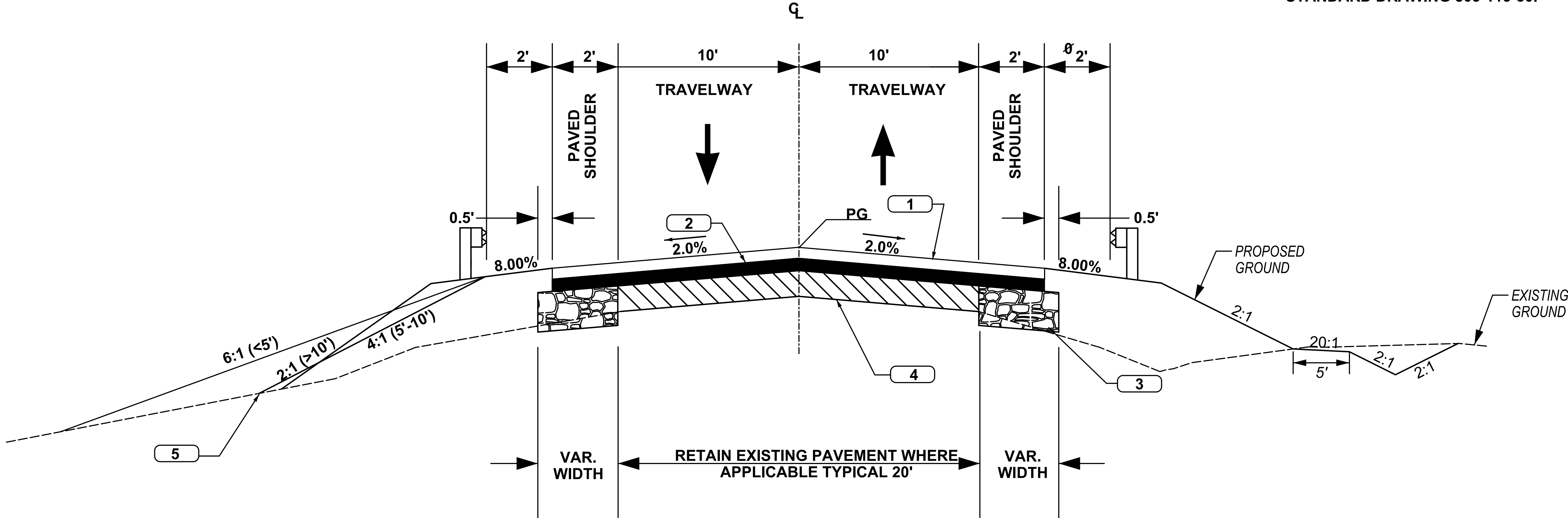
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TYPICAL SECTION OF IMPROVEMENT
SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION
COLUMBIA, S.C.

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD/ROUTE NO.	SHEET NO.
3	S.C.	CHEROKEE	1162220	S-106	3

ISLAND CREEK ROAD
BRIDGE OVER SUCK CREEK


NOTES:
Ø WHERE CLEARZONE IS UNATTAINABLE OR END TREATMENT IS REQUIRED FOR BRIDGE APPROACH, ADD 3.75' TO SHOULDER FOR GUARDRAIL AND 2:1 FORESLOPE. ADDITIONAL SHOULDER WIDTH REQUIRED FOR END TREATMENT TYPE "TL2" SEE SCDOT STANDARD DRAWING 805-115-50.



USE THIS SECTION RD. S-106 (ISLAND CREEK ROAD)
BRIDGE OVER SUCK CREEK
STA. 31+06.00 TO STA. 33+09.00

EXCEPTION 60'X33' BRIDGE
FROM STA. 31+70.00 TO 32+30.00

1		HMA SURFACE COURSE TYPE C 150 #/S.Y.
2		HMA SURFACE COURSE TYPE C 175 #/S.Y.
3		HMA BASE TYPE A 600 #/S.Y.
4		RETAIN EXISTING PAVEMENT
5		EXISTING GROUND

NOT FOR CONSTRUCTION		PAVEMENT DESIGN	RTE S-106			DESIGN SPEED			SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION ROAD DESIGN COLUMBIA, S.C.		
			MPH		FROM STA.	TO STA.					
			35		31+06.00	33+09.00					
									TYPICAL SECTION		
			EXCEPTIONS TO DESIGN SPEED								
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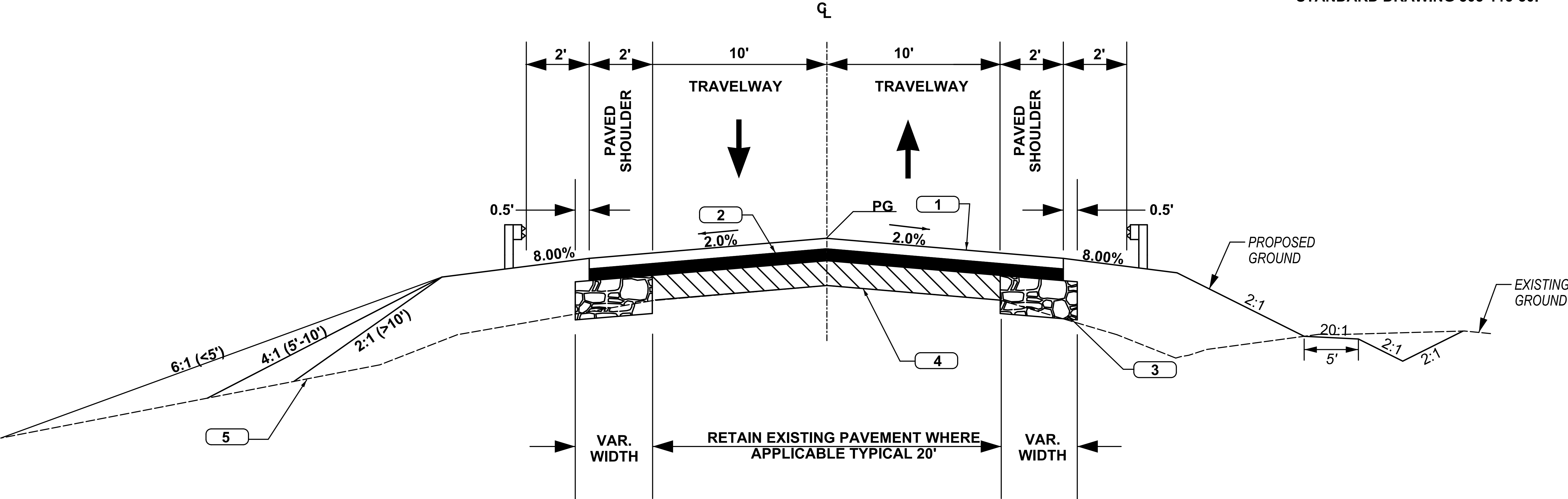
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TYPICAL SECTION OF IMPROVEMENT
SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION
COLUMBIA, S.C.

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD/ROUTE NO.	SHEET NO.
3	S.C.	CHEROKEE	1162220	S-138	3

GOUCHER SCHOOL ROAD
BRIDGE OVER GOUCHER CREEK


NOTES:
Ø WHERE CLEARZONE IS UNATTAINABLE OR END TREATMENT IS REQUIRED FOR BRIDGE APPROACH, ADD 3.75' TO SHOULDER FOR GUARDRAIL AND 2:1 FORESLOPE. ADDITIONAL SHOULDER WIDTH REQUIRED FOR END TREATMENT TYPE "TL3" SEE SCDOT STANDARD DRAWING 805-115-50.



USE THIS SECTION RD. S-138 (GOUCHER SCHOOL ROAD)
BRIDGE GOUCHER CREEK
STA. 87+28.00 TO STA. 90+56.00

EXCEPTION 130'X30' BRIDGE
FROM STA. 88+33.00 TO 89+53.00

1		HMA SURFACE COURSE TYPE C 150 #/S.Y.
2		HMA SURFACE COURSE TYPE C 175 #/S.Y.
3		HMA BASE TYPE A 600 #/S.Y.
4		RETAIN EXISTING PAVEMENT
5		EXISTING GROUND

NOT FOR CONSTRUCTION		PAVEMENT DESIGN	RTE S-138			DESIGN SPEED			SOUTH CAROLINA			
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			50			87+28.00		90+56.00		ROAD DESIGN COLUMBIA, S.C.		
			EXCEPTIONS TO DESIGN SPEED						TYPICAL SECTION			
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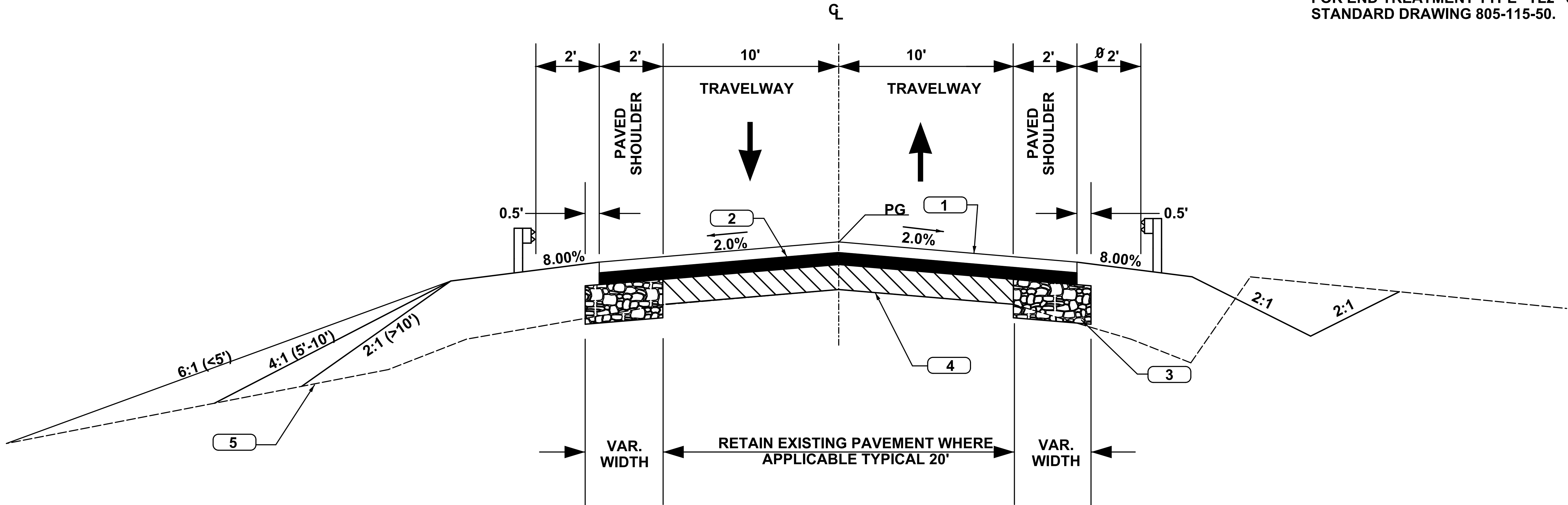
TYPICAL SECTION OF IMPROVEMENT
SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION
COLUMBIA, S.C.

FED. RD. DIV. NO.	STATE	COUNTY	PROJECT ID	ROAD/ROUTE NO.	SHEET NO.
3	S.C.	CHEROKEE	1162220	S-226	3

HAMMETT GROVE ROAD
BRIDGE OVER UNNAMED STREAM

NOTES:


Ø WHERE CLEARZONE IS UNATTAINABLE OR END TREATMENT IS REQUIRED FOR BRIDGE APPROACH, ADD 3.75' TO SHOULDER FOR GUARDRAIL AND 2:1 FORESLOPE. ADDITIONAL SHOULDER WIDTH REQUIRED FOR END TREATMENT TYPE "TL2" SEE SCDOT STANDARD DRAWING 805-115-50.



USE THIS SECTION RD. S-226 (HAMMETT GROVE ROAD)
BRIDGE UNNAMED STREAM
STA. 158+50.00 TO STA. 167+00.00

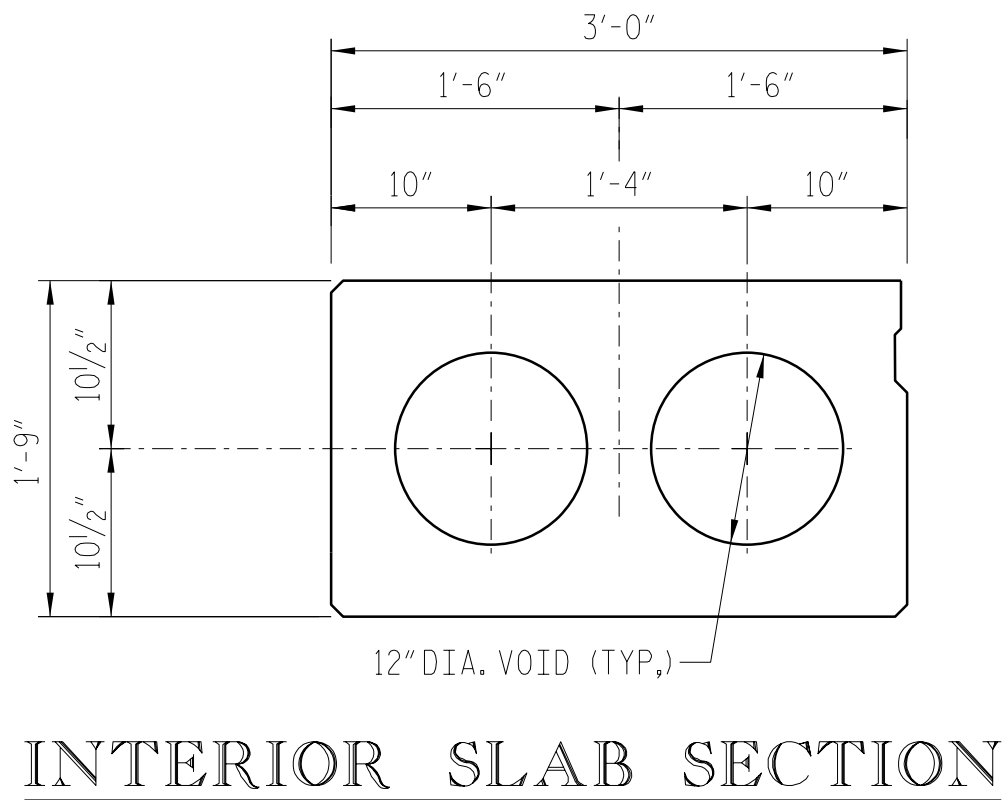
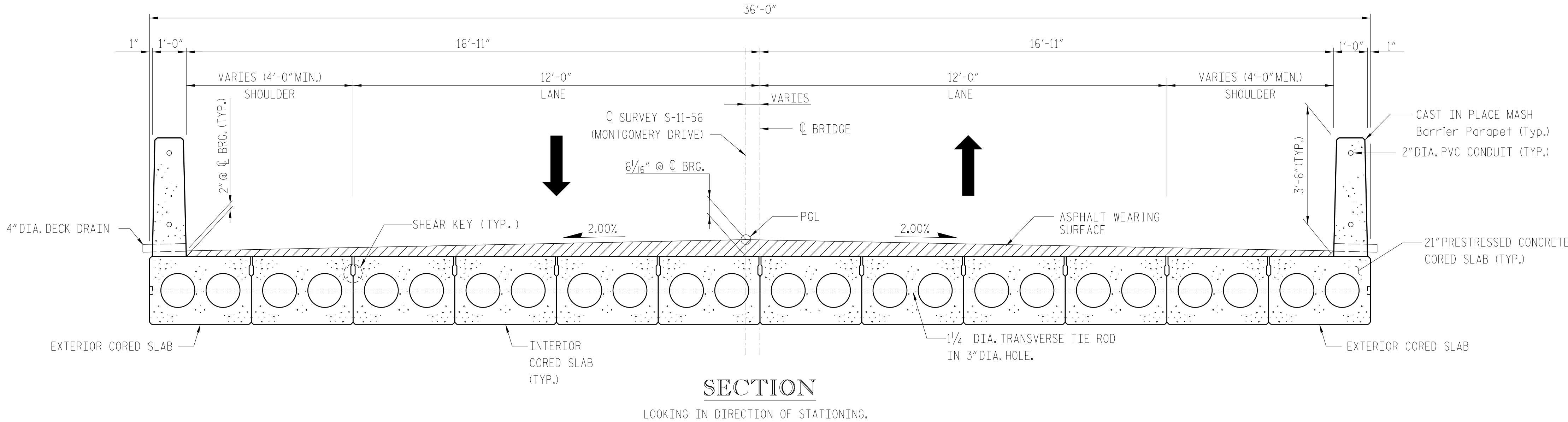
EXCEPTION 80'X30' BRIDGE
FROM STA. 161+93.00 TO 162+73.00

1	HMA SURFACE COURSE TYPE C 150 #/S.Y.
2	HMA SURFACE COURSE TYPE C 175 #/S.Y.
3	HMA BASE TYPE A 600 #/S.Y.
4	RETAIN EXISTING PAVEMENT
5	EXISTING GROUND

NOT FOR CONSTRUCTION		PAVEMENT DESIGN	RTE S-226 DESIGN SPEED			SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION ROAD DESIGN COLUMBIA, S.C.		
			MPH	FROM STA.	TO STA.			
			35	158+50.00	167+00.00			
			EXCEPTIONS TO DESIGN SPEED			TYPICAL SECTION		
			SCALE 1"V= SCALE 1"H= RTE./RD.					

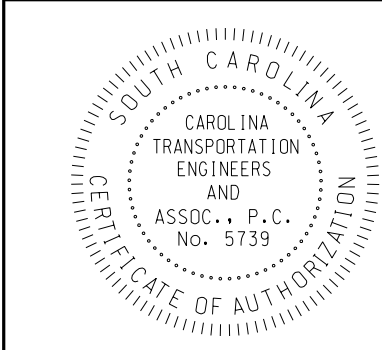
Appendix A.2 Bridge Plans
SCDOT Design Build Project
Bridge Package 14 DB Project
Contract ID 1162220





CAROLINA
Transportation
Engineers &
Assoc., PC

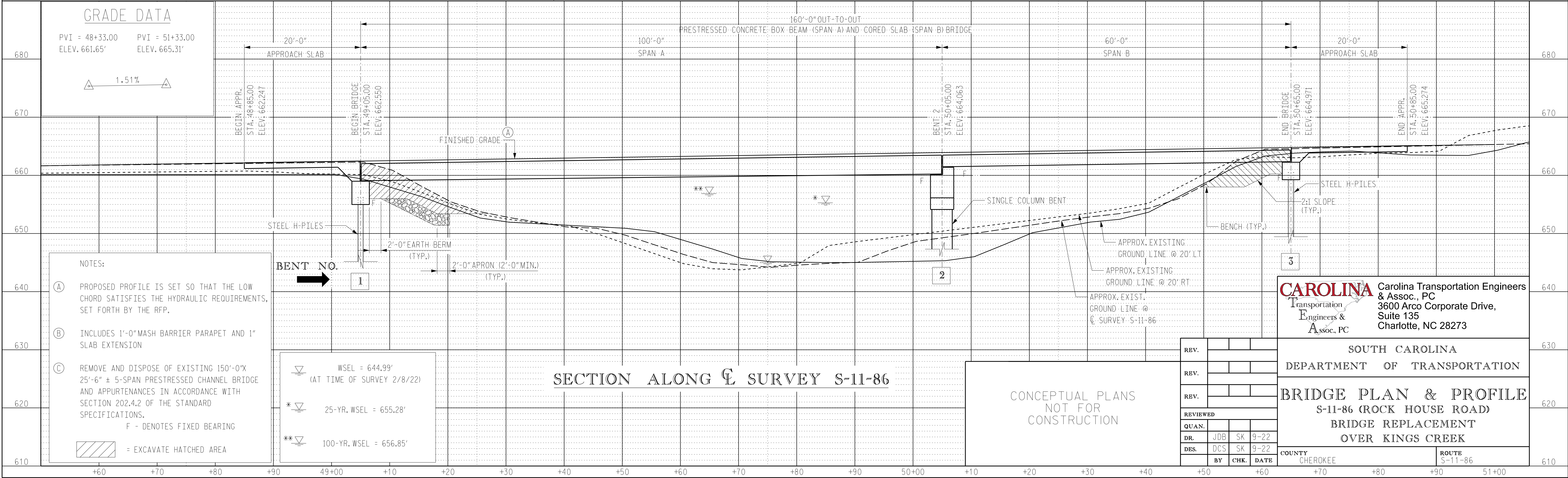
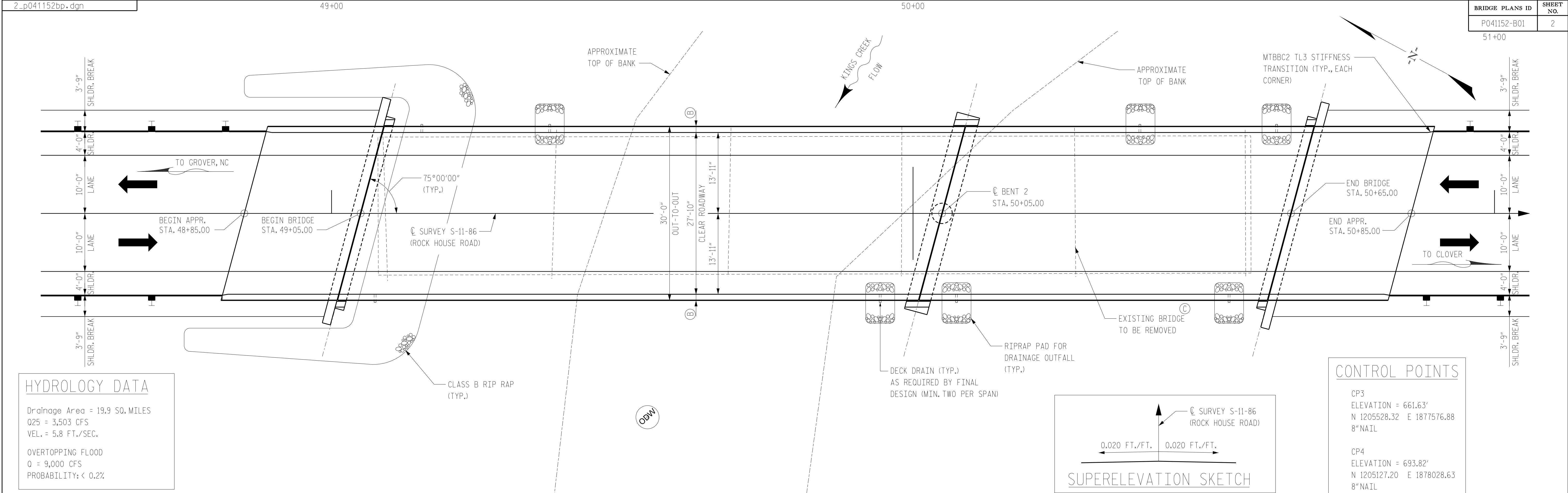
Carolina Transportation Engineers
& Assoc., PC
3600 Arco Corporate Drive,
Suite 135
Charlotte, NC 28273

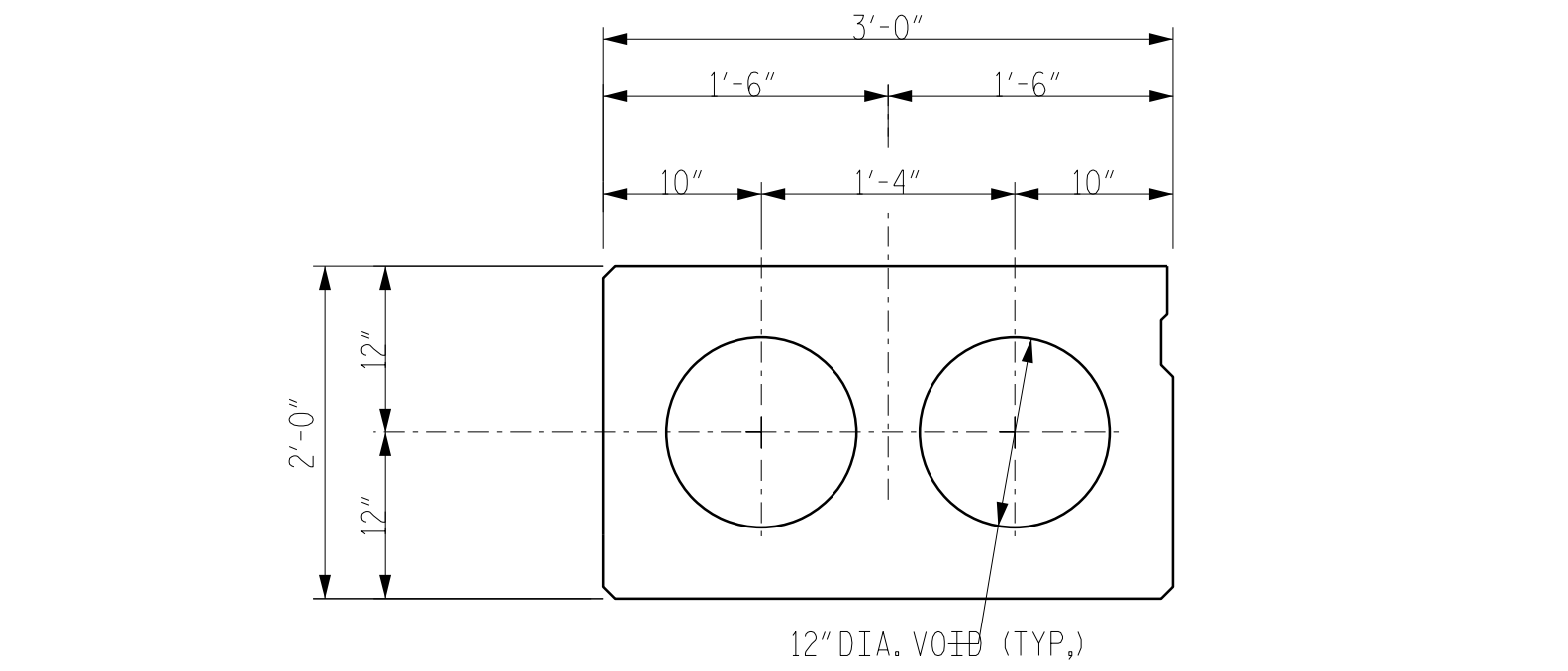
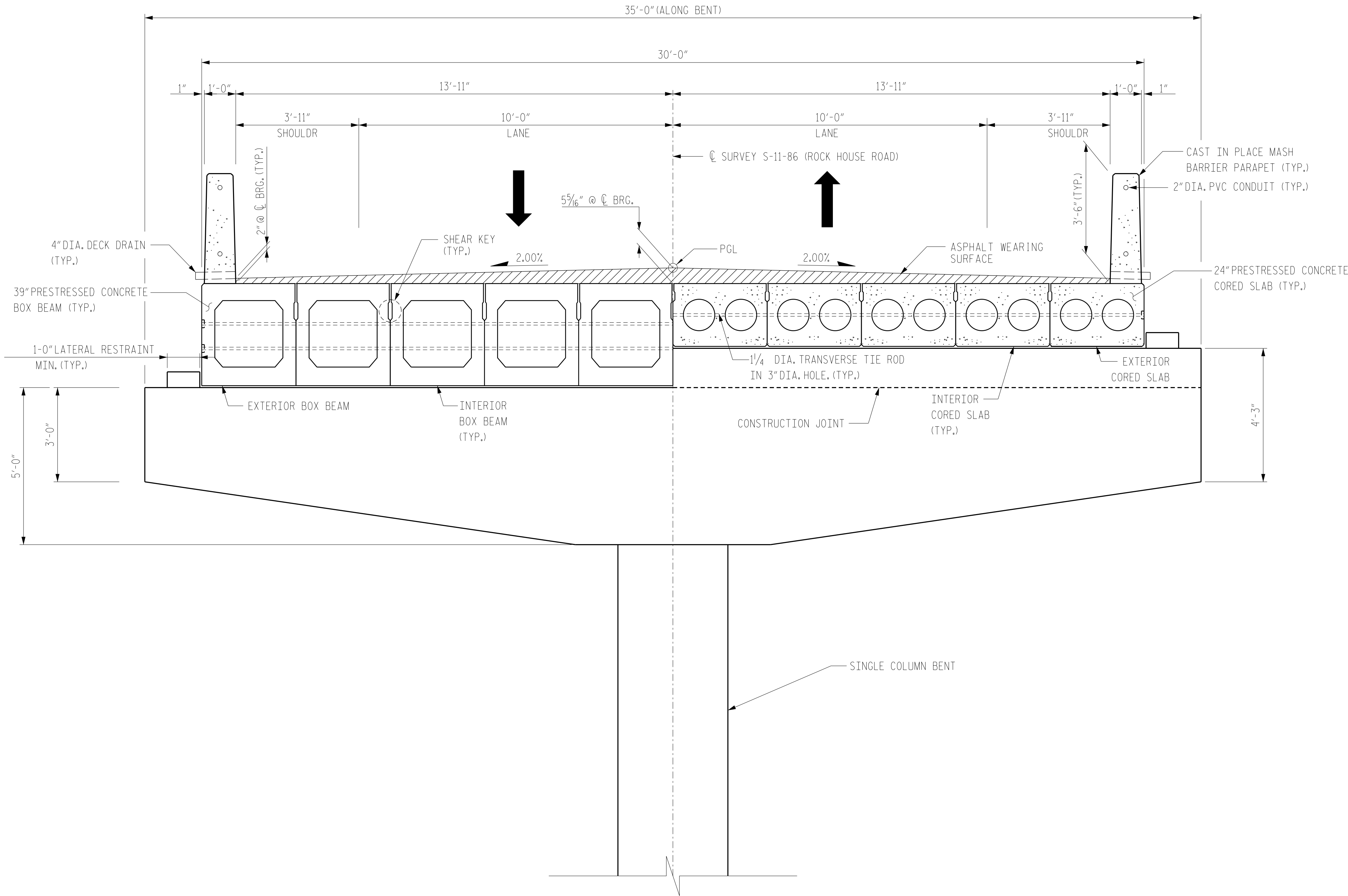


CONCEPTUAL
PLANS
NOT FOR
CONSTRUCTION

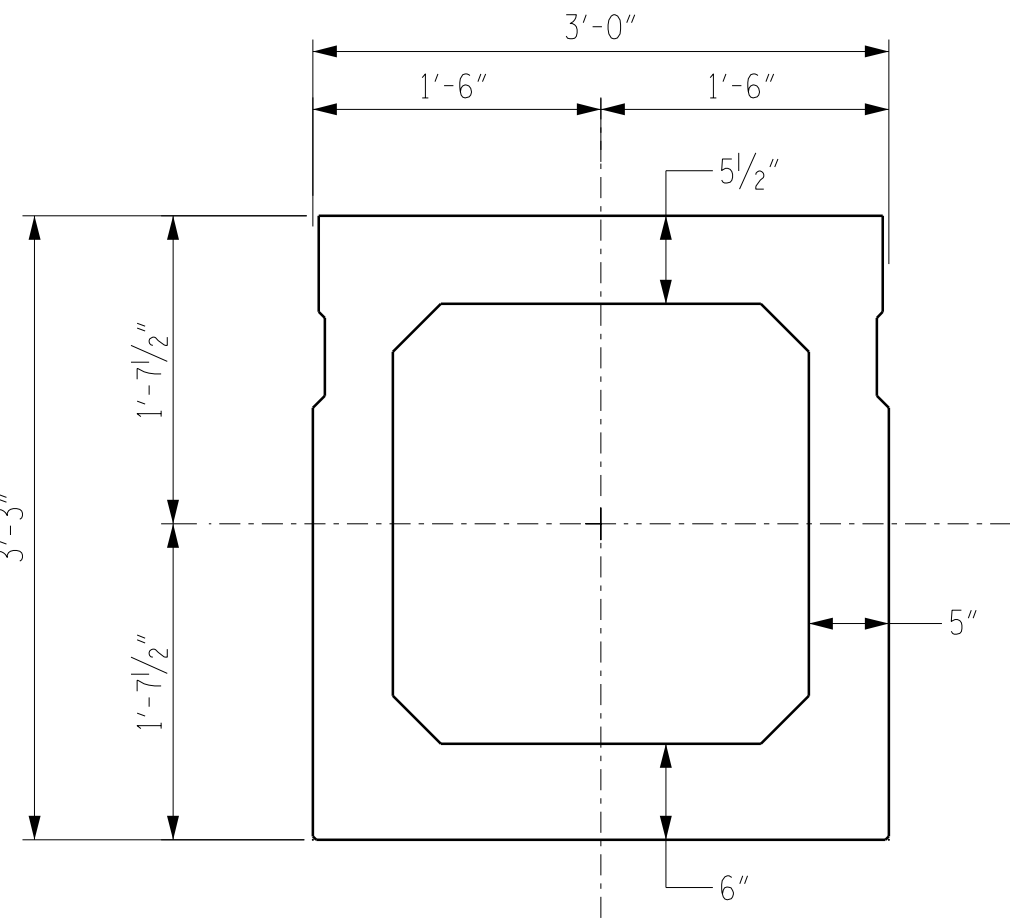
REV.			
REV.			
REV.			
REVIEWED XXX			
QUAN.			
DR.	JDB	SK	9-22
DES.	DCS	SK	9-22
	BY	CHK.	DATE

SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION	
BRIDGE TYPICAL SECTION S-11-56 (MONTGOMERY DRIVE) BRIDGE REPLACEMENT OVER HORSE CREEK	
COUNTY CHEROKEE	ROUTE S-11-56





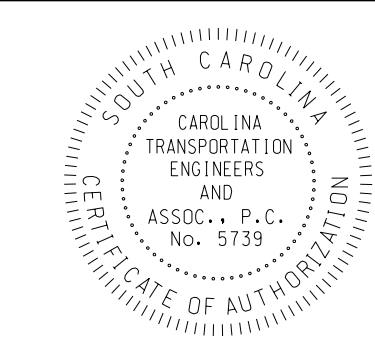
INTERIOR SLAB SECTION - CORED SLAB



INTERIOR SLAB SECTION - BOX BEAM

SECTION-100'-0" MAIN SPAN
LOOKING IN DIRECTION OF STATIONING.

SECTION-60'-0" END SPAN
LOOKING IN DIRECTION OF STATIONING.



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PLANS
NOT FOR
CONSTRUCTION

REV.			
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REV.			
REVIEWED XXX			
QUAN.			
DR.	JDB	SK	9-22
DES.	DCS	SK	9-22
BY	CHK.	DATE	

CAROLINA Carolina Transportation Engineers & Assoc., PC
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Suite 135
Charlotte, NC 28273

SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION

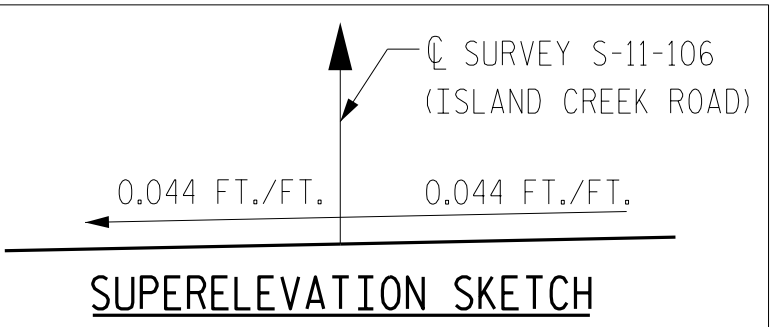
BRIDGE TYPICAL SECTION
S-11-86 (ROCK HOUSE ROAD)
BRIDGE REPLACEMENT
OVER KINGS CREEK

COUNTY CHEROKEE ROUTE S-11-86

- NOTES:
- (A) PROPOSED PROFILE IS SET SO THAT THE LOW CHORD SATISFIES THE HYDRAULIC REQUIREMENTS, SET FORTH BY THE RFP.
 - (B) INCLUDES 1'-0" MASH BARRIER PARAPET AND 1" SLAB EXTENSION
 - (C) REMOVE AND DISPOSE OF EXISTING 35'-0" X 25'-0" ± SINGLE SPAN STEEL BRIDGE AND APPURTENANCES IN ACCORDANCE WITH SECTION 202.4.2 OF THE STANDARD SPECIFICATIONS.

F - DENOTES FIXED BEARING

EXCAVATE HATCHED AREA



CONTROL POINTS

CP2
ELEVATION = 923.86'
N 1199167.28 E 1749440.99
RBR W/ CAP

CP3
ELEVATION = 924.28'
N 1197601.29 E 1752498.29
RBR W/ CAP

HORIZONTAL CURVE DATA

P.I. = 32+63.77
 $\Delta = 38^\circ 57' 35''$ (LT)
Dc = 6° 11' 39"
T = 327.19'
L = 628.98'
E = 56.16'
R = 925.00''

VERTICAL CURVE DATA

PVI = 32+00.00
ELEV. 848.67'
V.C. = 188'

-1.13% Δ 3.20%

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Charlotte, NC 28273

SOUTH CAROLINA
DEPARTMENT OF TRANSPORTATION

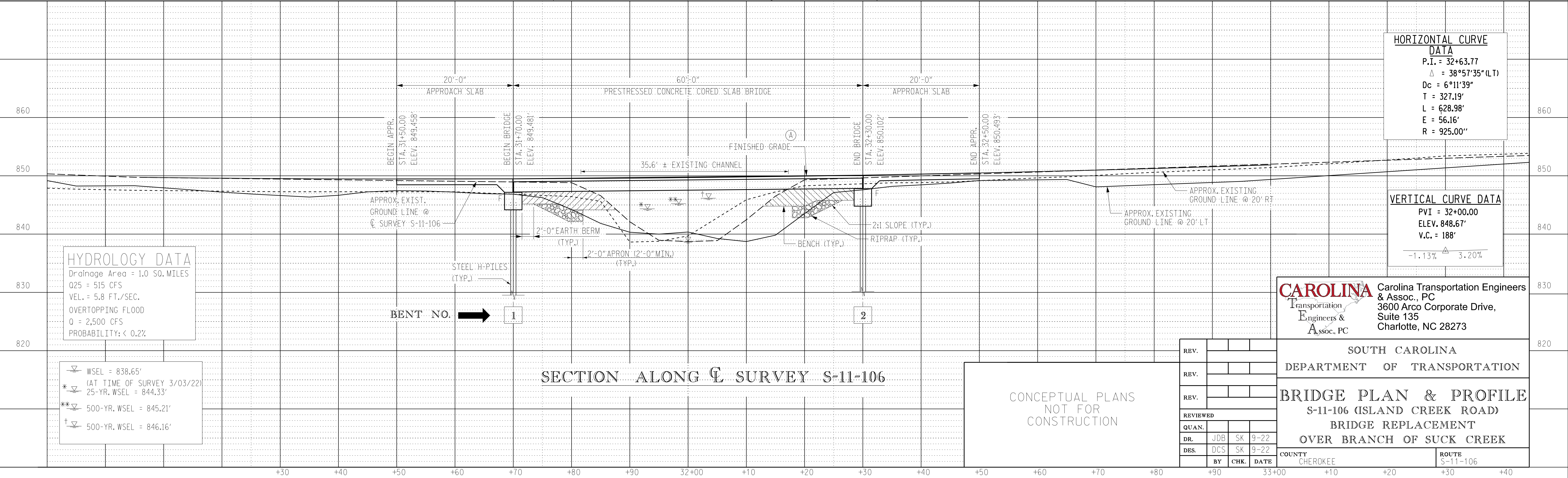
BRIDGE PLAN & PROFILE
S-11-106 (ISLAND CREEK ROAD)
BRIDGE REPLACEMENT
OVER BRANCH OF SUCK CREEK

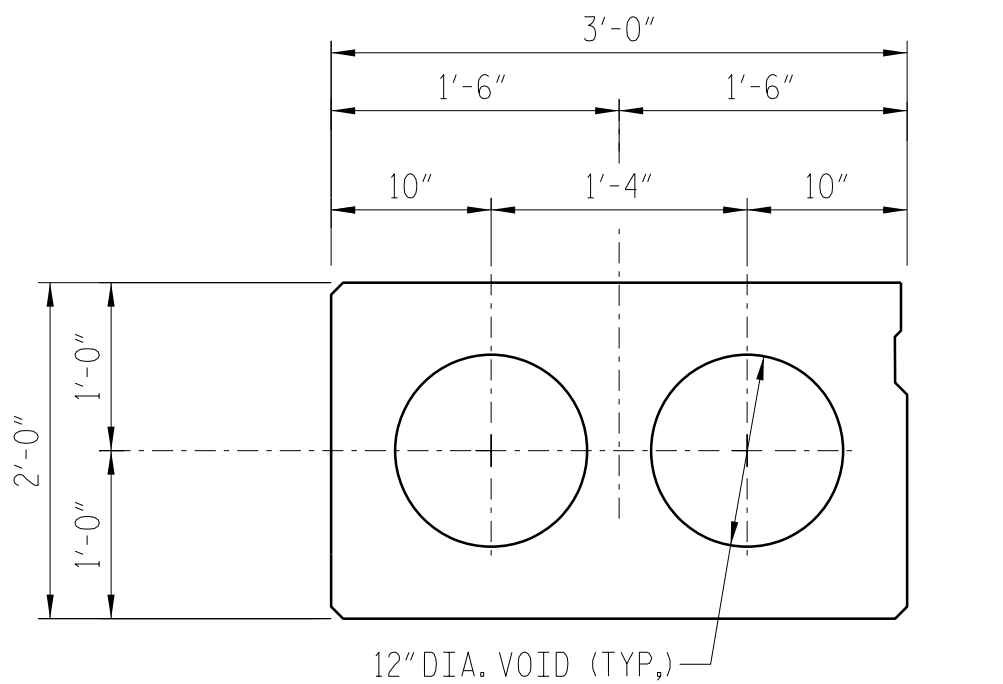
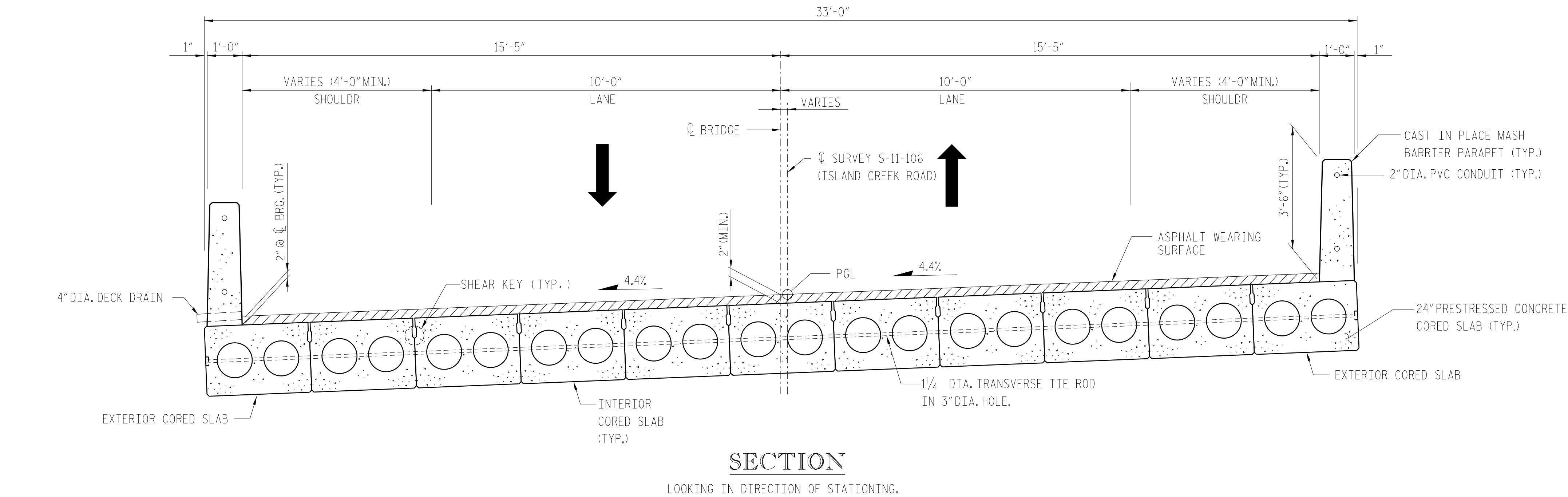
COUNTY CHEROKEE ROUTE S-11-106

REV.			
REV.			
REV.			
REVIEWED			
QUAN.			
DR.	JDB	SK	9-22
DES.	DCS	SK	9-22
BY	CHK.	DATE	

CONCEPTUAL PLANS
NOT FOR
CONSTRUCTION

SECTION ALONG C SURVEY S-11-106





CAROLINA

Transportation Engineers & Assoc., PC

Transportation Engineers & Assoc., PC

3600 Arco Corporate Drive, Suite 135 Charlotte, NC 28273

SOUTH CAROLINA

CAROLINA TRANSPORTATION ENGINEERS AND ASSOC., P.C.

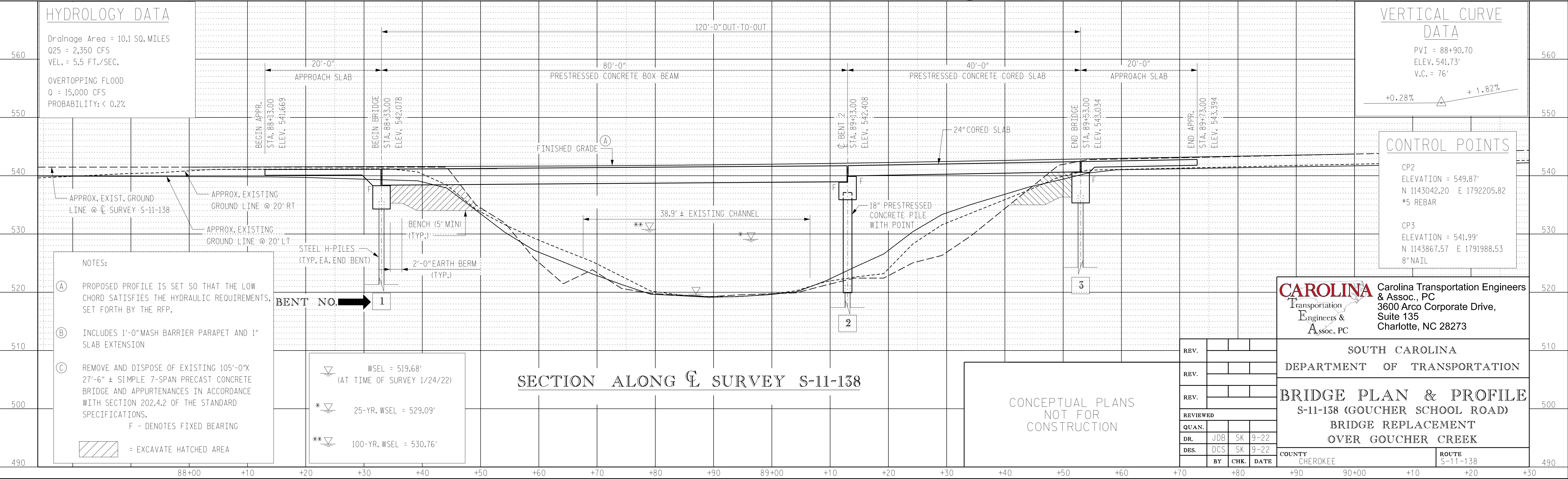
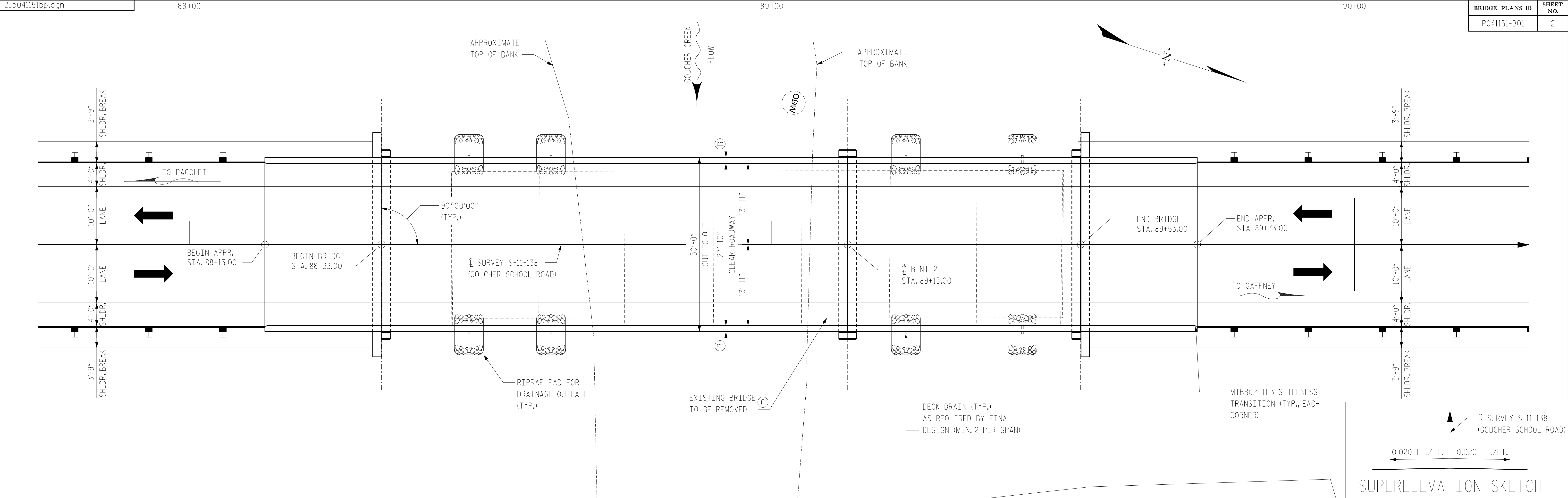
ASSOC., P.C. No. 5739

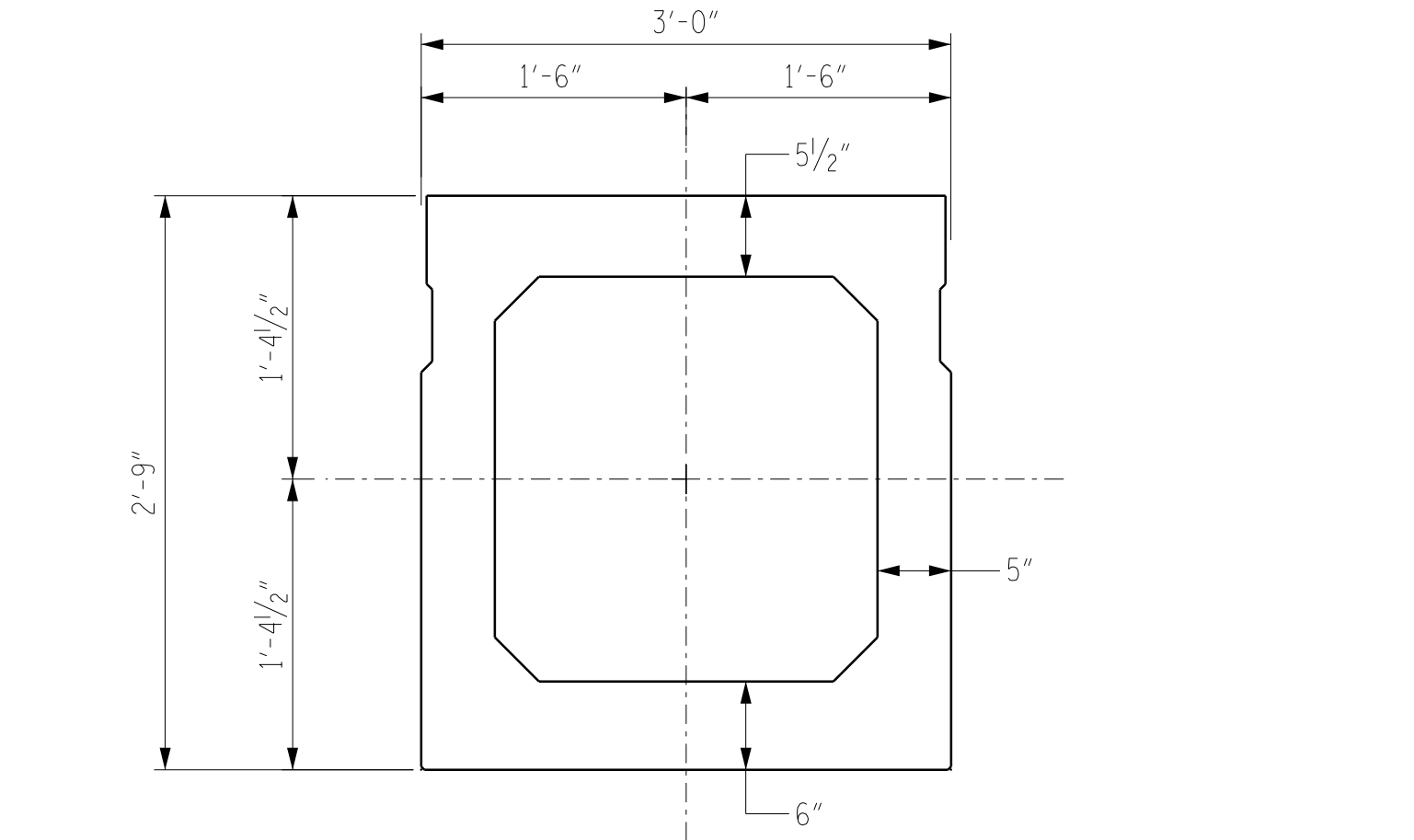
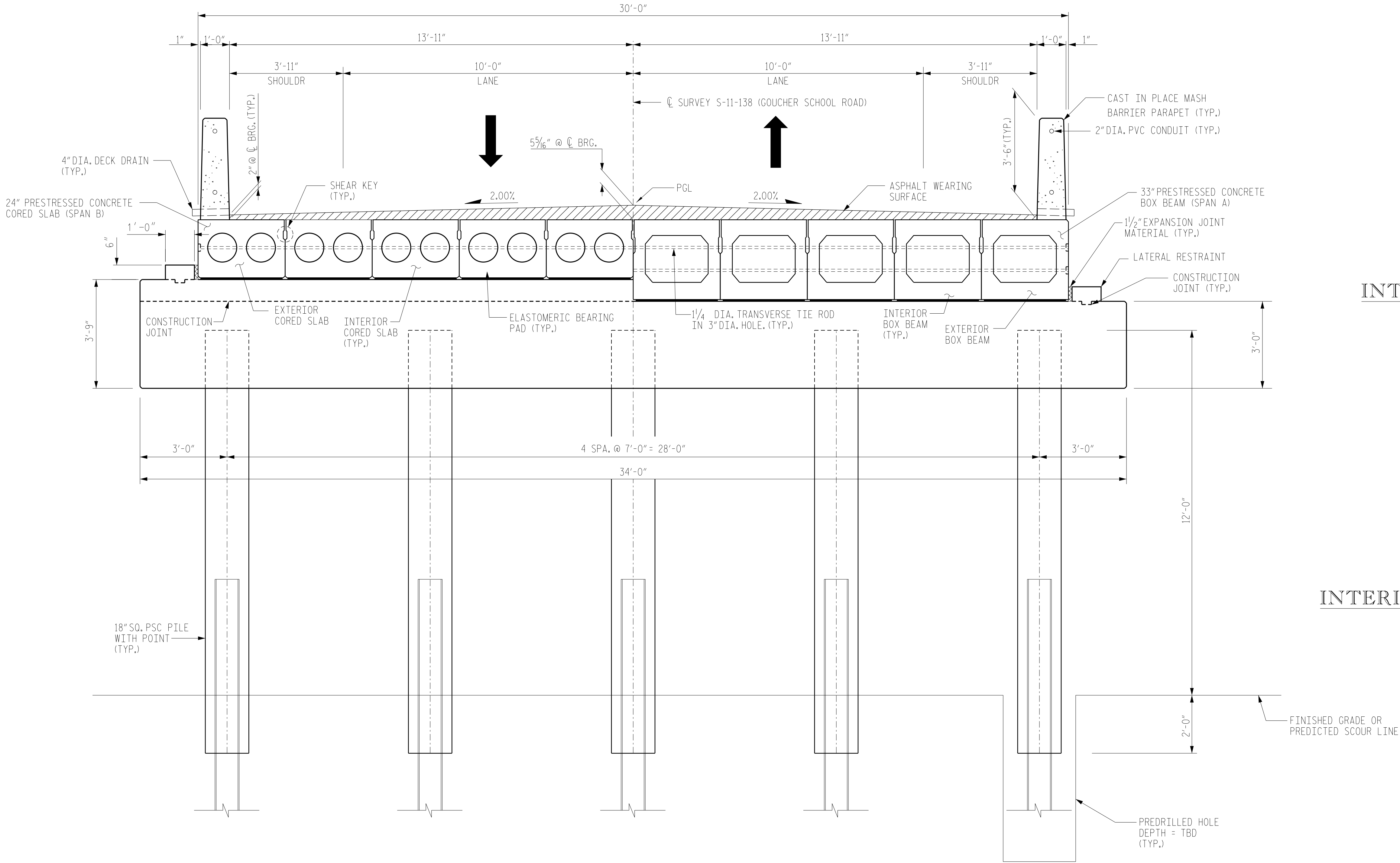
CERTIFICATE OF AUTHORIZATION

CONCEPTUAL PLANS NOT FOR CONSTRUCTION

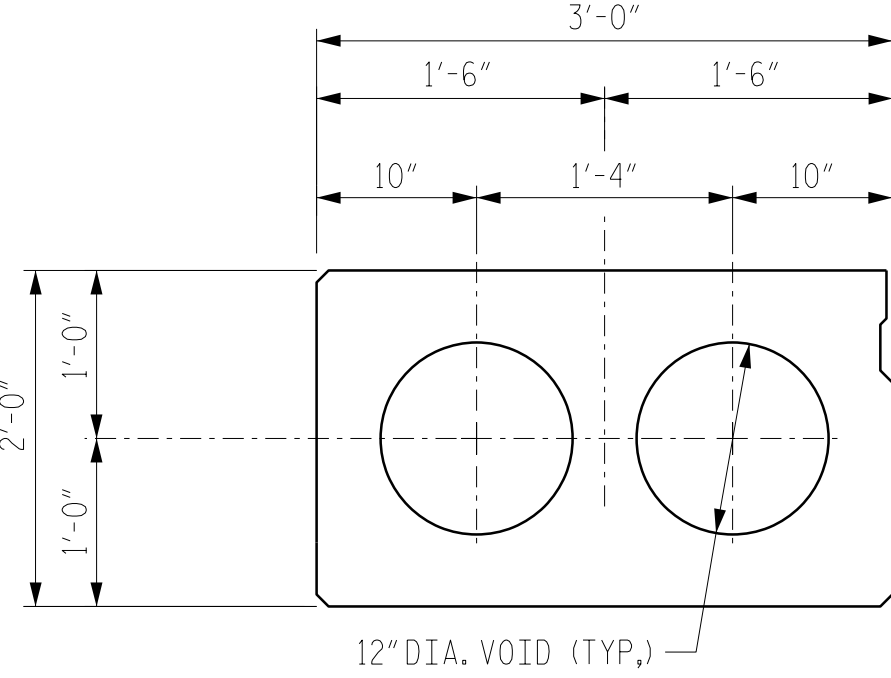
REV.			
REV.			
REV.			
REVIEWED XXX			
QUAN.			
DR.	JDB	SK	9-22
DES.	DCS	SK	9-22
BY	CHK.	DATE	

SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION	
BRIDGE TYPICAL SECTION	
S-11-106 (ISLAND CREEK ROAD)	
BRIDGE REPLACEMENT	
OVER BRANCH OF SUCK CREEK	
COUNTY CHEROKEE	ROUTE S-11-106



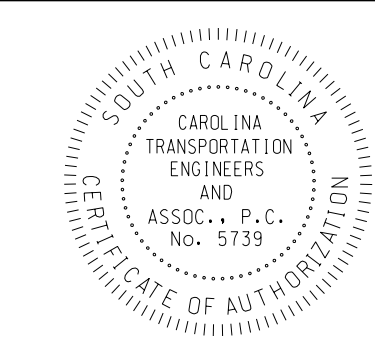


INTERIOR SLAB SECTION - BOX BEAM



INTERIOR SLAB SECTION - CORED SLAB

TYPICAL SECTION
LOOKING IN DIRECTION OF STATIONING.



CONCEPTUAL
PLANS
NOT FOR
CONSTRUCTION

REV.			
REV.			
REV.			
REVIEWED XXX			
QUAN.			
DR.	JDB	SK	9-22
DES.	DCS	SK	9-22
BY	CHK.	DATE	

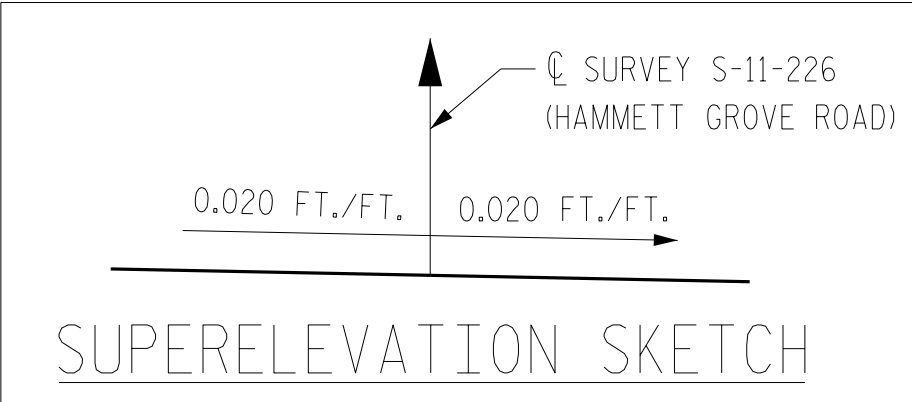
CAROLINA Carolina Transportation Engineers & Assoc., PC
Transportation Engineers & Assoc., PC
3600 Arco Corporate Drive,
Suite 135
Charlotte, NC 28273

SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION	
BRIDGE TYPICAL SECTION S-11-138 (GOUCHER SCHOOL ROAD) BRIDGE REPLACEMENT OVER GOUCHER CREEK	
COUNTY CHEROKEE	ROUTE S-11-138

NOTES:

- (A) PROPOSED PROFILE IS SET SO THAT THE LOW CHORD SATISFIES THE HYDRAULIC REQUIREMENTS, SET FORTH BY THE RFP.
- (B) INCLUDES 1'-0" MASH BARRIER PARAPET AND 1" SLAB EXTENSION
- (C) REMOVE AND DISPOSE OF EXISTING 56'-0"X 25'-6" ± SIMPLE 4-SPAN PRECAST CONCRETE BRIDGE AND APPURTENANCES IN ACCORDANCE WITH SECTION 202.4.2 OF THE STANDARD SPECIFICATIONS.
- F - DENOTES FIXED BEARING

 = EXCAVATE HATCHED AREA



CONTROL POINTS

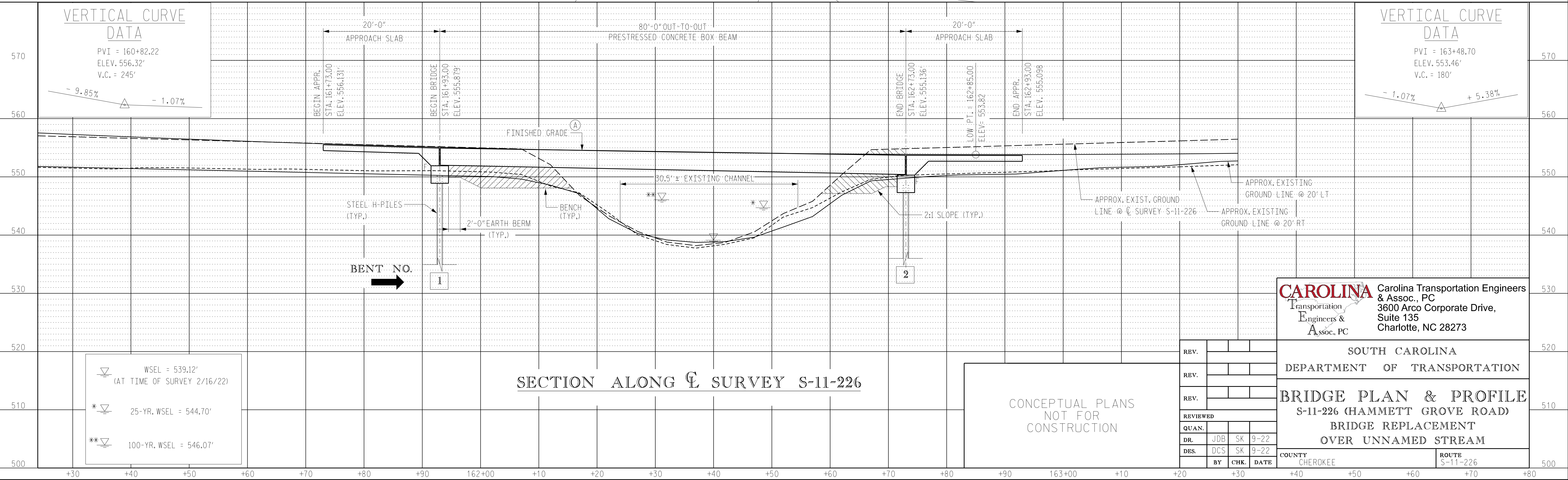
PCS4
ELEVATION = 554.84'
N 1129791.38 E 1775592.63
8" NAIL

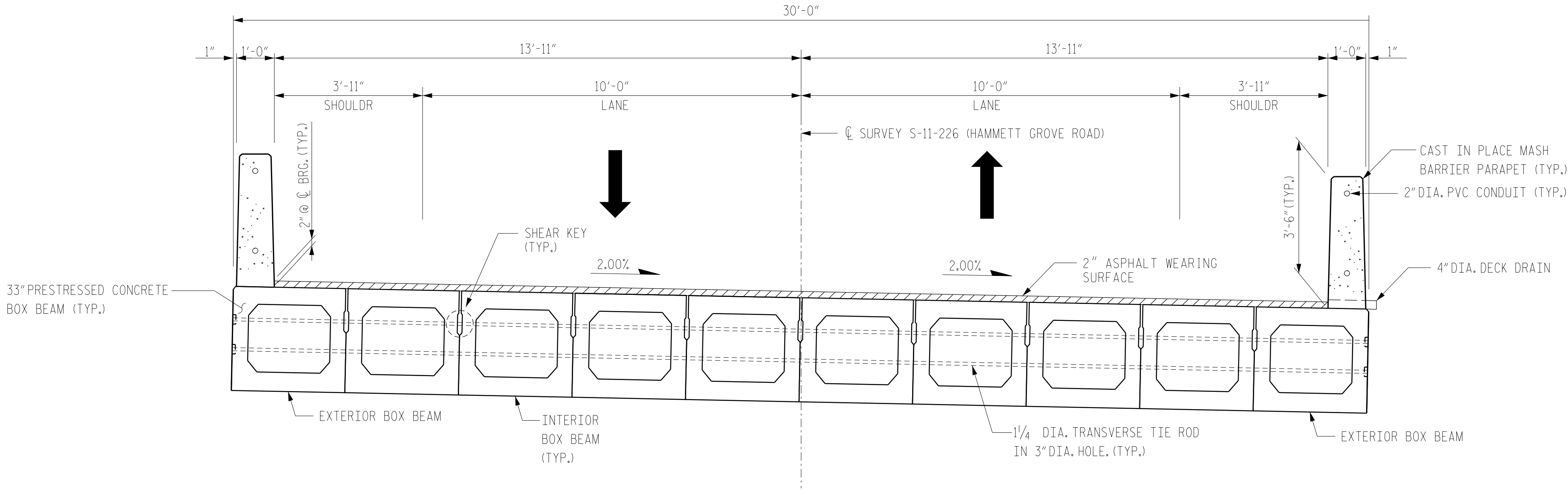
PCS5
ELEVATION = 581.34'
N 1130293.46 E 1776034.46
8" NAIL

HYDROLOGY DATA

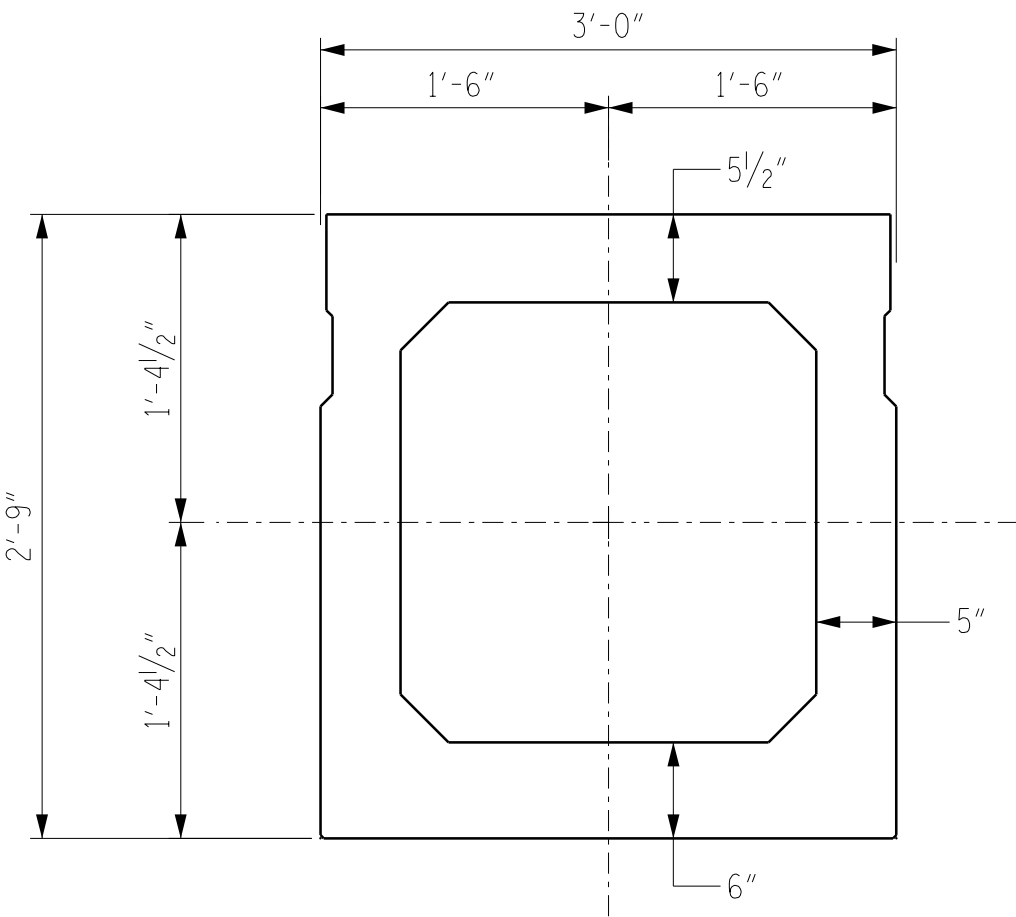
Drainage Area = 1.6 SQ. MILES
Q25 = 763 CFS
VEL. = 9.0 FT./SEC.

OVERTOPPING FLOOD
Q = 13,000 CFS
PROBABILITY: < 0.2%





TYPICAL SECTION
LOOKING IN DIRECTION OF STATIONING.



INTERIOR SLAB SECTION - BOX BEAM

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Carolina Transportation Engineers
& Assoc., PC
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Suite 135
Charlotte, NC 28273

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CAROLINA
TRANSPORTATION
ENGINEERS
AND
ASSOC., P.C.
No. 5739
CERTIFICATE OF AUTHORIZATION

CONCEPTUAL
PLANS
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CONSTRUCTION

REV.				SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION BRIDGE TYPICAL SECTION S-11-226 (HAMMETT GROVE ROAD) BRIDGE REPLACEMENT OVER UNNAMED STREAM	
REV.					
REV.					
REV.					
REVIEWED XXX				COUNTY CHEROKEE	
QUAN.					
DR.	JDB	SK	9-22		
DES.	DCS	SK	9-22		
	BY	CHK.	DATE	ROUTE S-11-226	

Appendix A.3 CPM Schedule
SCDOT Design Build Project
Bridge Package 14 DB Project
Contract ID 1162220



1162220 Bid Schedule - Bridge Package 14					Palmetto Infrastructure, Inc. - Summary												21-Oct-22																
Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2023												2024												2025			
						Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan				
1162220 Bid Schedule - Bridge Package 14		332	20-Feb-23	22-Sep-24	0	22-Sep-24, 1162220 Bid Schedule																											
Milestones		332	20-Feb-23	22-Sep-24	0	22-Sep-24, Milestones																											
M1000	Notice to Proceed	0	20-Feb-23*		0	◆ Notice to Proceed																											
M1010	Begin Bridge - S-138	0	07-Jul-23		251	◆ Begin Bridge - S-138																											
M1020	Begin Bridge - S-56	0	17-Apr-23		310	◆ Begin Bridge - S-56																											
M1030	Begin Bridge - S-106	0	13-Jul-23		246	◆ Begin Bridge - S-106																											
M1040	Begin Bridge - S-86	0	27-Dec-23		128	◆ Begin Bridge - S-86																											
M1050	Begin Bridge - S-226	0	25-Dec-23		129	◆ Begin Bridge - S-226																											
M1060	Open S-138 to Traffic	0	04-Dec-23		0	◆ Open S-138 to Traffic																											
M1070	Open S-56 to Traffic	0	14-Jul-23		245	◆ Open S-56 to Traffic																											
M1080	Open S-106 to Traffic	0	11-Oct-23		182	◆ Open S-106 to Traffic																											
M1090	Open S-86 to Traffic	0	24-Jun-24		0	◆ Open S-86 to Traffic																											
M1100	Open S-226 to Traffic	0	21-Jun-24		0	◆ Open S-226 to Traffic																											
M1110	Substantial Completion	0		24-Jun-24*	0	◆ Substantial Completion																											
M1120	Final Completion	0	22-Sep-24		0	◆ Final Completion																											
Submittals		160	20-Feb-23	24-Nov-23	172	24-Nov-23, Submittals																											
S-138 Submittals		36	20-Feb-23	21-Apr-23	296	21-Apr-23, S-138 Submittals																											
S1000	Submit Detour Plan - S-138	5	20-Feb-23	28-Feb-23	322	■ Submit Detour Plan - S-138																											
S1010	Review/Approve Detour Plan - S-138	5	28-Feb-23	08-Mar-23	322	■ Review/Approve Detour Plan - S-138																											
S1020	Submit DHEC Demolition Permit - S-138	5	20-Feb-23	28-Feb-23	48	■ Submit DHEC Demolition Permit - S-138																											
S1030	Review/Approve DHEC Demolition Permit - S-138	10	28-Feb-23	16-Mar-23	48	■ Review/Approve DHEC Demolition Permit - S-138																											
S1040	Notice of Demolition - S-138	0	21-Apr-23		34	◆ Notice of Demolition - S-138																											
S-56 Submittals		20	20-Feb-23	27-Mar-23	312	27-Mar-23, S-56 Submittals																											
S1050	Submit Detour Plan - S-56	10	20-Feb-23	08-Mar-23	312	■ Submit Detour Plan - S-56																											
S1060	Review/Approve Detour Plan - S-56	10	08-Mar-23	27-Mar-23	312	■ Review/Approve Detour Plan - S-56																											
S1070	Submit DHEC Demolition Permit - S-56	5	20-Feb-23	28-Feb-23	18	■ Submit DHEC Demolition Permit - S-56																											
S1080	Review/Approve DHEC Demolition Permit - S-56	10	28-Feb-23	16-Mar-23	18	■ Review/Approve DHEC Demolition Permit - S-56																											
S1090	Notice of Demolition - S-56	0	13-Mar-23		4	◆ Notice of Demolition - S-56																											
S-106 Submittals		66	20-Feb-23	13-Jun-23	266	13-Jun-23, S-106 Submittals																											
S1100	Submit Detour Plan - S-106	10	20-Feb-23	08-Mar-23	312	■ Submit Detour Plan - S-106																											
S1110	Review/Approve Detour Plan - S-106	10	08-Mar-23	27-Mar-23	312	■ Review/Approve Detour Plan - S-106																											
S1120	Submit DHEC Demolition Permit - S-106	5	20-Feb-23	28-Feb-23	69	■ Submit DHEC Demolition Permit - S-106																											
S1130	Review/Approve DHEC Demolition Permit - S-106	10	28-Feb-23	16-Mar-23	69	■ Review/Approve DHEC Demolition Permit - S-106																											
S1140	Notice of Demolition - S-106	0	13-Jun-23		1	◆ Notice of Demolition - S-106																											
S-86 Submittals		160	20-Feb-23	24-Nov-23	172	24-Nov-23, S-86 Submittals																											
S1150	Submit Detour Plan - S-86	10	20-Feb-23	08-Mar-23	312	■ Submit Detour Plan - S-86																											
S1160	Review/Approve Detour Plan - S-86	10	08-Mar-23	27-Mar-23	312	■ Review/Approve Detour Plan - S-86																											
S1170	Submit DHEC Demolition Permit - S-86	5	20-Feb-23	28-Feb-23	163	■ Submit DHEC Demolition Permit - S-86																											
S1180	Review/Approve DHEC Demolition Permit - S-86	10	28-Feb-23	16-Mar-23	163	■ Review/Approve DHEC Demolition Permit - S-86																											
S1190	Notice of Demolition - S-86	0	24-Nov-23		1	◆ Notice of Demolition - S-86																											
S-226 Submittals		159	20-Feb-23	22-Nov-23	173	22-Nov-23, S-226 Submittals																											
S1200	Submit Detour Plan - S-226	10	20-Feb-23	08-Mar-23	312	■ Submit Detour Plan - S-226																											
S1210	Review/Approve Detour Plan - S-226	10	08-Mar-23	27-Mar-23	312	■ Review/Approve Detour Plan - S-226																											
S1220	Submit DHEC Demolition Permit - S-226	5	20-Feb-23	28-Feb-23	162	■ Submit DHEC Demolition Permit - S-226																											
S1230	Review/Approve DHEC Demolition Permit - S-226	10	28-Feb-23	16-Mar-23	162	■ Review/Approve DHEC Demolition Permit - S-226																											
S1240	Notice of Closure/Demolition - S-226	0	22-Nov-23		1	◆ Notice of Closure/Demolition - S-226																											
S-138 Goucher School Road		165	20-Feb-23	04-Dec-23	0	04-Dec-23, S-138 Goucher School Road																											

Remaining Level of Effort

Actual Work

Critical Remaining Work

Actual Level of Effort

Remaining Work





◆

◆ Milestone

Page 1 of 5

Data Date: 20-Feb-23

1162220 Bid Schedule - Bridge Package 14
Palmetto Infrastructure, Inc. - Summary

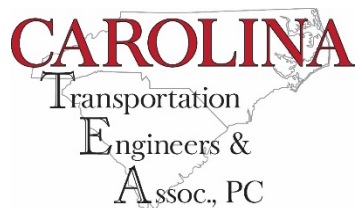
Remaining Level of Effort Actual Work Critical Remaining Work  Actual Level of Effort  Remaining Work   Milestone	Page 2 of 5	Data Date: 20-Feb-23	1162220 Bid Schedule - Bridge Package 14 Palmetto Infrastructure, Inc. - Summary
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1162220 Bid Schedule - Bridge Package 14						Palmetto Infrastructure, Inc. - Summary												21-Oct-22															
Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2023												2024												2025			
						Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan				
	A1240	Install Asphalt Paving - S-56	2	04-Jul-23	05-Jul-23	245																											
	A1250	Install Guardrail - S-56	2	12-Jul-23	13-Jul-23	246																											
	A1260	Asphalt Curing Time S-56	7	06-Jul-23	12-Jul-23	343																											
	A1270	Install Permanent Pavement Markings S-56	2	12-Jul-23	14-Jul-23	245																											
	Bridge Work - S-56		50	17-Apr-23	11-Jul-23	2																											
	A1280	Demo Existing Bridge - S-56	5	17-Apr-23	21-Apr-23	1																											
	A1290	Construct Substructure - S-56	15	24-Apr-23	17-May-23	1																											
	A1300	Construct Superstructure - S-56	8	18-May-23	31-May-23	1																											
	A1310	Construct Approach Slabs - S-56	10	15-Jun-23	03-Jul-23	2																											
	A1320	Install Barrier Wall - S-56	5	04-Jul-23	11-Jul-23	2																											
S-106 Island Creek Road		117	21-Mar-23	11-Oct-23	145																												
Design S-106		146	21-Mar-23	11-Oct-23	182																												
	D1160	Submit Final Plans - S-106	0		21-Mar-23	43																											
	D1170	SCDOT Review - S-106	15	22-Mar-23	11-Apr-23	43																											
	D1180	Response & Corrections - S-106	5	12-Apr-23	18-Apr-23	43																											
	D1190	5-Day SCDOT Review - S-106	5	19-Apr-23	25-Apr-23	43																											
	D1200	Finalize RFC Plans - S-106	5	26-Apr-23	02-May-23	43																											
	D1210	Submit Bridge/Roadway RFC Plans S-106	0		02-May-23	43																											
	D1220	Utility Coordination - S-106	146	22-Mar-23	11-Oct-23	182																											
	D1230	ROW Acquisitions - S-106	61	03-May-23	26-Jul-23	2																											
Construction S-106		115	27-Mar-23	11-Oct-23	145																												
	A1330	Install/Maintain Detour - S-106	143	27-Mar-23	11-Oct-23	182																											
	A1340	Mobilization - S-106	1	13-Jun-23	14-Jun-23	1																											
	A1350	Construction Surveying - S-106	86	13-Jun-23	11-Oct-23	182																											
	A1360	Install/Maintain Erosion Control - S-106	86	13-Jun-23	11-Oct-23	182																											
	A1370	Vibration Monitoring - S-106	86	13-Jun-23	11-Oct-23	182																											
Road Work - S-106		68	15-Jun-23	11-Oct-23	0																												
	A1380	Clearing & Grubbing - S-106	10	15-Jun-23	28-Jun-23	2																											
	A1390	Rough Grading - S-106	20	29-Jun-23	26-Jul-23	2																											
	A1400	Finish Grading - S-106	10	30-Aug-23	13-Sep-23	0																											
	A1410	Install Asphalt Paving - S-106	2	28-Sep-23	02-Oct-23	0																											
	A1420	Install Guardrail - S-106	2	09-Oct-23	11-Oct-23	0																											
	A1430	Asphalt Curing Time S-106	7	02-Oct-23	09-Oct-23	0																											
	A1440	Install Permanent Pavement Markings S-106	2	09-Oct-23	11-Oct-23	0																											
Bridge Work - S-106		49	13-Jul-23	09-Oct-23	0																												
	A1450	Demo Existing Bridge - S-106	5	13-Jul-23	20-Jul-23	1																											
	A1460	Construct Substructure - S-106	15	20-Jul-23	16-Aug-23	0																											
	A1470	Construct Superstructure - S-106	8	16-Aug-23	30-Aug-23	0																											
	A1480	Construct Approach Slabs - S-106	9	13-Sep-23	28-Sep-23	0																											
	A1490	Install Barrier Wall - S-106	5	28-Sep-23	09-Oct-23	0																											
S-86 Rock House Road		260	27-Mar-23	24-Jun-24	0																												
Design S-86		305	21-Apr-23	24-Jun-24	0																												
	D1240	Submit Final Plans - S-86	0		21-Apr-23	142																											
	D1250	SCDOT Review - S-86	15	24-Apr-23	12-May-23	142																											
	D1260	Response & Corrections - S-86	5	15-May-23	19-May-23	142																											
	D1270	5-Day SCDOT Review - S-86	5	22-May-23	26-May-23	142																											
	D1280	Finalize RFC Plans - S-86	5	29-May-23	02-Jun-23	142																											
<div>Remaining Level of Effort</div> <div>Actual Level of Effort</div>			<div>Actual Work</div> <div>Remaining Work</div>			<div>Critical Remaining Work</div> <div>Milestone</div>			Page 3 of 5						Data Date: 20-Feb-23						1162220 Bid Schedule - Bridge Package 14 Palmetto Infrastructure, Inc. - Summary												

1162220 Bid Schedule - Bridge Package 14					Palmetto Infrastructure, Inc. - Summary												21-Oct-22															
Activity ID		Activity Name		Original Duration	Start	Finish	Total Float	2023												2024												2025
								Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	
<div><div></div><div></div><div></div><div></div></div>	A1790	Construct Superstructure - S-226	15	22-Feb-24	20-Mar-24	0																										
	A1800	Construct Approach Slabs - S-226	15	14-May-24	10-Jun-24	0																										
	A1810	Install Barrier Wall - S-226	5	10-Jun-24	18-Jun-24	1																										
<div><div></div> Remaining Level of Effort</div> <div><div></div> Actual Level of Effort</div>				<div><div></div> Actual Work</div> <div><div></div> Remaining Work</div>				<div><div></div> Critical Remaining Work</div> <div><div></div> Milestone</div>				Page 5 of 5				Data Date: 20-Feb-23				1162220 Bid Schedule - Bridge Package 14 Palmetto Infrastructure, Inc. - Summary												

Appendix B – Required Forms

SCDOT Design Build Project Bridge Package 14 DB Project Contract ID 1162220



12. STIPEND ACKNOWLEDGEMENT FORM

Stipend Acknowledgement Form

Bridge Package 14 Cherokee County

Proposer: Palmetto Infrastructure, Inc.

ADDRESS: 3620 Pelham Rd. PMB 349 Greenville, SC 29615

The undersigned Proposer, hereby:

☐ Waives the stipend for this Project.

☒ Accepts the stipend for this Project.

By accepting the stipend for this Project, Proposer agrees:

- 1) to execute and include the Stipend Agreement in Article XIII of the RFP with its RFP response;
- 2) to submit an invoice with FEIN number for the stipend amount to the SCDOT POC after SCDOT's posting of the Notice of Award on SCDOT's Design-Build Website.;
- 3) to transfer all rights to its Work Product used to develop the Proposal as of the date of this acknowledgement. "Work Product" means all submittals, including ATCs, ideas, innovations, solutions, methods, processes, design concepts, materials, electronic files, marked up drawings, cross sections, quantity lists and intellectual property, made by Proposer during the RFP process, including the Proposal, exchange of information during the pre-Proposal and post-Proposal period.

SCDOT will pay the stipend to each eligible unsuccessful Proposer, who has signed a Stipend Agreement, within ninety (90) days after execution of the Contract or the decision to not award a contract.

10-24-22
Date

Palmetto Infrastructure, Inc.
Proposer

Greg Canniff
Print Name



13. STIPEND AGREEMENT

STIPEND AGREEMENT

Project ID: 1162220

Bridge Package 14

Cherokee County

THIS STIPEND AGREEMENT (the “Agreement”) is made and entered into as of the ____ day of _____, 20__, by and between the SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION (hereinafter “SCDOT”), and Palmetto Infrastructure, Inc. (“Proposer”), with reference to the following facts:

SCDOT issued a Request for Proposal (“RFP”) for design and construction of the above-referenced Design-Build Project (“Project”), pursuant to procurement authority granted in Section 57-5-1625 of the S.C. Code of Laws, 1976, as amended. The RFP provided for payment of stipends as provided herein. Capitalized terms used, but not defined, have the meanings ascribed in the RFP.

NOW, THEREFORE, Proposer hereby agrees as follows:

1. Work Product.

1.1 Proposer shall prepare and submit a responsible and responsive Technical Proposal and Cost Proposal that conforms in all material respects to the requirements and provisions of the RFP, as determined by SCDOT, and are timely received by SCDOT in accordance with the RFP Milestone Schedule.

1.2 By signing this Stipend Agreement, Proposer agrees to transfer full and complete ownership to SCDOT of all Work Product. The Work Product (as defined below) shall become the property of SCDOT without restriction or limitation on its use, without further compensation or consideration, and can be used in connection with this Project or any future projects by SCDOT. Neither Proposer nor any of its team members shall copyright any of the material developed under this Agreement.

1.3 The term “Work Product” shall mean the Proposal and all material, electronic files, marked up drawings, cross sections, quantity lists, submittals, alternative technical concepts (ATC), ideas, innovations, solutions, methods, processes, design concepts, Trade Secrets or confidential information, and intellectual property, made by or produced for Proposer in the development and submission of the Technical and Cost Proposal, including exchanges of information during the pre-Proposal and post-Proposal period.

2. Compensation and Payment.

2.1 A stipend to Proposer for the Work Product described herein shall be \$25,000.00 and is payable to Proposer that was determined to be responsible and (1) submitted a responsive Technical Proposal and responsive Cost Proposal to the RFP which is not selected for award of this Project, or (2) was awarded the Contract but the Contract was terminated by SCDOT for convenience after the Submittal of Proposal Due Date (See Final RFP Milestone schedule) but prior to the Notice to Proceed #1. Responsibility of Proposers and responsiveness of the Technical Proposal and Cost Proposal will be determined by SCDOT as a condition of payment.

2.2 SCDOT will pay the stipend to Proposer as follows, subject (as applicable) to the following conditions:

- (a) Proposer has submitted this signed Stipend Agreement, unchanged with its response to the RFP.
- (b) After posting of the Notice of Award on SCDOT’s Design-Build Website, Proposer has submitted to SCDOT an invoice, with FEIN Number, for the Stipend amount.
- (c) After execution of the Contract or the decision not to award a contract, SCDOT will pay the invoice for the stipend amount to the unsuccessful Proposer meeting the criteria of Section 2.1 within 90 calendar days of receipt of the invoice from Proposer.
- (d) If the procurement is suspended or cancelled prior to the Proposal Due Date (see FINAL RFP Milestone schedule), no stipend will be paid to Proposer.
- (e) After the submittal of Proposals, but prior to award, if the procurement is cancelled, all Proposers that provide a responsive Technical Proposal and Cost Proposal to the final RFP and submitted a signed Stipend Agreement with their RFP shall receive the stipend
- (f) In the event of a Best and Final Offer, only one stipend will be paid to each Proposer that executed a Stipend Agreement and met the other criteria and conditions herein.
- (g) No stipends will be paid for submitting RFQ responses.
- (h) No stipends will be paid to a Proposer who withdraws at any time from this procurement.

2.3 Acceptance by the Proposer of payment of the stipend amount from SCDOT shall constitute a waiver by Proposer of any and all right, equitable or otherwise, to bring any claim in connection with this procurement, procurement process, award of the Contract, or cancellation of this procurement.

2.4 The Proposer awarded the contract shall be not eligible to receive a stipend.

2.5 If Proposer elects to waive payment of the stipend, SCDOT will not use the ideas or information contained in that Proposer's Proposal for this Project. However, the Proposer's Proposal will be subject to the South Carolina Freedom of Information Act.

3. Indemnities.

3.1 Subject to the limitations contained in Section 3.2, Proposer shall indemnify, protect and hold harmless SCDOT and its directors, officers, employees and contractors from, and Proposer shall defend at its own expense, all claims, costs, expenses, liabilities, demands, or suits at law or equity arising, in whole or in part, from the negligence or willful misconduct of Proposer or any of its agents, officers, employees, representatives or subcontractors or breach of any of Proposer's obligations under this Agreement.

3.2 This indemnity shall not apply with respect to any claims, demands or suits arising from use of the Work Product by SCDOT.

4. Compliance With Laws.

4.1 Proposer shall comply with all federal, state, and local laws, ordinances, rules, and regulations applicable to the work performed or paid for under this Agreement and covenants and agrees that it and its employees shall be bound by the standards of conduct provided in applicable laws, ordinances, rules, and regulations as they relate to work performed under this Agreement. Proposer agrees to incorporate the provisions of this paragraph in any subcontract into which it might enter with reference to the work performed pursuant to this Agreement.

4.2 The Proposer agrees (a) not to discriminate in any manner against an employee or applicant for employment because of race, color, religion, creed, age, sex, marital status, national origin, ancestry or disability of a qualified individual with a disability; (b) to include a provision similar to that contained in subsection (a) in any subcontract; and (c) to post and to cause subcontractors to post in conspicuous places available to employees and applicants for employment, notices setting forth the substance of this clause.

5. Assignment.

Proposer shall not assign this Agreement without SCDOT's prior written consent. Any assignment of this Agreement without such consent shall be null and void.

6. Miscellaneous.

6.1 Proposer and SCDOT agree that Proposer, its team members, and their respective employees are not agents of SCDOT as a result of this Agreement.

6.2 This Agreement, together with the RFP, as amended from time to time, the provisions of which are incorporated herein by reference, embodies the entire agreement of the parties. There are no promises, terms, conditions, or obligations other than those contained herein or in the RFP, and this Agreement shall supersede all previous communications, representation, or agreements, either oral or written, between the parties hereto.

6.3 It is understood and agreed by the parties hereto that if any part, term, or provision of this Agreement is by the courts held to be illegal or in conflict with any law of the State of South Carolina, the validity of the remaining portions or provisions shall not be affected, and the rights and obligations of the parties shall be construed and enforced as if the Agreement did not contain the particular part, term, or provisions to be invalid.

6.4 This Agreement shall be governed by and construed in accordance with the laws of the State of South Carolina.

IN WITNESS WHEREOF, the parties have executed this Agreement as of the date first written above.

Witness:

Recommended:

{INSERT NAME}
Alternative Delivery Program Manager

Witness:

 _____

Derek Staton

SOUTH CAROLINA DEPARTMENT
OF TRANSPORTATION

By: _____
{INSERT NAME}
Alternative Delivery Engineer

Proposer

Palmetto Infrastructure, Inc.
Name of Proposer

By:  _____

Its: President

11. EQUAL EMPLOYMENT OPPORTUNITY CERTIFICATION

(COMPLETE THIS SECTION FOR FEDERAL PROJECTS ONLY) EQUAL EMPLOYMENT OPPORTUNITY PERFORMANCE

Select the Certification that applies to the PROPOSER:

Certification (1) ☐ or Certification (2) ☒

Select the appropriate responses in the applicable Certification:

Certification (1): Pursuant to 41 C.F.R. §60-1.7(b)(1), Previous Equal Employment Opportunity Performance Certification, as the Prospective Prime Contractor, I HEREBY CERTIFY THAT I:

(a) **(HAVE / HAVE NOT)** developed and filed an Affirmative Action Program pursuant to 41C.F.R. §60-2 and/or 60-4;

(b) **(HAVE / HAVE NOT)** participated in a previous contract or subcontract subject to the equal opportunity clause;

(c) **(HAVE / HAVE NOT)** filed with the Joint Reporting Committee, the Director of Office of Federal Contract Compliance, or the Equal Employment Opportunity Commission, all reports due under the applicable filing requirements,

OR

Certification (2): I, HEREBY CERTIFY that as the Prospective Prime Contractor submitting this Proposal **(CLAIM / DO NOT CLAIM)** exemption from the submission of the Standard Form 100 (EEO-1) due to the fact that it employs a total of less than fifty (50) employees under C.F.R. §60-1.7, or qualifies for an exempted status under 41 C.F.R. §60-1.5.

I FURTHER CERTIFY that the above Certification will be made part of any Subcontract Agreement, or other agreement involved with this project.

Executed on 10/24, 2022 .

Signed: _____

(Officer/PROPOSER)

Title: President

Company: Palmetto Infrastructure, Inc

Address: 3620 Pelham Rd. PMB 349 Greenville, SC 29615

Note: The above certification is required by the Equal Employment Opportunity Regulations of the Secretary of Labor (41 CFR 60-1.7(b)(1)), and must be submitted by PROPOSERS only in connection with contracts which are subject to the equal opportunity clause. Contracts that are exempt from the equal opportunity clause are set forth in 41 CFR 60-1.5. (Generally, only contracts of \$10,000 or under are exempt.)

Currently, Standard Form 100 (EEO-1) is the only report required by Executive Orders or their implementing regulations.

Proposers, Primary Members, or proposed Subcontractors (any tier) and Consultants who have participated in a previous contract subject to the Executive Orders and have not filed the required reports shall note that 41 CFR 60-1.7(b)(1) prevents the award of contracts and subcontracts unless such contractor submits a report covering the delinquent period or such other period specified by the Federal Highway Administration or by the Director, Office of Federal Contract Compliance, U.S. Department of Labor.

10. NON-COLLUSION CERTIFICATION

NON-COLLUSION CERTIFICATION

Project ID: 1162220

IN ACCORDANCE WITH THE PROVISIONS OF S.C. CODE ANN. §§ 39-3-10 ET. SEQ., 39-5-10 ET. SEQ., 15 U.S.C. §45; 23 C.F.R. §635.112(F); AND 28 U.S.C. §1746, I HEREBY ACKNOWLEDGE THAT I AM AN OFFICER OF THE PROPOSER FIRM AND, UNDER PENALTY OF PERJURY UNDER THE LAWS OF THE UNITED STATES AND SOUTH CAROLINA, DECLARE, BY MY CERTIFICATION BELOW, THAT THE FOLLOWING IS TRUE AND CORRECT, AND FURTHER, THAT THIS JOINT-VENTURE, FIRM, PARTNERSHIP, ASSOCIATION OR CORPORATION, OR ANY OTHER LEGAL ENTITY HAS NOT, EITHER DIRECTLY OR INDIRECTLY, ENTERED INTO ANY AGREEMENT, PARTICIPATED IN ANY COLLUSION, OR OTHERWISE TAKEN ANY ACTION IN RESTRAINT OF FREE COMPETITIVE BIDDING IN CONNECTION WITH THE SUBMISSION OF A BID PROPOSAL ON THE ABOVE REFERENCED PROJECT.

BY CHECKING THIS BOX ☒ , I CERTIFY THAT I HAVE READ, UNDERSTAND, ACCEPT, AND ACKNOWLEDGE ALL OF THE ABOVE STATEMENTS.

Executed on 10-24-22
(Date)

Signed: 
(Officer/Proposer)

President

(Title)

3620 Pelham Rd. PMB 349

(Address)

Greenville, SC 29615

NOTICE OF RECEIPT
Bridge Package 14
Design-Build – Contract ID 1162220
Cherokee County

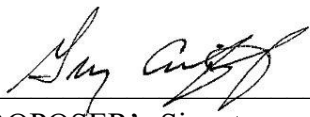
Addendum 1

The information in this addendum shall be made part of the contract documents. PROPOSERS are instructed to incorporate the information into the previously provided RFP documents.

PROPOSERS are required to sign this document and enclose it with their Technical Proposal. Receipt of this signed document by The South Carolina Department of Transportation serves as confirmation that the PROPOSER has received and incorporated this Addendum into the contract documents.

Confirmation Statement:

I, the PROPOSER confirm that I have received this addendum package and have incorporated the information provided in the addendum into the contract documents.



PROPOSER's Signature

10-24-22

Date

Greg Canniff

Printed Name

For: Palmetto Infrastructure, Inc.

Design-Build Team Name



NOTICE OF RECEIPT
Bridge Package 14
Design-Build – Contract ID 1162220
Cherokee County

Addendum 2

The information in this addendum shall be made part of the contract documents. PROPOSERS are instructed to incorporate the information into the previously provided RFP documents.

PROPOSERS are required to sign this document and enclose it with their Technical Proposal. Receipt of this signed document by The South Carolina Department of Transportation serves as confirmation that the PROPOSER has received and incorporated this Addendum into the contract documents.

Confirmation Statement:

I, the PROPOSER confirm that I have received this addendum package and have incorporated the information provided in the addendum into the contract documents.



PROPOSER's Signature

10-24-22

Date

Greg Canniff

Printed Name

For: Palmetto Infrastructure, Inc.

Design-Build Team Name





October 24, 2022

Bridge Package 14 DB Project

Contract ID 1162220

Cherokee County, South Carolina

Organizational Chart Updates

The following updates have been made to the Organizational Chart:

None

A handwritten signature in black ink, appearing to read "Greg Canniff", written in a cursive style.

Gregory Canniff

President



October 24, 2022

Bridge Package 14 DB Project

Contract ID 1162220

Cherokee County, South Carolina

Confidential and/or Proprietary Information Page List

The following section of this proposal is considered confidential and should not be disclosed under the South Carolina Freedom of Information Act:

None

A handwritten signature in black ink, appearing to read "Greg Canniff", is written over a light gray rectangular background.

Gregory Canniff

President



October 24, 2022

Bridge Package 14 DB Project

Contract ID 1162220

Cherokee County, South Carolina

Joint Venture Organizational Agreement

Not Applicable.

A handwritten signature in black ink, appearing to read "Greg Canniff", written in a cursive style.

Gregory Canniff

President



Columbia, South Carolina

**SOUTH CAROLINA DEPARTMENT
OF
TRANSPORTATION**

PRIME CONTRACTOR

PREQUALIFICATION CERTIFICATE

This Certifies that your company has complied with the rules and regulations of the Department and the State of South Carolina, and subject to the rules and regulations for a prime contractor, is declared eligible to submit a bid and be awarded any construction contract issued by the Department, subject to obtaining proper bonds and insurance acceptable to the Department and complying with all other statutory and contract requirements.

ALL BIDS SUBMITTED TO THE DEPARTMENT MUST BE IN THE NAME AS SHOWN BELOW.

PALMETTO INFRASTRUCTURE, INC.

Vendor ID: 1PA056

Issued : March 8, 2022

Expires: April 30, 2023

Approved By:

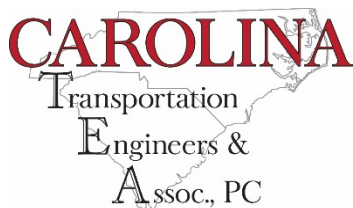

Prequalification Coordinator

Number	Description	Added Value/Benefits	Cost/Schedule Impacts	Self-imposed Assurance
1	Schedule: Submit a complete final plan package for each site as the initial plan / report submittal.	Bridge design will be completed faster allowing construction to commence sooner - and replacements will be completed faster (Public benefit). SCDOT will have a reduced effort in design reviews. Each site will have all information available for a complete review (SCDOT benefit).	Schedule will be accelerated by ~60 days each bridge by eliminating Preliminary and ROW review steps. SCDOT review cost is reduced (amount unknown, say \$20,000 each site = \$100,000).	SCDOT has thorough and detailed requirements for each bridge site. The PII / CTEA Team has confidence that we understand and follow these requirements. We will meet with SCDOT post bid to discuss our submitted plans and any outstanding questions we may have, determine if SCDOT has any additional requests, and carry plans directly to final design at risk. Any modifications needed to the plans will be made by the team at no additional cost, and the PII /CTEA Team maintains our assurances for the schedule presented through our self imposed LDs discussed below.
2	Schedule: Early delivery of S-138 over Goucher Creek	PII commits to a 150 day schedule in lieu of the 180 day requirement. Further, we commit to open this site to the public within 180 days of completion of the water line relocation (Public benefit through earlier use of roadway). Reduced construction timeframe will reduce CEI costs (SCDOT benefit).	Site schedule will be accelerated 30 days. SCDOT CEI cost will be reduced (amount estimated at \$35,000). Guaranteed site opening allows the public to use the roadway sooner than required by the RFP (240 days sooner than required @ \$750 / day user costs = \$180,000).	LD's will be self-imposed on the team for time over 150 days of construction or 180 days from waterline relocation, whichever occurs first.
3	Schedule: Early delivery of S-56 over Horse Creek	PII commits to a 90 day delivery in lieu of the 120 day requirement. Further, we commit to open this site to the public within 180 days from NTP (Public benefit). Reduced construction timeframe will reduce CEI efforts (SCDOT benefit).	Schedule will be accelerated 30 days. SCDOT CEI cost will be reduced (amount estimated at \$35,000). Guaranteed site opening allows the public to use the roadway sooner than required by the RFP (640 days - 180 days = 460 days early @ \$750 / day user costs = \$345,000).	LD's will be self-imposed on the team for time over 90 days of construction or 180 days from NTP, whichever occurs first.
4	Schedule: Early delivery of S-106 over Suck Creek	PII commits to a 90 day delivery in lieu of the 120 day requirement. Further, we commit to open this site to the public within 300 days from NTP (Public benefit). Reduced construction timeframe will reduce CEI efforts (SCDOT benefit).	Schedule will be accelerated 30 days. SCDOT CEI cost will be reduced (amount estimated at \$35,000). Guaranteed site opening allows the public to use the roadway sooner than required by the RFP (640 days - 300 days = 340 days early @ \$750 / day user costs = \$255,000).	LD's will be self-imposed on the team for time over 90 days of construction or 300 days from NTP, whichever occurs first.
5	Schedule: Early delivery of S-86 over King's Creek	PII commits to a 180 day delivery in lieu of the 210 day requirement. Further, we commit to open this site to the public within 490 days from NTP (Public benefit). Reduced construction timeframe will reduce CEI efforts (SCDOT benefit).	Schedule will be accelerated 30 days. SCDOT CEI cost will be reduced (amount estimated at \$35,000). Guaranteed site opening allows the public to use the roadway sooner than required by the RFP (640 days - 490 days = 150 days early @ \$750 / day user costs = \$112,500).	LD's will be self-imposed on the team for time over 90 days of construction or 490 days from NTP, whichever occurs first.
6	Schedule: Early Completion of entire contract	PII commits to an early delivery (490 days) for substantial completion of the project as compared to the 640 day requirement. (Public benefit).	PII schedule and available crews allow us to commit to substantial completion of the project 150 days (approximately 25%) earlier than required. We anticipate beating this schedule as well.	LD's will be self-imposed on the team after 490 days of construction.
7	Utility avoidance: S-56	Waterline will be avoided and can remain in place. (Public and SCDOT benefit)	SCDOT will not have to pay for waterline relocation. \$50,000 savings for design and construction.	Utility coordination and relocation built into our schedule and bid. LD's imposed for late delivery of bridge or contract.
8	Utility avoidance: S-86	OH power will be maintained in existing location. (Public and SCDOT benefit)	No cost or schedule impacts	Utility coordination and relocation built into our schedule and bid. LD's imposed for late delivery of bridge or contract.
9	Utility avoidance: S-106	All utilities at bridge can be backfed from the east and permanently removed from the bridge site prior to mobilization. (Public and SCDOT benefit)	No cost or schedule impacts	Utility coordination and relocation built into our schedule and bid. LD's imposed for late delivery of bridge or contract.

10	ROW: S-106 Reduce ROW @ begin bridge	Reduction of required 75 ft ROW to 70 ft ROW will eliminate driveway from SCDOT ownership (SCDOT benefit)	SCDOT will not have to own or maintain driveway, say \$25,000 over bridge life?	Plans show SCDOT requirement of 75 ft. Will discuss with SCDOT and incorporate reduction if preferred. No difference in price or schedule to PII Team.
11	ROW: S-106 Reduce ROW @ end bridge right.	Minor reduction to required 75 ft ROW will eliminate a take from additional parcel and clean up the remnants of the parcel being taken - use variable transition from 70 to 75 ft. (SCDOT benefit)	There is no advantage to SCDOT to take additional parcel for maintenance and no advantage to homeowner for remnant past required ROW.	Plans a reduction to the SCDOT required ROW. Will discuss with SCDOT and incorporate 75 ft requirement if preferred. No difference in price or schedule to PII Team.
12	ROW: S-106 Reduce ROW if septic tanks / drain fields are disturbed.	Modify ROW if impacts occur to septic tanks and/or drainage fields. Additional survey to be performed post award to determine conflicts. (SCDOT & Public Benefit)	PII to determine limits of drainage fields for septic systems. Proposed driveways and ROW takes will be modified to avoid these areas if found. This work included in base bid price.	Plans show SCDOT requirement of 75 ft. Will discuss with SCDOT and incorporate reduction if impacts are discovered. No difference in price or schedule to PII Team.
13	Design: S-86 Modify bridge to 2-span structure	Faster to construct (Public benefit). Reduced maintenance over life of bridge (SCDOT benefit)	Schedule will be accelerated 30 days. SCDOT cost will be reduced (design reviews and CEI). Bid price is reduced. Reduced future maintenance (Interior piers, joints). Schedule is reduced 30 days for construction.	Early delivery commitment with LD's
14	Design: S-86 Shorten project limits. Use retaining wall / steepened slopes to avoid National Park ROW.	Shortening project limits avoids the rock cut, avoids shifting the roadway alignment, and avoids taking any ROW in National Park. (Public and SCDOT benefit)	Cost is reduced by \$100,000 for roadway work and \$250,000 for rock cut required to shift the roadway. (Cost reductions included in bid price).	Early delivery commitment with LD's. Lump Sum bid for project guarantees price.
15	Design: S-106 end bridge Eliminate trailing end guardrail	Facilitates parcel access. (Public and SCDOT benefit)	ROW is avoided. Cost is improved by eliminating guardrail construction, driveway construction / relocation, reducing required ROW, and eliminating future maintenance of guardrail and stream impacts. (Say \$40,000)	Allowed by RFP. Incorporated into design.
16	Design: S-106 Attach guardrail to bridge (in lieu of approach).	Modification allows teams to shorten low point offset from bridge, lower the roadway profile, and improve driveway sight distance. Lower profile results in less ROW impacts and avoids driveways. (Public Benefit)	Schedule is reduced by reducing roadway work and shortening project limits. Schedule is improved by 30 days. Reductions to project limits and ROW (Say \$10,000).	Allowed by RFP. Incorporated into design.
17	Design: S-226 Attach guardrail to bridge (in lieu of approach)	Modification allows teams to shorten low point offset from bridge and lower the roadway profile. Lower profile results in eliminating walls. (SCDOT Benefit) Lower profile results in avoidance of driveway modifications, ROW impacts and minimizes material delivery to the project site. (Public Benefit).	Schedule is reduced by eliminating fill and eliminating walls. Gabion basket retaining wall is eliminated and driveways can be maintained. Schedule is improved by 60 days minimum. Cost reductions included in bid price = say \$150,000.	Allowed by RFP. Incorporated into design.
18	Design: S-106 Reduce profile (maintain low chord)	Profile adjustment facilitates tie-in of existing driveways. (SCDOT benefit). Less hauling and efficient, low-impact construction. (Public benefit).	SCDOT will not have to build up / modify driveways, ~ \$50,000.	Incorporated into plans.
19	Design: S-138 Top Down Construction	Span B utilizes 24" cored slabs and is designed to hold crane during construction. This saves time for building and removing construction access, allows construction to occur within existing ROW, and minimizes environmental impacts.	Top down facilitates placement of 80 ft box beams with smaller crane and minimizes need for BCA. Savings = \$75000. Schedule is accelerated through less temporary works.	Incorporated into plans and schedule. Most recently completed same concept on EBP 2020-1 DB in York County.
20	Design: S-226 Lower existing profile. Utilize Extra length guardrail with compressed shoulders. Maintain clearance to 500 yr HWEL.	Lower the profile while maintaining maximum grades and minimum k-values. Low-impact construction (public benefit). Use of compressed shoulders and steepened slopes eliminates stream impacts without the use of retaining walls - environmental benefits and future maintenance benefits (SCDOT benefit).	Construction cost reduction estimated at \$400,000 of grading and wall construction. (SCDOT benefit)	Incorporated into plans.

21	Environmental: Minimize idling of machinery/vehicles on site.	Noise and air pollution will be reduced. (Public and SCDOT benefit)	No schedule impact or additional cost.	PII will identify concept as a priority with staff at preconstruction meetings and provide regular monitoring and documentation on site.
22	Environmental: Independent "watchdog" crew to perform S&E inspection and monitor corrective actions.	Permit conditions compliance. (Public and SCDOT benefit)	Cost covered in the bid.	Reports will be made available to SCDOT upon request.
23	Environmental: Good neighbors.	PII is a good neighbor to the communities we impact during construction. We commit to maintain a clean project site; and will work with locals to replace damaged fencing, limit disruption, donate bridge components to local farmers, donate waste dirt, or other items as opportunity arises.	You can't put a price on good will with the local constituents.	This is who we are and is evidenced by our previous work on the Emergency Bridge Packages in York and Anderson Counties as well as many past and ongoing projects.
24	DBE: Provide 2.25% minimum DBE Goals for Professional Services	CTEA commits to exceeding the 0.4% goal for Professional Services with local consultants. (SCDOT benefit).	We believe our cost is lower, schedule is faster and quality is higher through the use of trusted, qualified DBE firms.	CTEA will self impose a penalty of \$10,000 for not achieving a minimum of 2.25% DBE participation for professional services.
25	DBE: Provide 7% minimum DBE participation for subcontractor services	PII commits to exceeding the 11.6% total DBE goal for the contract with local contractors, fabricators and suppliers. (SCDOT benefit).	We believe our cost is lower, schedule is faster and quality is higher through the use of trusted, qualified DBE subcontractors.	PII will self impose a penalty of \$40,000 for not achieving a minimum total of 18% DBE participation for the contract.
26	DBE: Provide 10% minimum DBE participation for fabricator services	PII commits to exceeding the 11.6% total DBE goal for the contract with local contractors and suppliers. (SCDOT benefit).	We believe our cost is lower, schedule is faster and quality is higher through the use of trusted, qualified DBE suppliers.	PII will self impose a penalty of \$40,000 for not achieving a minimum total of 18% DBE participation for the contract.

Appendix C – Approved Formal ATCs
SCDOT Design Build Project
Bridge Package 14 DB Project
Contract ID 1162220





October 14, 2022

Bridge Package 14 DB Project

Contract ID 1162220

Cherokee County, South Carolina

Final Alternative Technical Concepts

The following FATCs are incorporated into this proposal:

None

A handwritten signature in black ink, appearing to read "Greg Canniff", written in a cursive style.

Gregory Canniff

President