

# INDEX OF SHEETS

- SHEET NO. 1 : TITLE SHEET
- " 2 : DETAILS OF FLARED CURB & GUTTER
  - " 3 : CONCRETE TILE SLOPE DRAINS
  - " 4 : PIPE HANDRAIL
  - " 5 & 6 : ROAD PLAN & PROFILE
  - " 7 : BRIDGE PLAN & PROFILE
  - " 8 : BENT FOR 80 FT. SPAN
  - " 9 : PIERS A, B, C, I & J
  - " 10 : PIERS D, G & H
  - " 11 : PIERS E & F
  - " 12 : 30 FT. R.C. SPAN
  - " 13 : 322 FT. SUPERSTRUCTURE
  - " 14 : DETAILS OF 322 FT. SUPERSTRUCTURE
  - " 15 & 16 : 175 FT. TRUSS SPAN - WIDENING DETAILS
  - " 17 : DESIGN PLAN - EXISTING TRUSS SPAN
  - " 18-31 : SHOP PLANS - EXISTING TRUSS SPAN

## SOUTH CAROLINA STATE HIGHWAY DEPARTMENT COLUMBIA

# PLAN AND PROFILE OF PROPOSED STATE HIGHWAY

FEDERAL AID PROJECT NO. F-241(5)

S. C. DOCKET NO. 2226.226.3

U. S. ROUTE NO. 701

GEORGETOWN-HORRY COUNTIES

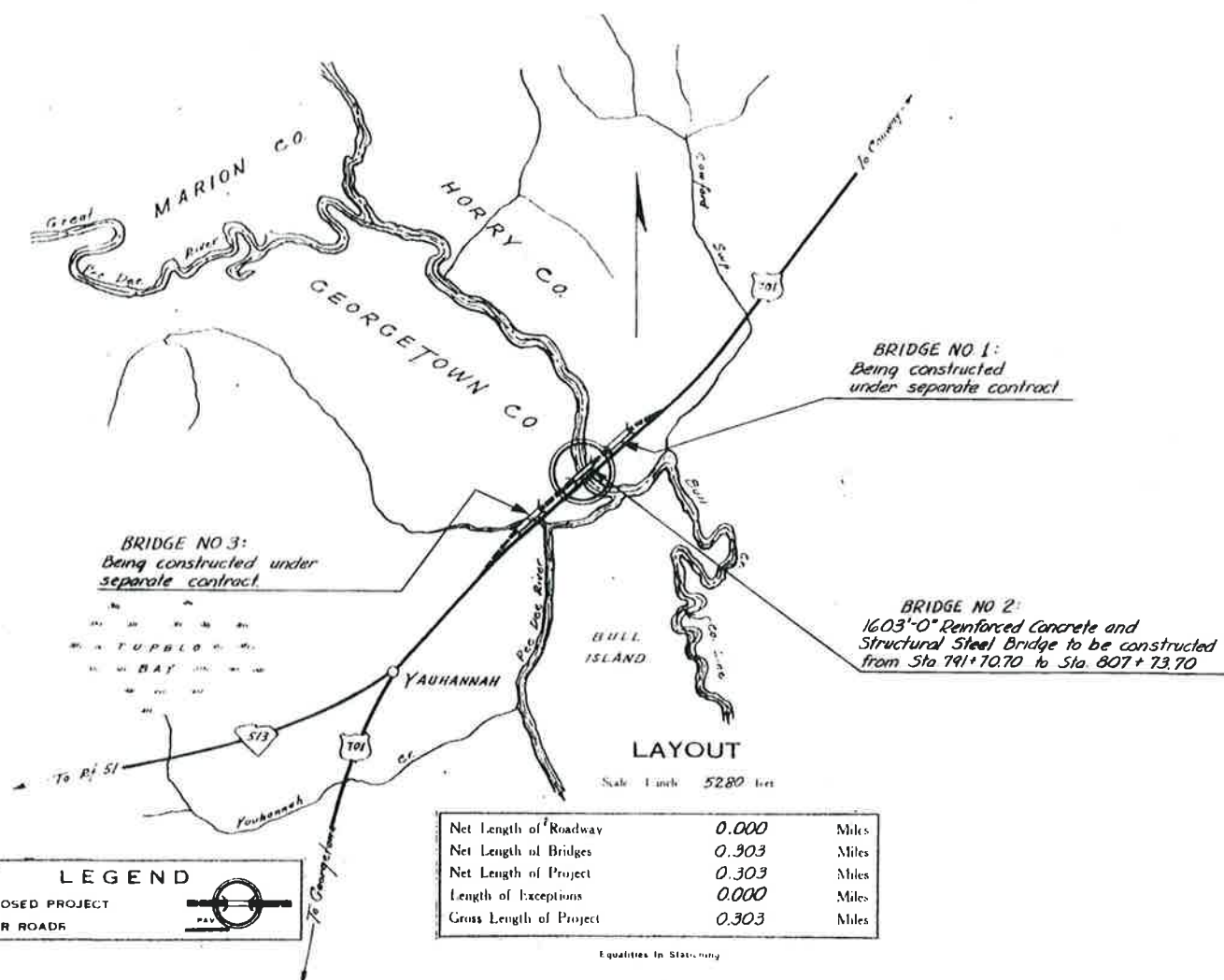
## MAIN BRIDGE NO. 2 OVER GREAT PEE DEE RIVER AT YAUHANNAH

FED. ROAD DIST. NO.	STATE	COUNTY	DOCKET NO.	F. A. PROJ. NO.	SHEET NO.	TOTAL SHEETS
3	S. C.	GEORGETOWN-HORRY	2226.226.3	ND	701	31

### SUMMARY OF ESTIMATED QUANTITIES

DRY EXCAVATION	320 CY.
WET EXCAVATION	3375 CY.
CLASS 'AA' CONCRETE	1422.76 CY.
CLASS 'A' CONCRETE	3551.07 CY.
REINFORCING STEEL	464,919 LBS.
NEW STRUCTURAL STEEL	*NECESSARY L.S.
UNTREATED TIMBER PILING	16,320 L.F.
18" PRE-CAST CONCRETE PILING	5360 L.F.
CONCRETE TILE SLOPE DRAINS	212 L.F.
PIPE HANDRAIL	3206 L.F.
ROADWAY FLOOR FOR TRUSS SPAN	NECESSARY L.S.
REMOVING, TRANSPORTING, WIDENING & RE-ERECTING 175 FT. THRU TRUSS	NECESSARY L.S.
PAINTING OLD STRUCTURAL STEEL (175 FT. TRUSS SPAN)	NECESSARY L.S.
REMOVING AND DISASSEMBLING TWO EXISTING 175 FT. DECK TRUSS SPANS	NECESSARY L.S.

\*APPROX. 715,300 LBS.



### CONVENTIONAL SIGNS

State Line	Trolley Poles	• • • • •
County Line	Power Poles	• • • • •
City or Town Limits	Telephone or Telegraph Poles	• • • • •
Property Line	Marsh	• • • • •
Fence	Trees	• • • • •
Marshland Weir	Brush	• • • • •
Drinking Fountain	Slumps	• • • • •
Water and Power Lines of	Buildings	• • • • •
Power and Water	Bridge	• • • • •
Railroad	Concrete Box Culvert	• • • • •
Water or Embankment	Pipe Culvert	• • • • •
Board Walk	Drop Inlet and Culvert	• • • • •
Point of Intersection (P.I.)	Hub on Center Line	• • • • •

Note: All workmanship and material on this project to conform with South Carolina State Highway Department Standard Specifications for Highway Construction dated March 15th 1939, as approved by the Administrator, Federal Works Agency.

APPROVED:  
*S. J. Roman* 12/16/50  
STATE HIGHWAY ENGINEER DATE

DEPARTMENT OF COMMERCE  
BUREAU OF PUBLIC ROADS

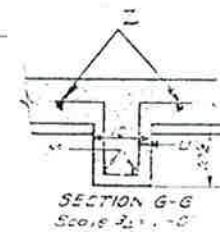
RECOMMENDED FOR APPROVAL

DISTRICT ENGINEER DATE

APPROVED:

DIVISION ENGINEER DATE





SEAL AS NOTED

C 26 30 2.5 194.

All dimensions relative to reinf steel are to		
BREAK-DOWN OF CONCRETE QUANTITIES		
	1 INT. SPAN	1 END SPAN
Beams, Slabs, Brackets	29.81 CY	31.49 CY
Sidewalks	3.96 CY	4.05 CY
Fasts & Railing	1.52 CY	1.58 CY
Totals	35.29 CY	37.12 CY

AS NOTED C 26 30 2.5 1944

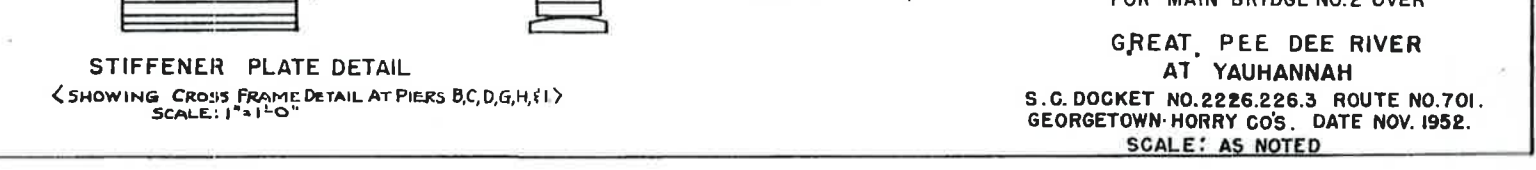
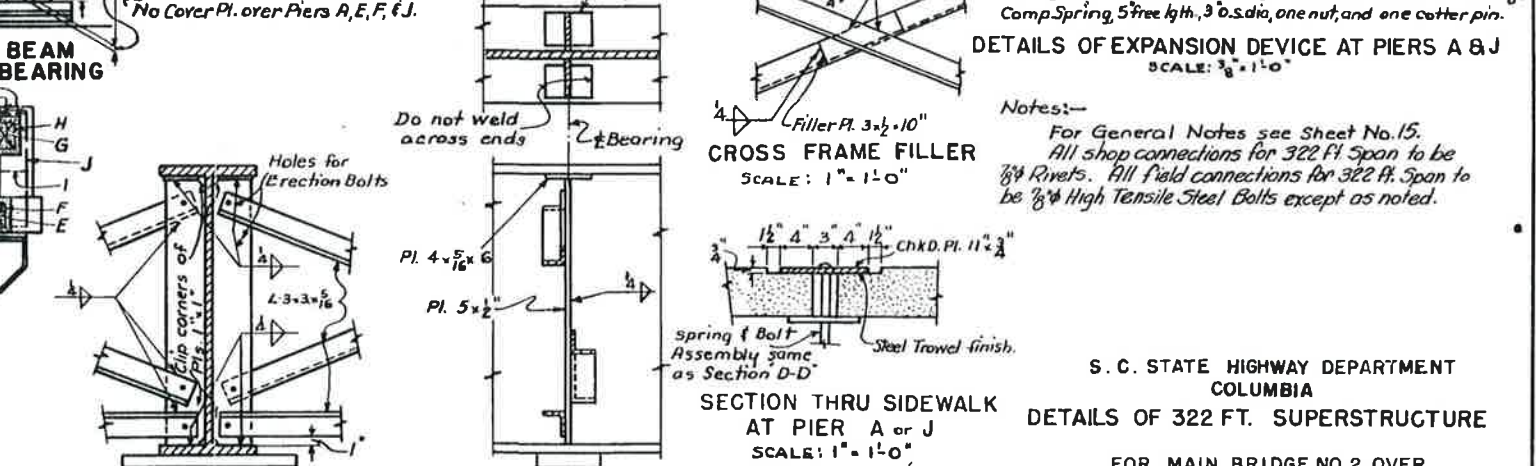
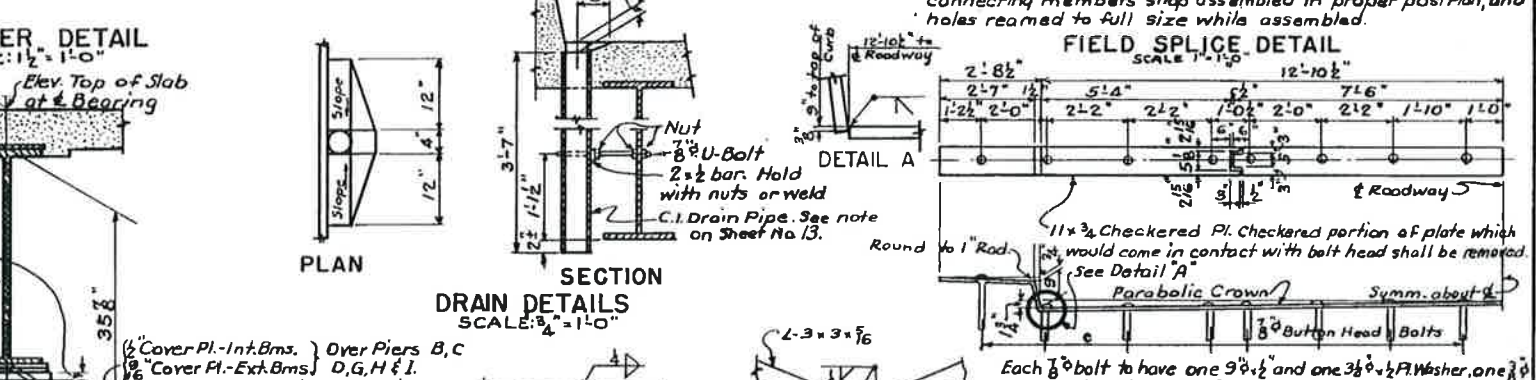
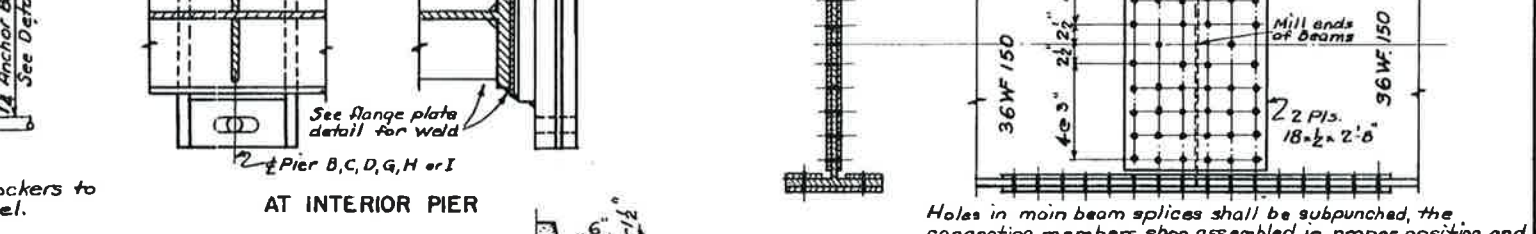
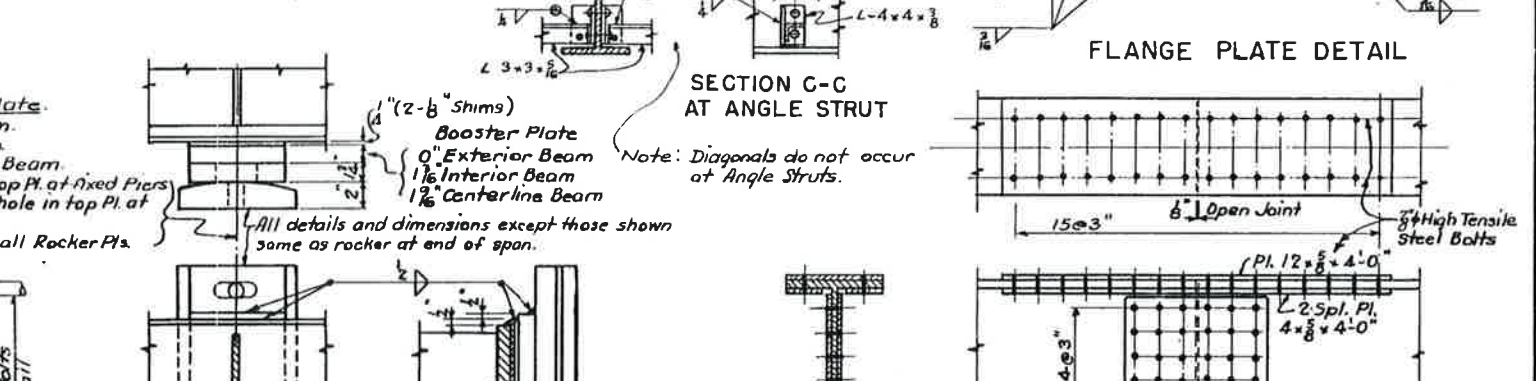
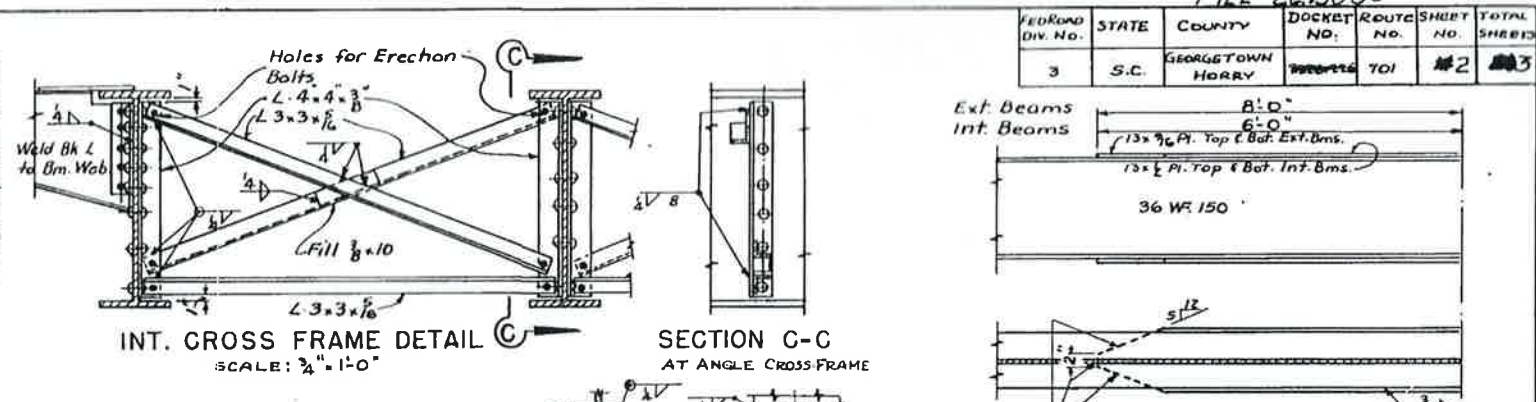
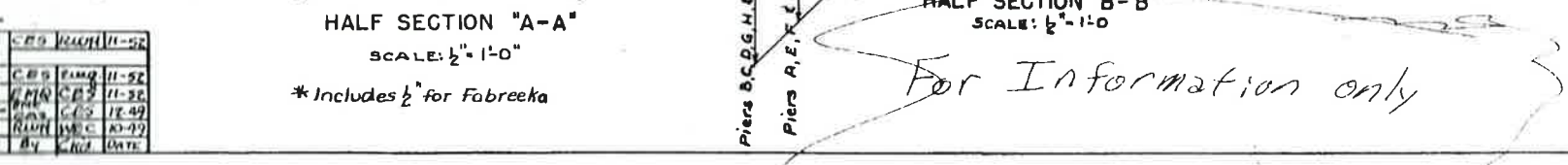
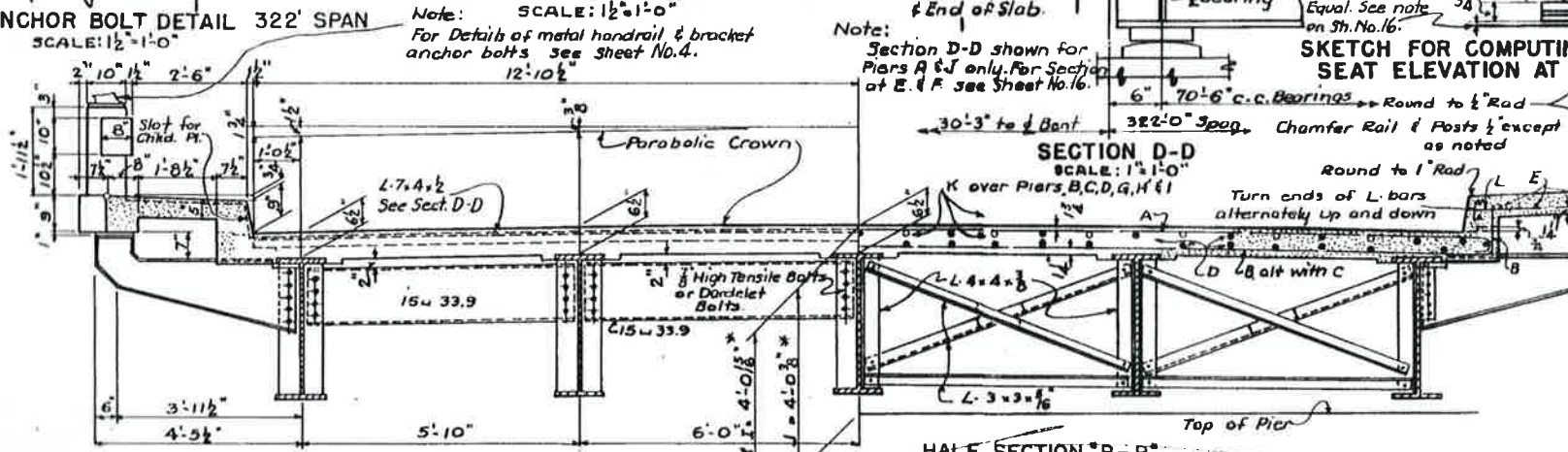
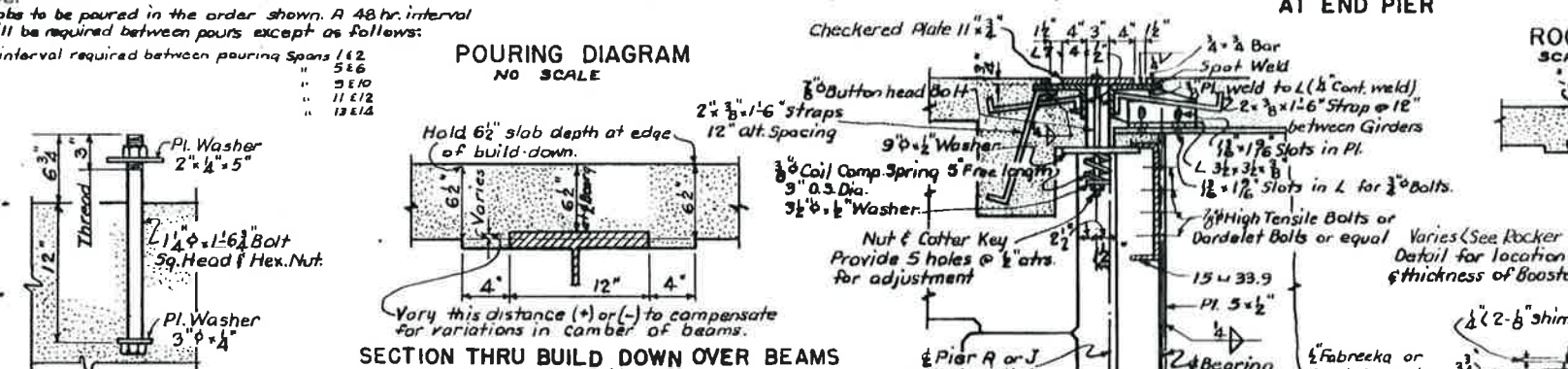
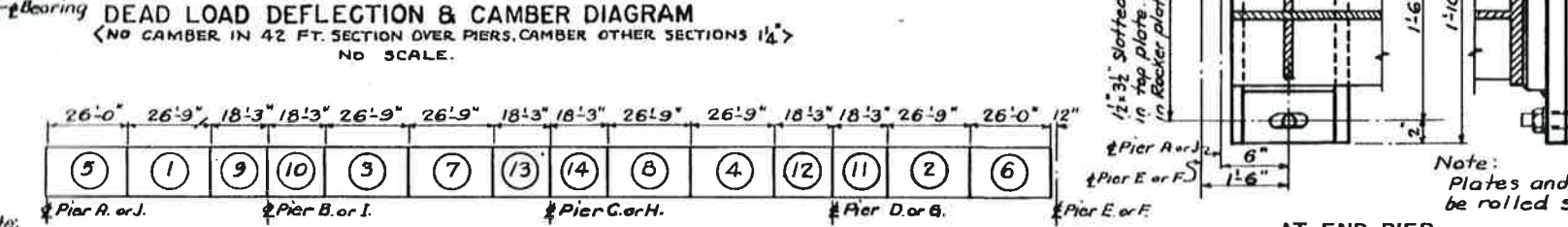
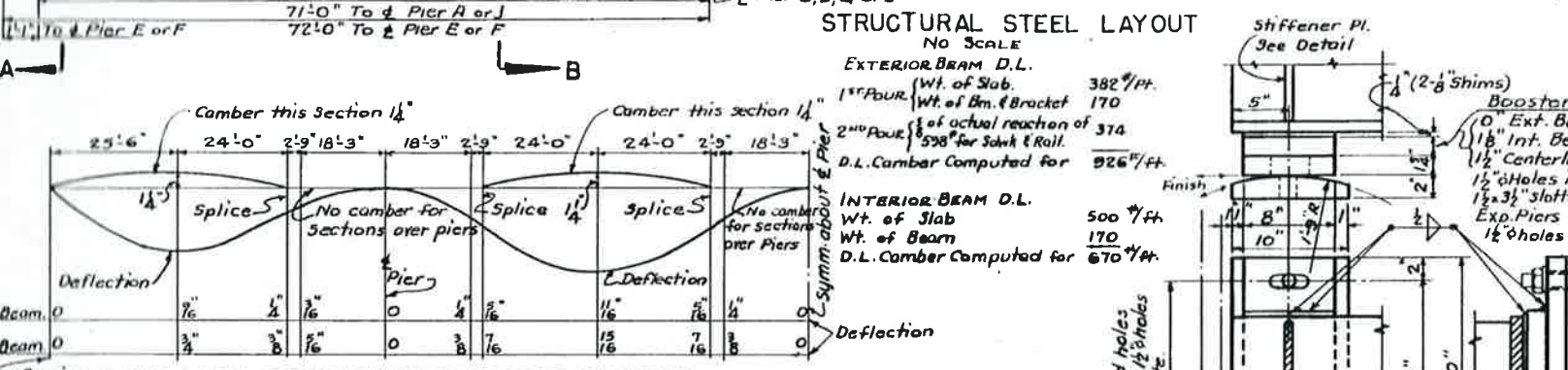
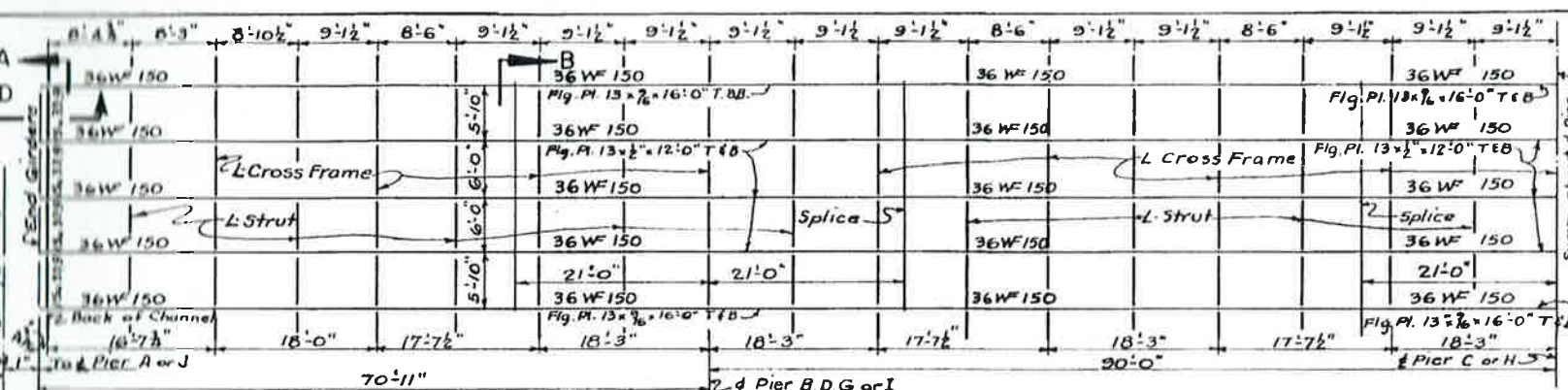
FOR INFORMATION  
ONLY







FED. ROAD DIV. NO.	STATE	COUNTY	DOCKET NO.	ROUTE NO.	SHEET NO.	TOTAL SHEETS
3	S.C.	GEORGETOWN	HORRY	701	#2	#3

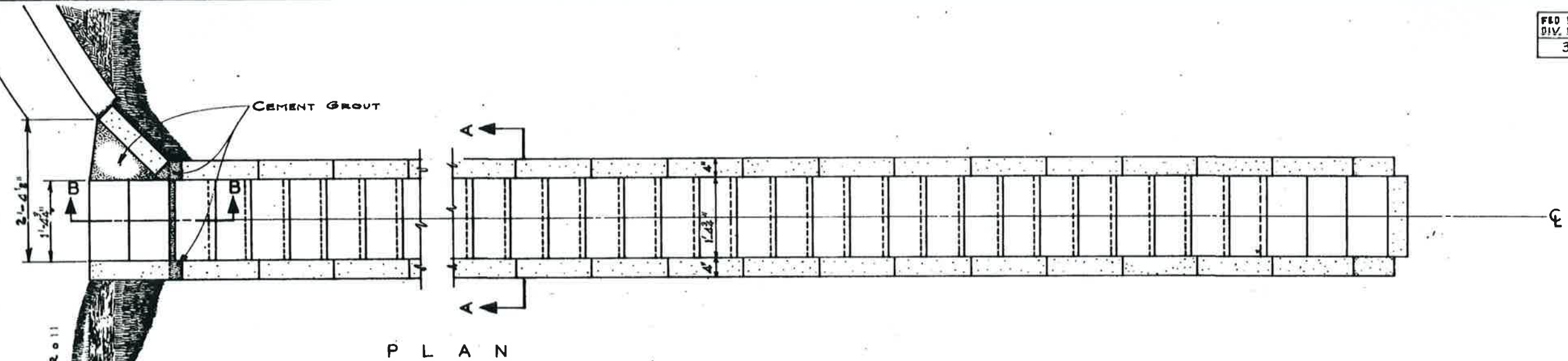


S. C. STATE HIGHWAY DEPARTMENT  
COLUMBIA  
DETAILS OF 322 FT. SUPERSTRUCTURE  
FOR MAIN BRIDGE NO. 2 OVER  
GREAT PEE DEE RIVER  
AT YAUHANNAH  
S. C. DOCKET NO. 2226.226.3 ROUTE NO. 701.  
GEORGETOWN-HORRY COS. DATE NOV. 1952.  
SCALE: AS NOTED



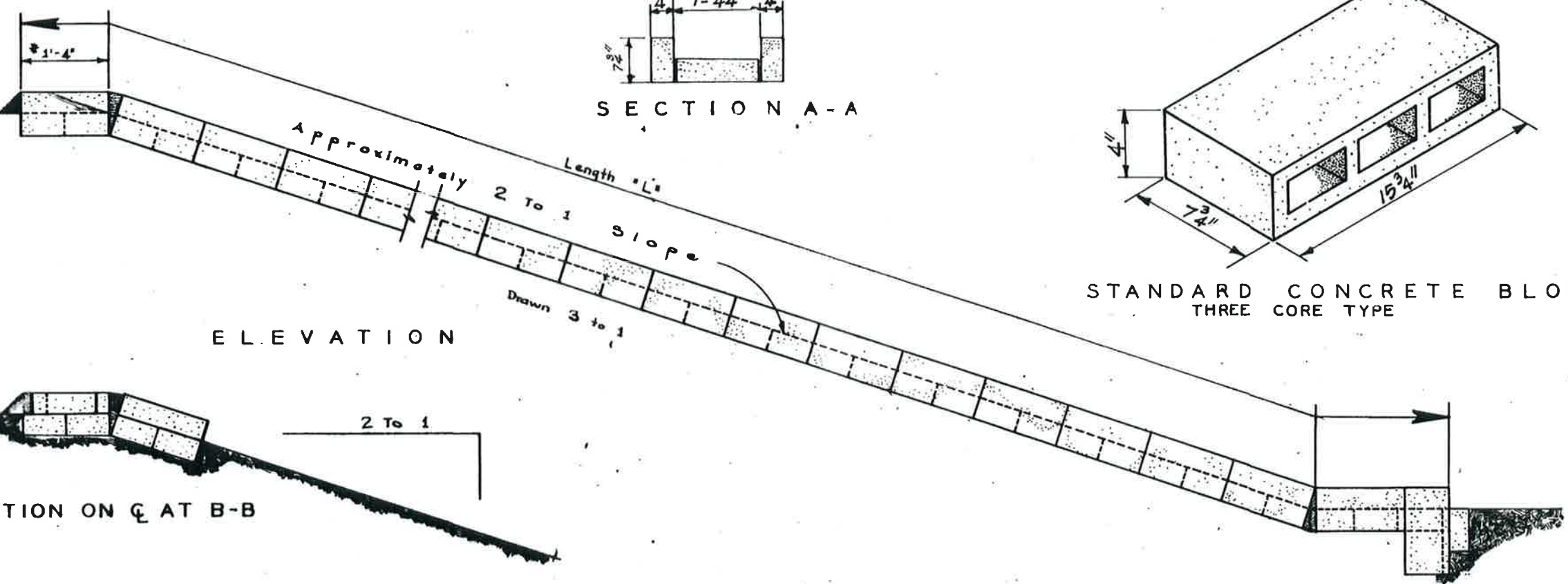
FED. ROAD DIV. NO.	STATE	COUNTY	DOCKET NO.	ROUTE NO.	SHEET NO.	TOTAL SHEETS
3	S.C.	GEORGETOWN Horry	2226.226	701	3	31

ROADWAY

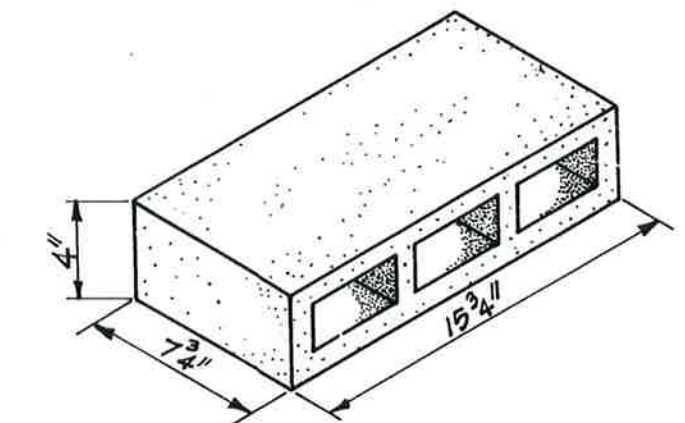


P L A N

Resident Engineer to vary this dimension to suit shoulder

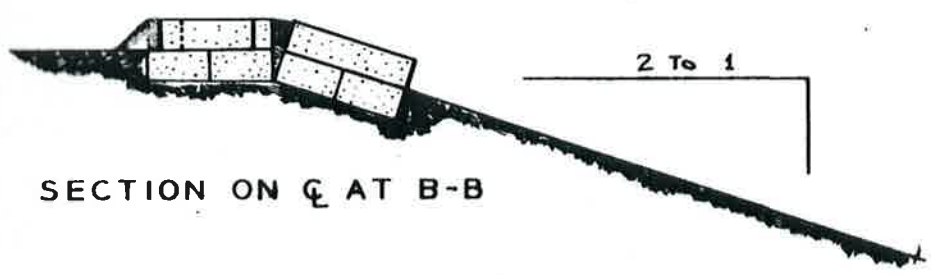


SECTION A-A



STANDARD CONCRETE BLOCK  
THREE CORE TYPE

E L E V A T I O N



SECTION ON Q AT B-B

TOTAL LENGTH 212 FT.

Scale 1" = 1Ft

NOTE: Necessary concrete and extra blocks at end of drain will be paid for as 2 feet of slope drain at unit price bid per lin. ft. All blocks shall be laid with a 1/2" joint of 1:3 Mortar. All costs of necessary excavation for the construction of the slope drains shall be included in the unit-price bid per lin. ft. of concrete Tile Slope Drains.

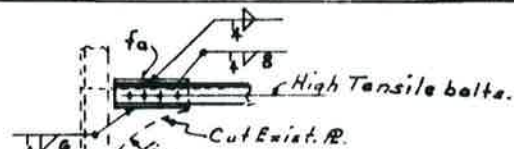
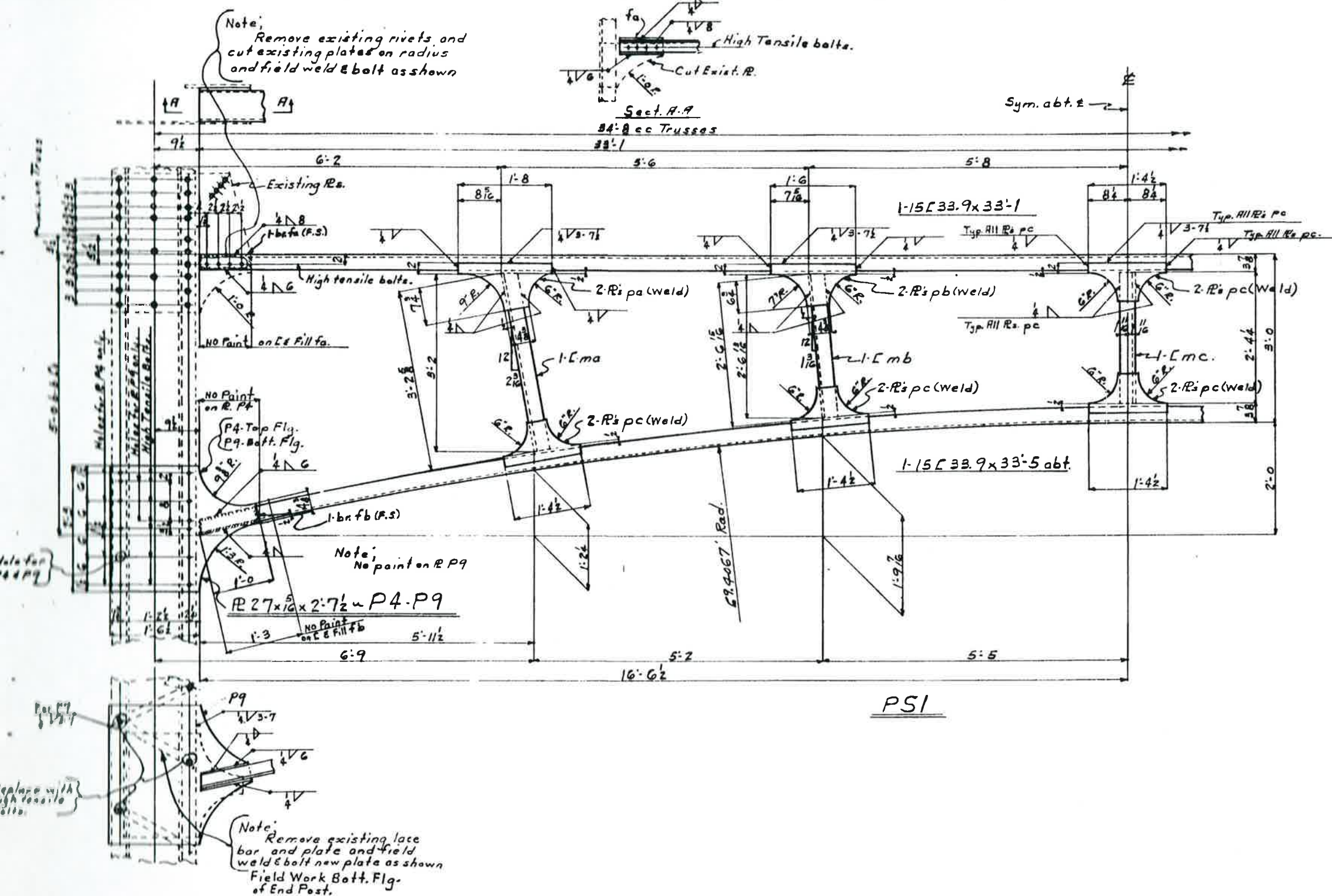
S . C . STATE HIGHWAY DEPARTMENT  
COLUMBIA

**TYPICAL CONCRETE TILE  
SLOPE DRAINS**

GEORGETOWN  
S C DOCKET NO. 2226.226 Horry COUNTY  
ROUTE NO. 701 DATE DEC. 1938

Sheet	2226.226-32
Drawn	W.M.T.
Check	W.M.T.
App'd	
Date	





Sect. A-A  
34'-0" cc Trusses  
33'-1"

Sym. abt.  $\epsilon$  —

1-15 [33.9 x 33'-1]

1-15 [ 33.9 x 33'-5 aft.

PSI

Portal Struts.  
Widening One 175'-0 Thru Truss Span  
Great Pee Dee River Bridge  
Georgetown & Horry Counties, S.C.  
F.R. Project No F241 (5).  
S.C. Docket No 222G. 22G. 3.  
McMeekin Const. Co.

MATERIAL				ASSEM. BLIND MARK	RE MARKS	CALCULATED WEIGHT FOR ONE SHIP. PIECE	ORDERED		ASSEM. BLIND MARK	RE MARKS	CALCULATED WEIGHT FOR ONE SHIP. PIECE	ORDERED		ASSEM. BLIND MARK	RE MARKS	CALCULATED WEIGHT FOR ONE SHIP. PIECE	ORDERED		ASSEM. BLIND MARK	RE MARKS	CALCULATED WEIGHT FOR ONE SHIP. PIECE	ORDERED	
LINE	SHAPE	LENGTH Feet Inches	ITEM				LINE	SHAPE				LENGTH Feet Inches	ITEM				LINE	SHAPE				LENGTH Feet Inches	ITEM
2	Plates	5' 0"	5	16	2 Plates ~ P4			31					46										
2	Plates	5' 0"	5	17	2 Plates ~ P4			32					47										
2	Plates	5' 0"	5	18	2 Plates ~ P4			33					48										
2	Plates	5' 0"	5	19	2 Plates ~ P4			34					49										
2	Plates	5' 0"	5	20	2 Plates ~ P4			35					50										
2	Plates	5' 0"	5	21	2 Plates ~ P4			36					51										
2	Plates	5' 0"	5	22	2 Plates ~ P4			37					52										
2	Plates	5' 0"	5	23	2 Plates ~ P4			38					53										
2	Plates	5' 0"	5	24	2 Plates ~ P4			39					54										
2	Plates	5' 0"	5	25	2 Plates ~ P4			40					55										
2	Plates	5' 0"	5	26	2 Plates ~ P4			41					56										
2	Plates	5' 0"	5	27	2 Plates ~ P4			42					57										
2	Plates	5' 0"	5	28	2 Plates ~ P4			43					58										
2	Plates	5' 0"	5	29	2 Plates ~ P4			44					59										
2	Plates	5' 0"	5	30	2 Plates ~ P4			45					60										

Rivets	Non
--------	-----

Holes	✓
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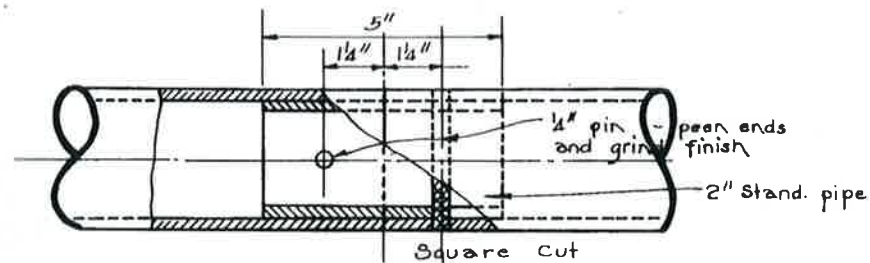
PAINT yes (Seen)  
SHOP CONTACT 102  
SURFACES NO

F	
E	
D	
C	
B	
X	
REVISION	

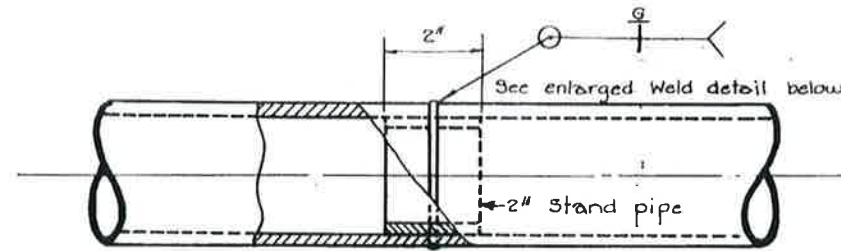
DRAWINGS MADE AT Roanoke PLANT  
WORK FABRICATED AT Roanoke PLANT  
IN CHARGE OF Crossgrove  
DRAW. MADE BY E.W.S. DATE 6-18-69  
DRAW. CHECKED BY THL DATE 7-1-69

ORDER No. Q3619 SHEET No. 106



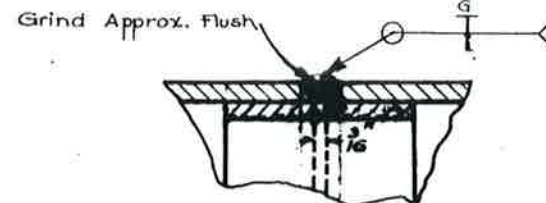


ALTERNATE No 1

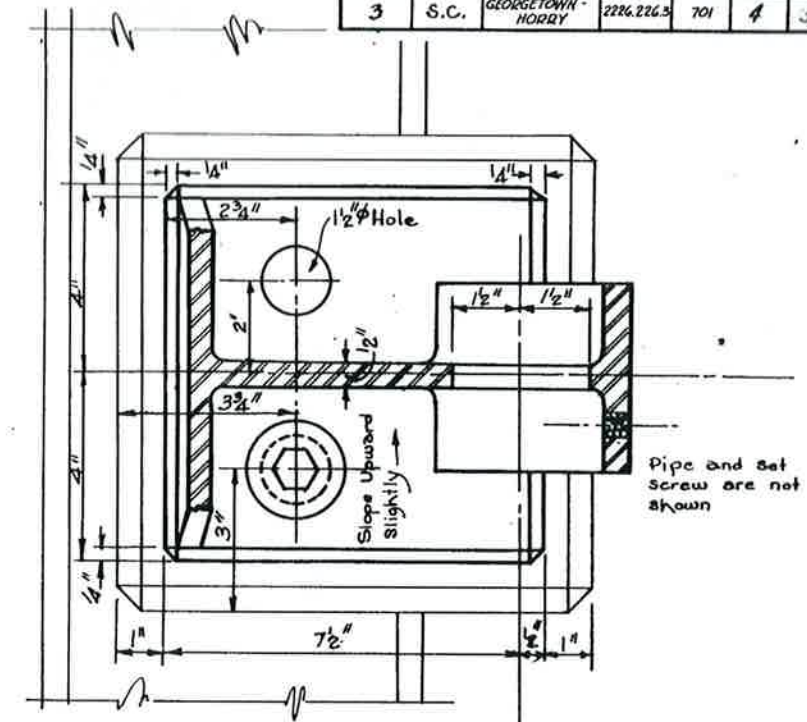


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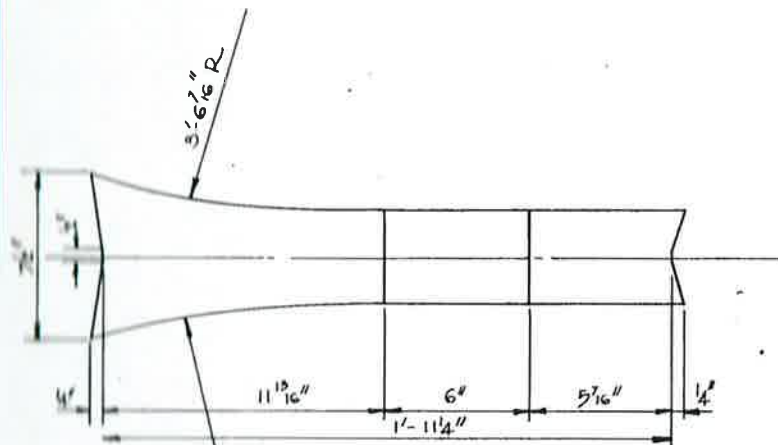
DETAIL OF PIPE JOINT



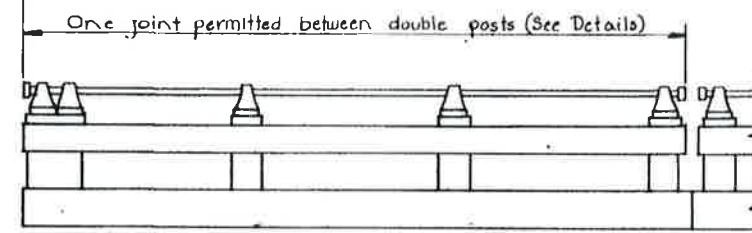
DETAIL OF WELDED JOINT  
FULL SIZE



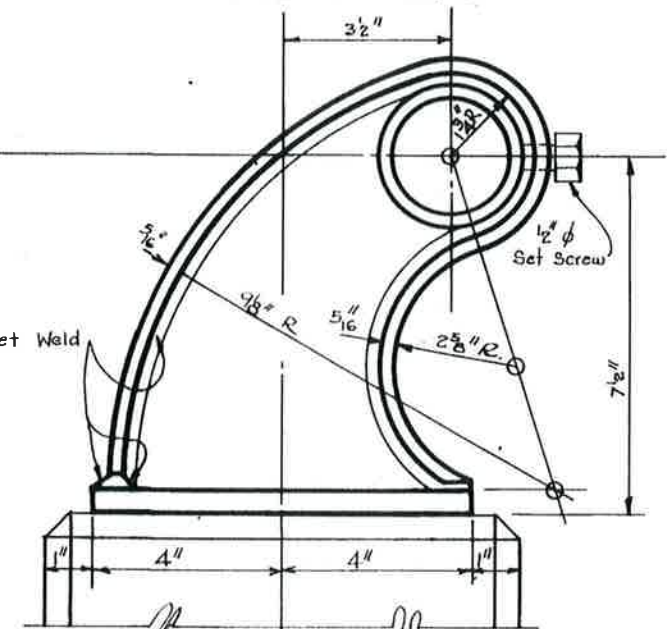
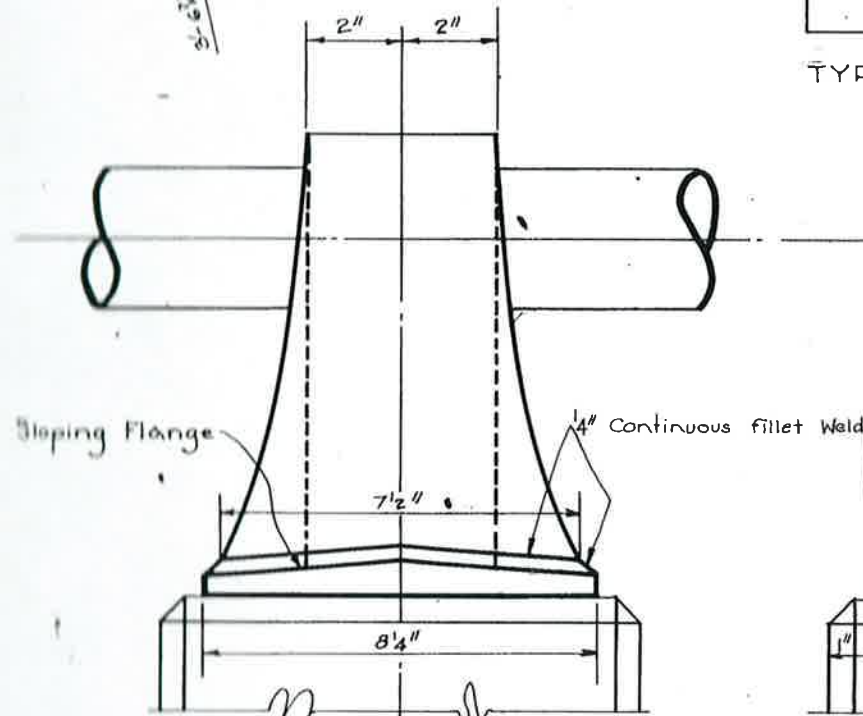
SECTION A-A



DEVELOPED VIEW OF COVER PLATE



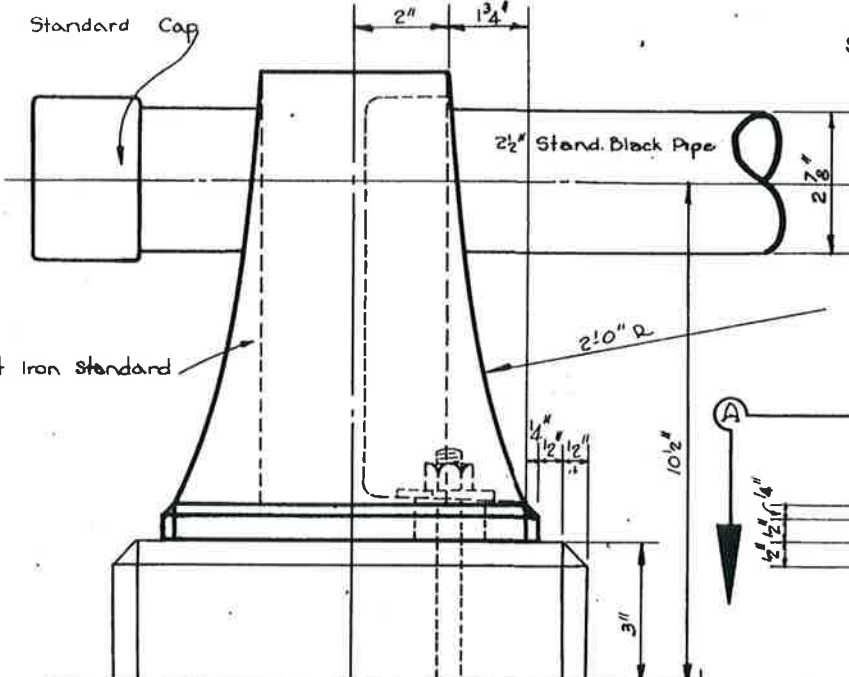
TYPICAL PANEL OF RAILING



Shape plate to fit periphery of stem and weld all contacts

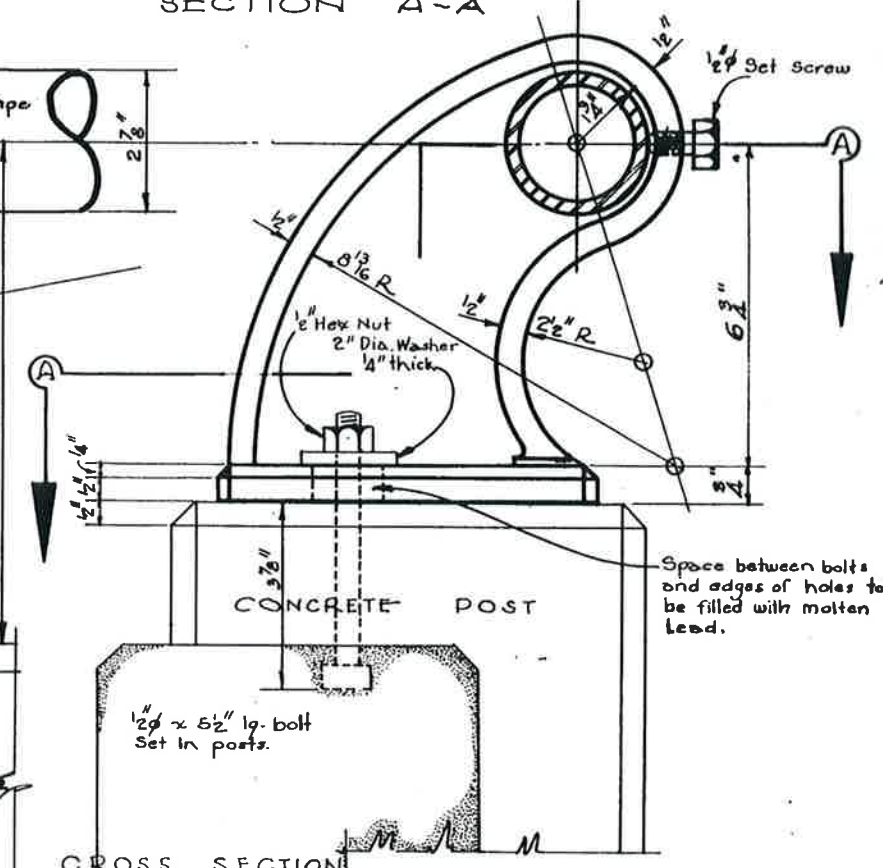
Stem flange

Section cut from 10" T @ 31"



Note: Bottom surface of cast bracket shall be finished up as to be perpendicular to vertical axis and to within 1/16" of a true plane.

SIDE ELEVATION  
As seen from Roadway  
Scale - Half Size



CROSS SECTION

Details not shown shall be the same or as closely similar as practicable to those shown for Cast Iron Standard. Accessible welds shall be ground to a smooth surface. One inch thick between post top and standard.

Approx. Weight = 24 Lbs. Each.

FABRICATED RAILING STANDARD

The Contractor may use either the Fabricated or Cast Iron Standard

DETAIL FOR CAST IRON BRACKET

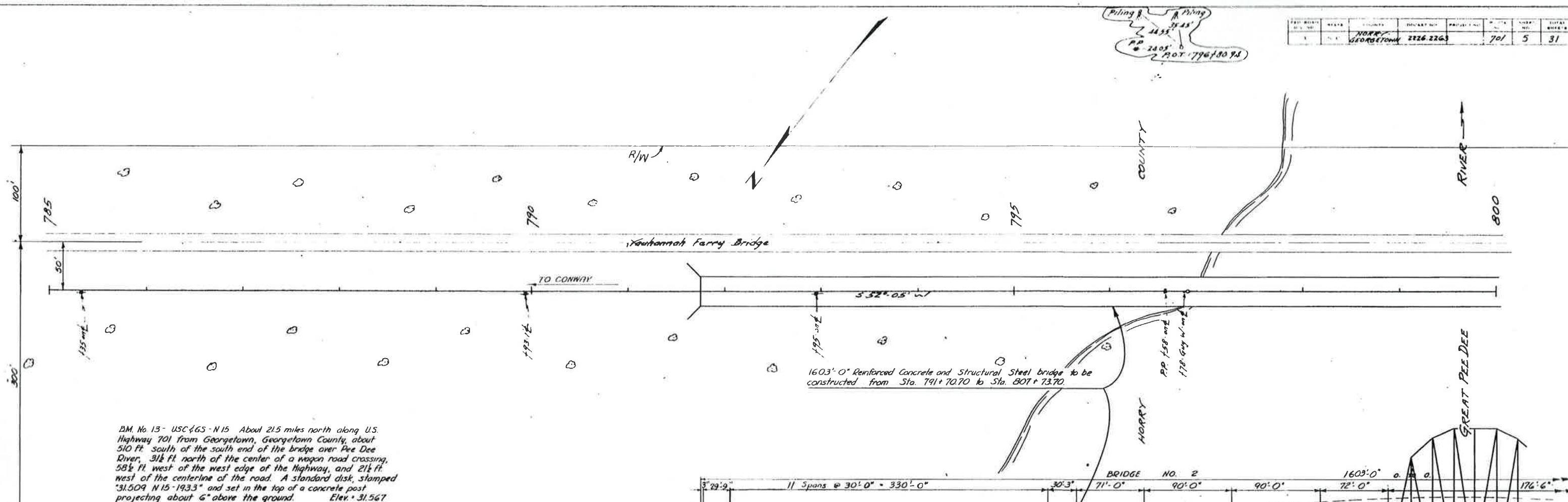
S. C. STATE HIGHWAY DEPARTMENT  
COLUMBIA  
DETAILS OF PIPE RAILING:  
FOR MAIN BRIDGE NO. 2 OVER  
GREAT PEE DEE RIVER  
AT YAUHANNAH

DOCKET NO. 2226.226.3 US ROUTE NO. 701  
COUNTY GEORGETOWN-HORRY DATE DEC. 1932

REV.	DATE	BY	CHKD	DATE
1	10-10-32	W.B.T.	W.B.T.	10-10-32
2	11-10-32	W.B.T.	W.B.T.	11-10-32

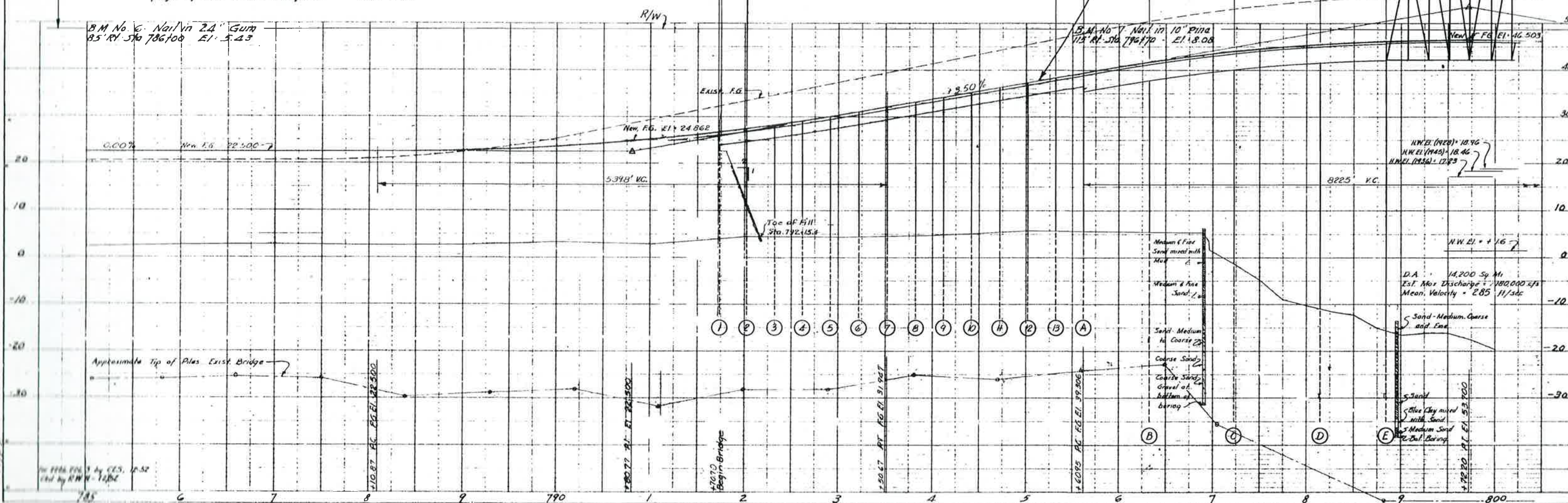


PROJECT	DATE	BY	CHKD	APP'D	SCALE	SHEET	TOTAL
NO. 13	1935	W. H. H.			1" = 100'	5	31



B.M. No. 13 - USC 465 - N15 About 215 miles north along U.S. Highway 701 from Georgetown, Georgetown County, about 510 ft. south of the south end of the bridge over Pee Dee River, 3 1/8 ft. north of the center of a wagon road crossing, 58 1/2 ft. west of the west edge of the highway, and 21 1/2 ft. west of the centerline of the road. A standard disk, stamped "31.509 N15-1933" and set in the top of a concrete post projecting about 6" above the ground. Elev. 31.567

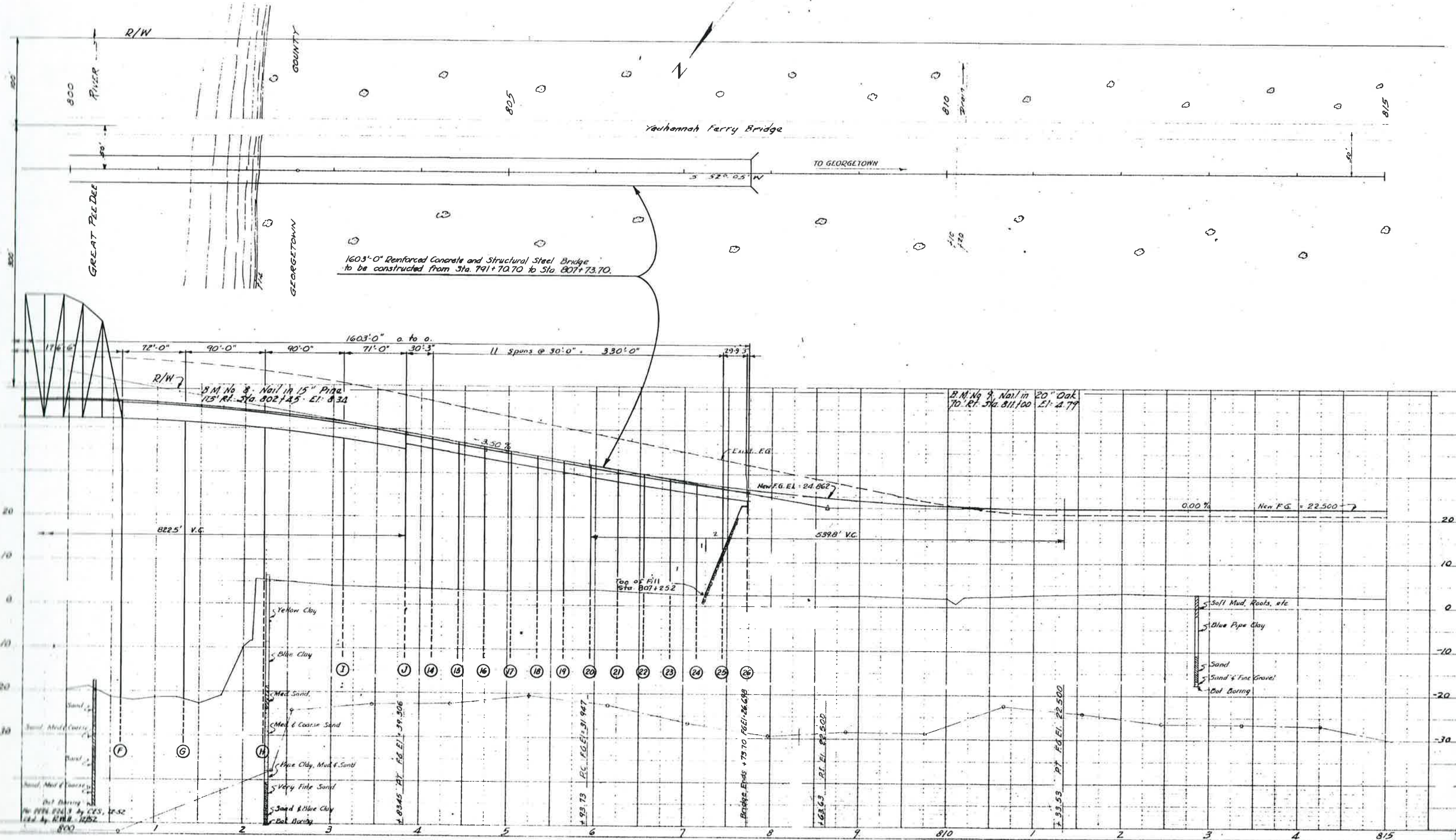
B.M. No. 6 - Nail in 2 1/2" Gum  
85' RT Sta 786+00 El. 54.3



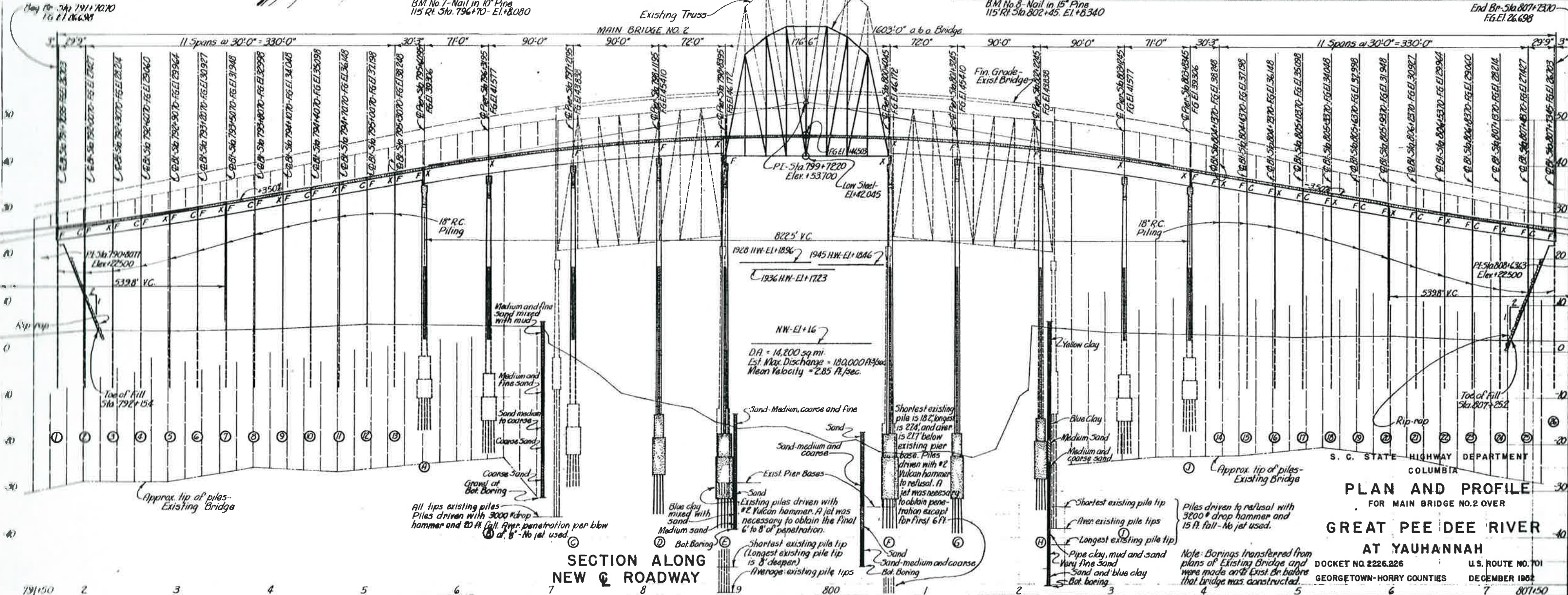
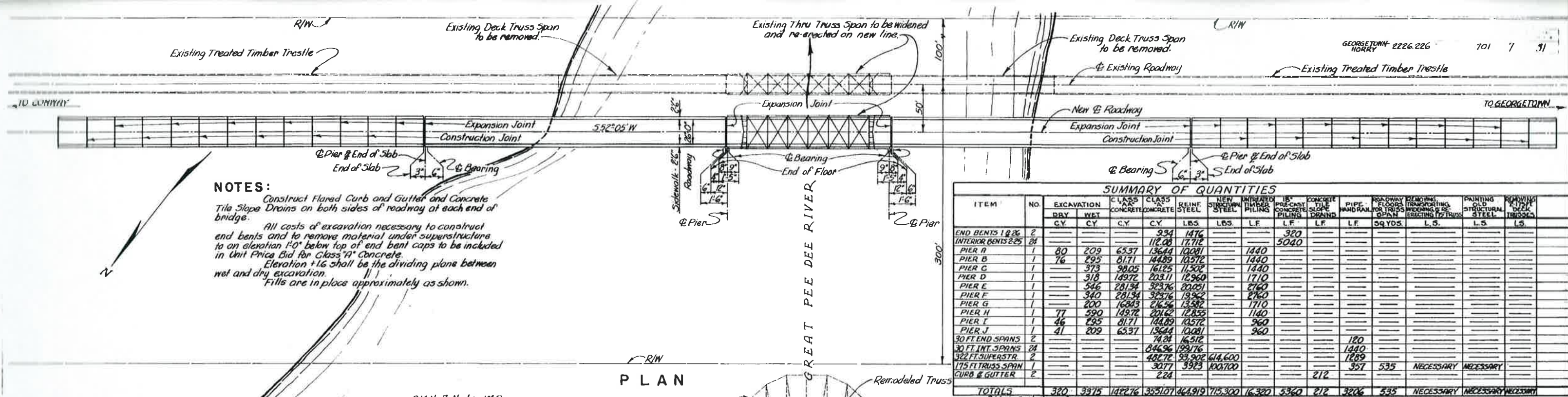
IN 1935 FILED BY CES. 12-52  
CHKD BY RWH-1252



Bridge Piling  
 37.55'  
 34.60'  
 POT 802+59.73  
 45.35  
 0.5' Hinge







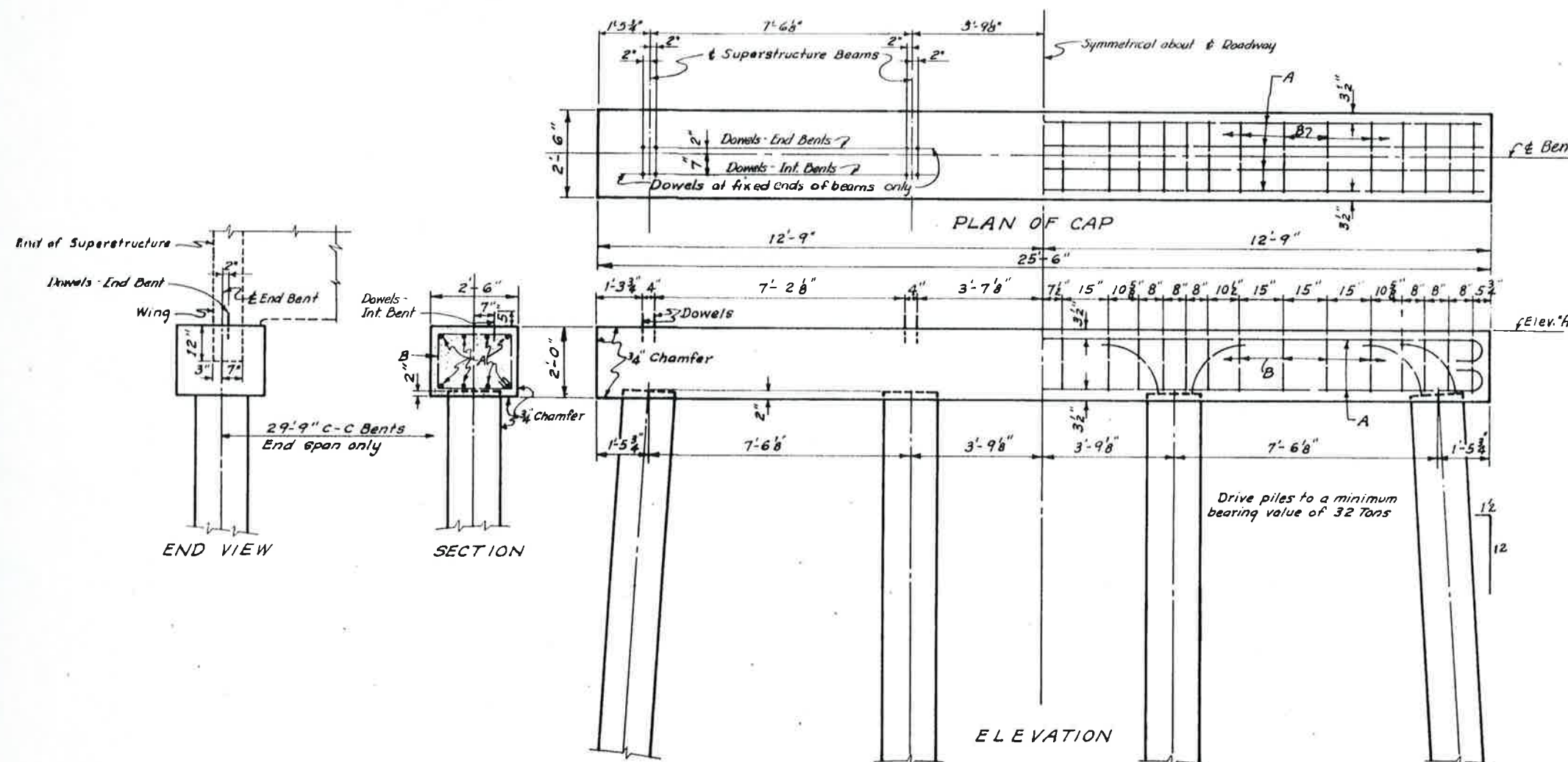


FED. ROAD DIV. NO.	STATE	COUNTY	DOCKET NO.	ROUTE NO.	SHEET NO.	TOTAL SHEETS
3	S. C.	GEORGETOWN-HORRY	2226226	701	8	31

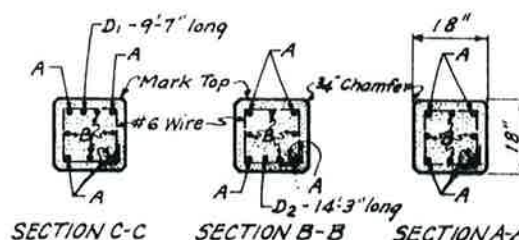
STEEL SCHEDULE				BENDING DETAILS	
MARK	NO. PROD.	SIZE	LENGTH		
A	8	8	27'-0"	A	
B	28	4	7'-8"	B	
Dowels	8	8	0'-10"	Dowels	
Class A Concrete 4.67 CY					
Reinforcing Steel 738 Lbs					

$\frac{2\frac{3}{4}}{4}$  Hook for #4 & smaller bars add 6" per hook.  
 $\frac{3\frac{3}{4}}{4}$  Hook for #5 & #6 bars add 8" per hook.  
 $\frac{4\frac{3}{4}}{7}$  Hook for #7 & larger bars add 12" per hook.

**NOTES:**  
 All concrete to be class 'A'.  
 Structural grade Reinforcing Steel not permitted.  
 All exposed edges to be chamfered  $\frac{3}{4}$ " unless otherwise noted.  
 All dimensions relative to reinforcing steel are to centers of bars.

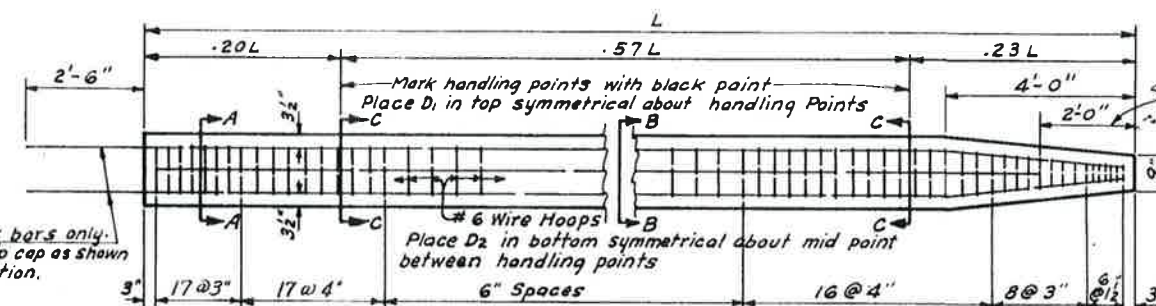


MARK	UNDER 45	45-50	50.1-55	55.1-60
A	1" (#8)	1" (#8)	1" (#8)	1" (#8)
B	1" (#8)	1" (#8)	1" (#8)	1" (#8)
D1, D2	OMIT	1" (#8)	1" (#8)	1" (#8)



SECTION C-C: Bend B bars over to third point to make room for D bars.  
 SECTION B-B: Bend B bars over to third point to make room for D bars.

SECTION A-A: Corner bars only. Bend into cap as shown in elevation.



DETAIL OF 18" SQUARE CONCRETE PILE

QUANTITIES FOR 50'-0" PILE  
 Concrete = 4.028 CY, Steel = 1214 Lbs.  
 For quantities for lengths greater or less than 50'-0" add or deduct 0.0833 CY of concrete and 22 lbs of steel per foot. (22# of steel per foot is approximate)

BENT NO.	ELEVATION 'A'
1 or 26	23.948
2 or 25	24.677
3 or 24	25.464
4 or 23	26.310
5 or 22	27.214
6 or 21	28.177
7 or 20	29.198
8 or 19	30.248
9 or 18	31.298
10 or 17	32.348
11 or 16	33.398
12 or 15	34.448
13 or 14	35.498

S. C. STATE HIGHWAY DEPARTMENT  
COLUMBIA

**BENT FOR 30' SPAN**  
 FOR MAIN BRIDGE NO 2 OVER  
**GREAT PEE DEE RIVER**  
 AT YAUHANNAH

DOCKET NO. 2226226 U.S. ROUTE NO. 701  
 GEORGETOWN-HORRY COUNTIES DATE DEC. 1952

SCALE  $\frac{1}{2}$ " = 1'-0"

Rev.	CHS	NWH	12-52
By	PPG	PPG	3
Quoted	JVD	ATC	8-44
Checked	JVD	ATC	8-44
Designed	JVD	ATC	8-44
Drawn	JVD	ATC	8-44
By	CHS	Date	



FED. RD. DIST. NO.	STATE	COUNTY	DOCKET NO.	ROUTE NO.	SHEET NO.	TOTAL SHEETS
3	S.C.	GEORGETOWN	2226.226	701	9	31

# REINFORCING STEEL SCHEDULE

MARK	NO.	PIER "A"	PIER "B"	PIER "C"	PIER "I"	PIER "J"	D
A	5	80	80	80	80	80	80
B	10	24	24	24	24	24	24
C	10	24	24	24	24	24	24
C <sub>1</sub>	3	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4
C <sub>2</sub>	3	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4
C <sub>3</sub>	3	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4
C <sub>4</sub>	3	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4
C <sub>5</sub>	3	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4
C <sub>6</sub>	3	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4
C <sub>7</sub>	3	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4
C <sub>8</sub>	3	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4
C <sub>9</sub>	3	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4
C <sub>10</sub>	3	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4	11-3/4
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## SECTION "B-B"

SCALE: 3/8" = 1'-0"

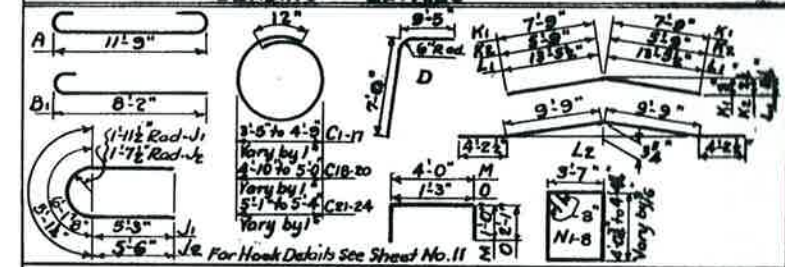
## SECTION "C-C"

SCALE: 3/8" = 1'-0"

## DETAIL OF STRIPE

FULL SCALE

## BENDING DETAILS



## QUANTITIES

	PIER "A"	PIER "B"	PIER "C"	PIER "I"	PIER "J"
Dry Excavation	80 CY	76 CY	—	46 CY	41 CY
Wet Excavation	209 CY	295 CY	373 CY	295 CY	209 CY
Class "A" Concrete	136.44 CY	144.89 CY	161.25 CY	144.89 CY	136.44 CY
Class "AR" Concrete	65.37 CY	81.71 CY	98.05 CY	81.71 CY	65.37 CY
Reinforcing Steel	210.08 Lbs	210.57 Lbs	211.50 Lbs	210.57 Lbs	210.08 Lbs
Untreated Timber Piling	1440 LF	1440 LF	1440 LF	960 LF	960 LF

- ① Wt of one 1/2 Expansion Bolt - 15.94 Lbs. (Incl. wt. of bolt).
- ② Includes 16 Exp. Bolt Assemblies (Wt. 255 Lbs.), 10 Anchor Bolt Assemblies (Wt. 89 Lbs.) and 8 Dowels (Wt. 18 Lbs.).
- ③ Includes 18 Exp. Bolt Assemblies (Wt. 255 Lbs.) and 10 Anchor Bolt Assemblies (Wt. 89 Lbs.).

NOTES:  
For General Notes see Sheet No. 15  
Anchor Bolts to be paid for as reinforcing steel and are included in Pier Quantities. See Sheet No. 14 for details.  
No cable clips required.  
Footings may be lowered a maximum of 2'-0" without providing additional vertical column steel by decreasing length of splice.  
Drive piles to a minimum bearing value of 18 Tons per Pile.  
All Seal concrete to be Class "AR". All other concrete to be Class "A".  
Build-up to be cast monolithic with cap.  
For special requirements and payment of pier quantities see Special Provisions.

S. C. STATE HIGHWAY DEPARTMENT  
COLUMBIA

PIERS A, B, C, I, & J.

FOR MAIN BRIDGE NO. 2 OVER  
GREAT PEE DEE RIVER  
AT YAUHANNAH  
DOCKET NO. 2226.226 U.S. ROUTE NO. 701  
GEORGETOWN-HORRY COUNTIES NOV. 1952  
SCALE: 1/4" = 1'-0" OR AS NOTED

## MAXIMUM FOOTING REACTION

Dead Load, Superstructure	200 K
Live Load, Superstructure	86 K
Dead Load, Pier	582 K
Buoyancy on Pier (Water Elev. 0.0)	148 K
Backfill (14.5' Net Wt.)	145 K
Total of above	805 K

Average Bearing - 16.8 Tons/Pile

Maximum Pile Bearing due to Wind:

Long Wind Comp. on Superstr. (For 45° Wind - 25% of 30° Trans. W.) = 3.4 T/Pile

Transv. Wind Comp. on Superstr. (For 45° Wind - 50% of 30° Trans. W.) = 1.2 T/Pile

Long Wind Comp. on Pier (For 45° Wind - 50% of 30° W. L. Pier) = 2.6 T/Pile

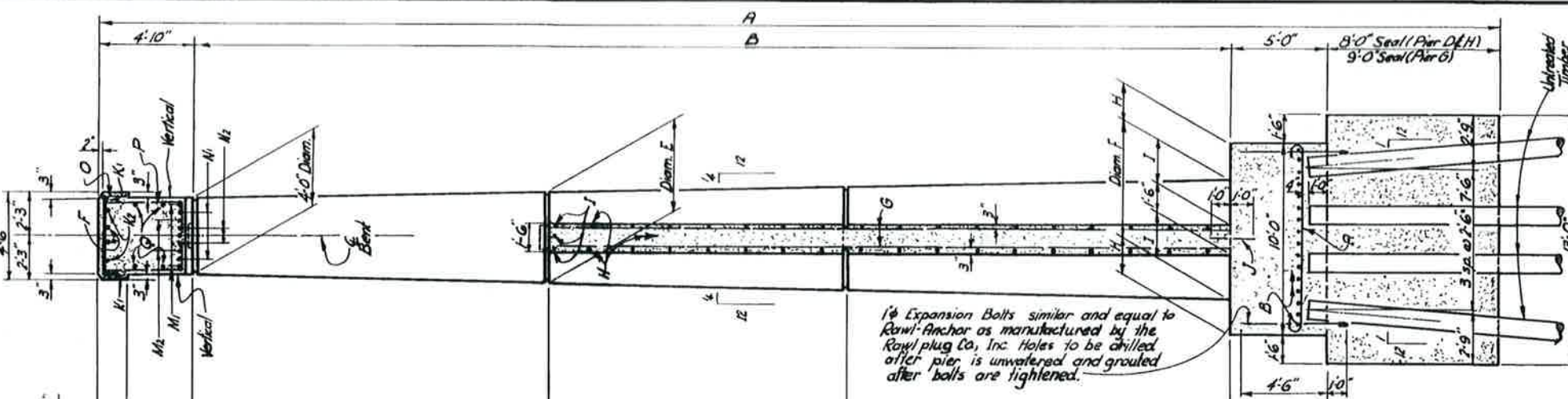
## SCHEDULE OF ELEVATIONS

ELEVATION	PIER "A"	PIER "B"	PIER "C"	PIER "I"	PIER "J"
A	+35.293	+37.429	+39.760	+37.429	+35.293
B	-6.207	-10.001	-16.240	-10.001	-6.207
C	-10.207	-15.001	-22.240	-15.001	-10.207

## BREAK-DOWN OF CONCRETE QUANTITIES

	PIER "A"	PIER "B"	PIER "C"	PIER "I"	PIER "J"
CAP	21.97 CY	20.08 CY	20.08 CY	20.08 CY	21.97 CY
COLS. & WALL	57.93 CY	68.27 CY	84.63 CY	68.27 CY	57.93 CY
DRY FOOTING	56.54 CY	56.54 CY	56.54 CY	56.54 CY	56.54 CY
TOTAL (CL. "A" CONC.)	136.44 CY	144.89 CY	161.25 CY	144.89 CY	136.44 CY
SEAL (CL. "A" CONC.)	65.57 CY	81.71 CY	98.05 CY	81.71 CY	65.57 CY



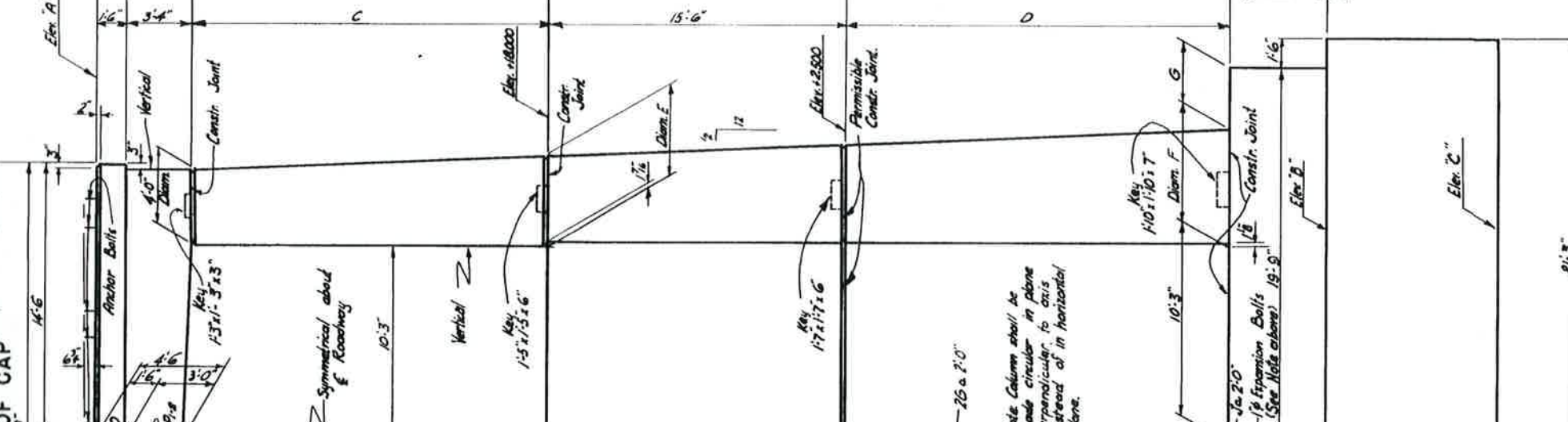


**QUANTITIES**

	PIER D	PIER G	PIER H
Dry Excavation	318 CY	200 CY	590 CY
Wet Excavation	203.11 CY	216.56 CY	201.62 CY
Class A Concrete	149.72 CY	168.43 CY	149.72 CY
Reinforcing Steel	1710 LB	1710 LB	1710 LB
Untreated Timber Piling	1710 LF	1710 LF	1710 LF

**REINFORCING STEEL SCHEDULE**

MARK	SIZE	NO.	LENGTH	MARK	SIZE	NO.	LENGTH
A	5	63	10'-7"	G	5	20	35'-2"
B	5	32	20'-8"	H	5	30	23'-4"
C	10	24	9'-2"	I	9	3	25'-4"
D	10	24	34'-2"	J	8	11	2'-0"
E	10	24	22'-10"	K	8	2	20'-0"
F	10	24	22'-10"	L	8	2	16'-8"
G	10	24	22'-10"	M	11	2	15'-6"
H	10	24	22'-10"	N	11	2	11'-6"
I	9	3	25'-4"	O	3	17	6'-0"
J	8	11	2'-0"	P	4	1	15'-10"
K	8	2	20'-0"	Q	3	17	6'-0"
L	8	2	16'-8"	R	4	1	15'-10"
M	11	2	15'-6"	S	4	1	15'-10"
N	11	2	11'-6"	T	4	1	15'-10"
O	3	17	6'-0"	U	4	1	15'-10"
P	4	1	15'-10"	V	4	1	15'-10"
Q	3	17	6'-0"	W	4	1	15'-10"
R	4	1	15'-10"	X	4	1	15'-10"
S	4	1	15'-10"	Y	4	1	15'-10"
T	4	1	15'-10"	Z	4	1	15'-10"
U	4	1	15'-10"				
V	4	1	15'-10"				
W	4	1	15'-10"				
X	4	1	15'-10"				
Y	4	1	15'-10"				
Z	4	1	15'-10"				

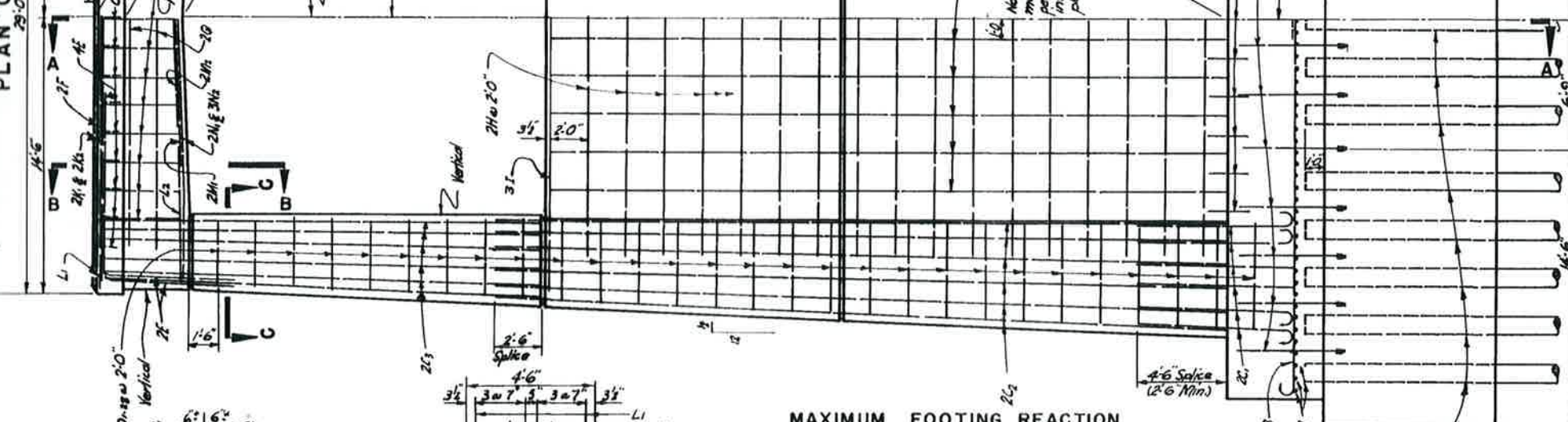


**SCHEDULE OF ELEVATIONS**

ELEV.	PIER D	PIER G	PIER H
A	141.332	141.332	139.760
B	18.668	22.668	19.240
C	26.668	31.668	27.240

**SCHEDULE OF DIMENSIONS**

DIMENSION	PIER D	PIER G	PIER H
A	68'-0"	73'-0"	67'-0"
B	30'-2"	34'-2"	29'-2"
C	18'-6"	18'-6"	16'-11"
D	16'-2"	20'-2"	16'-8"
E	4'-9"	4'-9"	4'-8"
F	6'-11"	6'-3"	6'-0"
G	3'-4"	3'-2"	3'-3"
H	1'-11"	1'-10"	1'-11"
I	2'-3"	2'-4"	2'-3"



**MAXIMUM FOOTING REACTION**

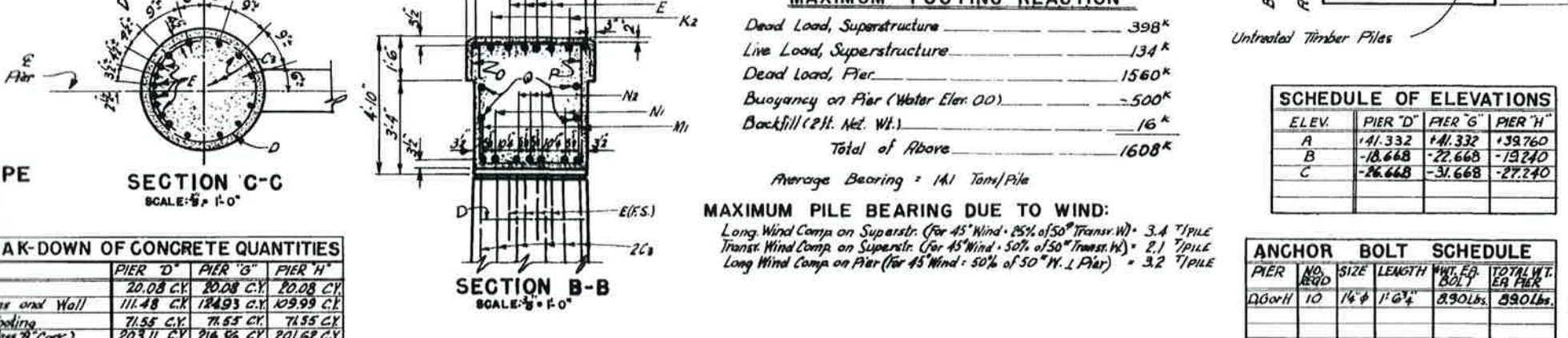
	Value
Dead Load, Superstructure	398*
Live Load, Superstructure	134*
Dead Load, Pier	1560*
Buoyancy on Pier (Water Elev. 00)	-500*
Backfill (2 ft. Net. Wt.)	16*
Total of Above	1608*

**MAXIMUM PILE BEARING DUE TO WIND:**

Long Wind Comp. on Superstr. (for 45° Wind - 25% of 50° Trans. W.) = 3.4 <sup>1</sup>/<sub>PILE</sub>

Trans. Wind Comp. on Superstr. (for 45° Wind - 50% of 50° Trans. W.) = 2.1 <sup>1</sup>/<sub>PILE</sub>

Long Wind Comp. on Pier (for 45° Wind - 50% of 50° W. 1 Pile) = 3.2 <sup>1</sup>/<sub>PILE</sub>



**BREAK-DOWN OF CONCRETE QUANTITIES**

	PIER D	PIER G	PIER H
Cap	20.08 CY	20.08 CY	20.08 CY
Columns and Wall	111.48 CY	124.93 CY	109.99 CY
Dry Footing	71.55 CY	71.55 CY	71.55 CY
Wet (Class A Conc.)	203.11 CY	216.56 CY	201.62 CY
Small Class "A" Conc.	149.72 CY	168.43 CY	149.72 CY

**ANCHOR BOLT SCHEDULE**

PIER	NO. REQD	SIZE	LENGTH	WGT. EA. BOLT	TOTAL WGT. EA. PIER
DOOR#1	10	1/2"	1'-0"	8.90 lbs.	89.0 lbs.

**BENDING DETAILS**

**PLAN OF FOOTING**

**DETAIL OF STRIPE**

**SECTION C-C**

**SECTION B-B**

**NOTES:**

For General Notes see Sheet No. 15.

Anchor Bolts to be paid for as reinforcing steel and are included in Pier Quantities. See Sheet No. 14 for details.

No cable clips required.

Footings may be lowered a maximum of 2'-0" without providing additional vertical column steel by decreasing length of splice.

Drive piles to a minimum bearing value of 15 Tons per Pile.

S. C. STATE HIGHWAY DEPARTMENT  
COLUMBIA

**PIERS D, G & H**  
**FOR MAIN BRIDGE NO. 2 OVER**  
**GREAT PEE DEE RIVER**  
**AT YAUHANNAH**

DOCKET NO. 222&226 U.S. ROUTE NO. 701  
GEORGETOWN-HORRY COUNTIES NOV. 1952  
SCALE: 1/4" = 1'-0", OR AS NOTED



### REINFORCING STEEL SCHEDULE

MARK	SIZE	NO. REQ'D	LENGTH	BENDING DETAILS
A	7	79	15'-10"	
B	6	38	25'-10"	
C	10	32	9'-2"	
D	10	24	38'-3"	
E	10	24	21'-8"	
F	10	8	35'-6"	
G	5	30	35'-6"	
H	5	34	33'-3"	
I	9	3	39'-3"	
J	8	16	2'-0"	
K	8	2	27'-6"	
L	8	2	38'-0"	
M	8	2	20'-6"	
N	8	2	19'-11"	
O	8	2	23'-0"	
P	8	2	15'-0"	
Q	8	2	37'-6"	
R	8	2	38'-8"	
S	8	2	38'-8"	
T	8	2	38'-8"	
U	8	2	38'-8"	
V	8	2	38'-8"	
W	8	2	38'-8"	
X	8	2	38'-8"	
Y	8	2	38'-8"	
Z	8	2	38'-8"	
AA	8	2	38'-8"	
AB	8	2	38'-8"	

### HOOK DETAILS

FOR NO. 4 & SMALLER BARS:	FOR NO. 5 & NO. 6 BARS:	FOR NO. 7 & LARGER BARS:
Add 6" per hook.	Add 8" per hook.	Add 12" per hook.

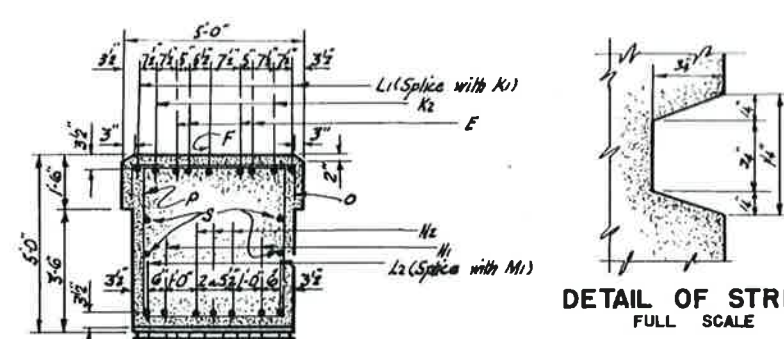
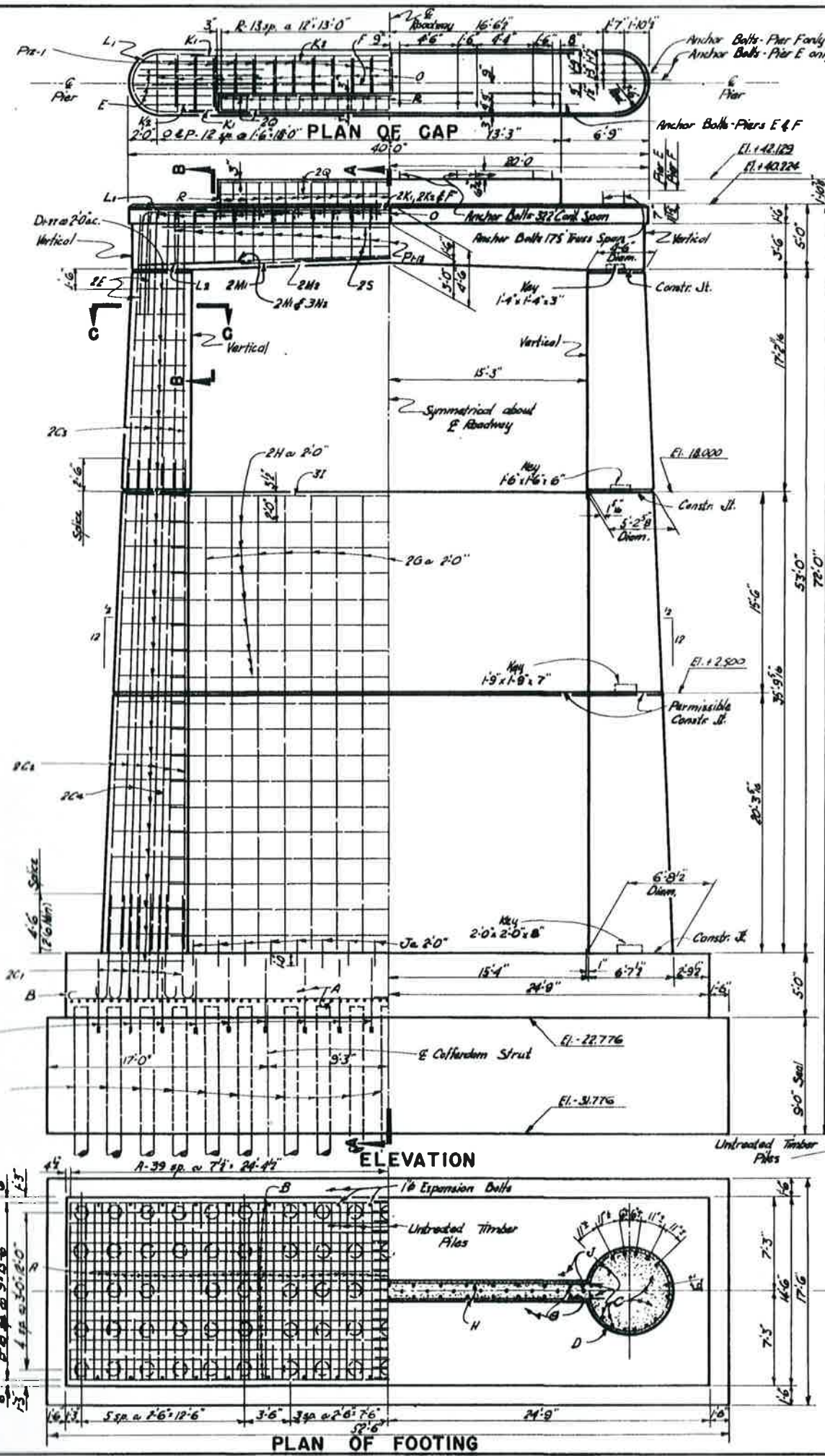
### QUANTITIES

	PIER E	PIER F
Dry Excavation	546 C.Y.	340 C.Y.
Class "A" Concrete	323.76 C.Y.	323.76 C.Y.
Class "AA" Concrete	281.34 C.Y.	281.34 C.Y.
Reinforcing Steel	22051 lbs.	19962 lbs.
Untreated Timber Piling	2760 LF	2760 LF

① Wt. of one 1" Exp. Bolt = 15.94 lbs. (Incl. wt. of bolt)  
 ② Includes 36 Expansion Bolt Assemblies (Wt. 574 lbs.) and 18 Anchor Bolt Assemblies (Wt. 285 lbs.)  
 ③ Includes 36 Expansion Bolt Assemblies (Wt. 574 lbs.) and 14 Anchor Bolt Assemblies (Wt. 196 lbs.)

S. C. STATE HIGHWAY DEPARTMENT  
 COLUMBIA  
**PIERS E & F**  
**FOR MAIN BRIDGE NO. 2 OVER GREAT PEE DEE RIVER AT YAUHANNAH**  
 DOCKET NO. 2226226 U.S. ROUTE NO. 701  
 GEORGETOWN-HORRY COUNTIES NOV. 1962  
 SCALE: 3/8" = 1'-0" OR AS NOTED

NOTE:  
 For details of Anchor Bolts see Sheet No. 14 & 15  
 For Anchor Bolt Layout see Sheet No. 15



### ANCHOR BOLT SCHEDULE

PIER	NO. REQ'D	SIZE	LENGTH	WT. EA. BOLT	TOTAL WT. EA. PIER
E	10	1 1/2"	1'-6 1/2"	8.90 lbs.	89.0 lbs.
F	8	1 1/2"	2'-10"	24.54 lbs.	196.3 lbs.
F	10	1 1/2"	1'-6 1/2"	8.90 lbs.	89.0 lbs.
F	4	1 1/2"	3'-2 1/2"	24.80 lbs.	107.2 lbs.

\* Complete Assembly

### BREAK-DOWN OF CONCRETE QUANTITIES

	PIER E	PIER F
Cap and Build-up	34.73 C.Y.	34.73 C.Y.
Columns and Wall	158.68 C.Y.	158.68 C.Y.
Dry Footing	130.35 C.Y.	130.35 C.Y.
Total (Class "A" Conc.)	323.76 C.Y.	323.76 C.Y.
Seal (Class "AA" Conc.)	281.34 C.Y.	281.34 C.Y.

### MAXIMUM FOOTING REACTION

Dead Load Superstructure	36.9*
Live Load Superstructure	157*
Dead Load Pier	2383*
Buoyancy on Pier (Water Elev. 0.0)	787*
Backfill (7 ft. Net Wt.)	147*
Total of Above	2269*
Average Bearing	= 12.3 TONS/PIER

### MAXIMUM PILE BEARING DUE TO WIND

Long. Wind Comp. on Superstr. (For 45° Wind + 25% of 50° Trans. Wind Comp. Sp. & 50% on Truss Sp.) = 4.9 T/PIER  
 Trans. Wind Comp. on Superstr. (For 45° Wind + 50% of 50° Trans. Wind) = 2.0 T/PIER  
 Long. Wind Comp. on Pier (For 45° Wind + 50% of 50° W.L. Pier) = 1.9 T/PIER

Notes:  
 For General Notes see Sheet No. 15  
 All seal concrete to be class "AA". All other concrete to be class "A".  
 Buildup to be cast monolithic with cap.  
 Anchor Bolts to be paid for as reinforcing steel and are included in Pier Quantities. See Sheets No. 14 & 15 for details.  
 No cable clips required.  
 Footings may be lowered a maximum of 2'-0" without providing additional vertical column steel by decreasing length of splice.  
 Drive Piles to a minimum bearing value of 15 tons per pile.  
 For special requirements and payment of pier quantities see special provisions.

NOTE: Column shall be made circular in plane perpendicular to axis instead of in horizontal plane



**STEEL SCHEDULE**

MARK	SIZE	NO.	LENGTH	D	MARK	SIZE	NO.	LENGTH	D
A	5	27	29'-0"	B	L	4	6	3'-8"	B
B	4	32	27'-11"	B	L	4	8	3'-3"	B
C	4	28	28'-11"	B	L	4	8	3'-5"	B
D	4	51	29'-8"	B	L	4	8	3'-8"	B
E	11	12	31'-4"	B	L	4	8	3'-11"	B
E	9	4	31'-10"	B	M	9	6	23'-3"	S
E	8	4	31'-11"	B	N	4	42	3'-9"	B
E	9	2	27'-4"	B	O	4	16	6'-6"	B
E	8	2	27'-7"	B	S	6	16	8'-8"	B
F	4	54	5'-10"	B	S	6	16	5'-0"	S
F	4	54	6'-1"	B	U	4	21	4'-0"	B
G	2	30	2'-4"	B	V	6	8	0'-10"	S
H	4	8	29'-8"	S	Y	3	90	3'-0"	B
I	4	8	29'-8"	S	Z	4	2	20'-10"	S
J	2	36	2'-6"	B	Q	6	16	6'-4"	B
K	2	16	2'-10"	B					
L	4	8	2'-9"	B					
L	4	8	2'-11"	B					

**SUMMARY OF QUANTITIES**

	ONE INTERIOR SPAN	ONE END SPAN
CLASS "A" CONCRETE	35.29 CY.	37.12 CY.
REINFORCING STEEL	8,299 Lbs.	8,256 Lbs.
PIPE HANDRAIL	60 L.F.	60 L.F.

**BREAKDOWN OF CONCRETE QUANTITIES**

	1 INT. SPAN	1 END SPAN
Beams, Slabs, Brackets	29.81 CY.	31.49 CY.
Sidewalks	3.96 CY.	4.05 CY.
Posts & Railing	1.52 CY.	1.58 CY.
Totals	35.29 CY.	37.12 CY.

**NOTES:**

- Design Data: A. A. S. H. O. Specifications (1941)
- H15-S12 Live load:  $f_s = 20,000$  P.S.I.,  $f_c = 1,000$  P.S.I.,  $n = 10$ ,  $v = 90$  P.S.I.,  $u = 150$  P.S.I.
- All concrete shall be Class "A".
- Diaphragms, brackets and wings shall be cast monolithic with slabs and beams.
- Exposed edges shall be chamfered  $\frac{3}{8}$ " unless otherwise specified.
- No sidewalk concrete shall be cast until beam falsework has been struck.
- Structural grade reinf steel not permitted.
- All dimensions relative to reinf steel are to centers of bars.

**HOOK DETAILS**

Hook for F Bars Add 6" per hook.

Hook for No. 4 and smaller bars Add 6" per hook.

Hook for No. 5 & No. 6 bars Add 8" per hook.

Hook for No. 7 and larger bars Add 12" per hook.

**STEEL SCHEDULE**

ONE INTERIOR SPAN				ONE END SPAN			
MARK	SIZE	LENGTH	D	MARK	SIZE	LENGTH	D
A	5	27	29'-0"	B	L4	4	6
B	4	32	27'-11"	C	L4	4	8
C	4	28	28'-11"	D	L4	4	8
D	4	51	29'-8"	E	L4	4	8
E	11	12	31'-4"	F	L4	4	8
F	9	4	31'-10"	G	L4	4	8
G	8	4	31'-11"	H	L4	4	8
H	9	2	27'-4"	I	L4	4	8
I	8	2	27'-7"	J	L4	4	8
J	4	54	5'-10"	K	L4	4	8
K	4	54	6'-1"	L	L4	4	8
L	2	30	2'-4"	M	L4	4	8
M	4	8	29'-8"	N	L4	4	8
N	4	8	29'-8"	O	L4	4	8
O	2	36	2'-6"	P	L4	4	8
P	2	16	2'-10"	Q	L4	4	8
Q	4	8	2'-9"	R	L4	4	8
R	4	8	2'-11"	S	L4	4	8

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Sidewalks	3.96 CY.	4.05 CY.
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Hook for No. 6 and smaller bars Add 6" per hook.

Hook for No. 5 & No. 6 bars Add 8" per hook.

Hook for No. 7 and larger bars Add 12" per hook.

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MARK	SIZE	LENGTH	D	MARK	SIZE	LENGTH	D
A	5	27	29'-0"	B	L4	4	6
B	4	32	27'-11"	C	L4	4	8
C	4	28	28'-11"	D	L4	4	8
D	4	51	29'-8"	E	L4	4	8
E	11	12	31'-4"	F	L4	4	8
F	9	4	31'-10"	G	L4	4	8
G	8	4	31'-11"	H	L4	4	8
H	9	2	27'-4"	I	L4	4	8
I	8	2	27'-7"	J	L4	4	8
J	4	54	5'-10"	K	L4	4	8
K	4	54	6'-1"	L	L4	4	8
L	2	30	2'-4"	M	L4	4	8
M	4	8	29'-8"	N	L4	4	8
N	4	8	29'-8"	O	L4	4	8
O	2	36	2'-6"	P	L4	4	8
P	2	16	2'-10"	Q	L4	4	8
Q	4	8	2'-9"	R	L4	4	8
R	4	8	2'-11"	S	L4	4	8

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ONE INTERIOR SPAN				ONE END SPAN			
MARK	SIZE	LENGTH	D	MARK	SIZE	LENGTH	D
A	5	27	29'-0"	B	L4	4	6
B	4	32	27'-11"	C	M	9	4
C	4	28	28'-11"	D	N1	4	21
D	4	51	29'-8"	E	N2	4	22
E	11	12	31'-4"	F	P1	4	2
F	9	4	31'-10"	G	P2	4	3
G	8	4	31'-11"	H	Q	6	16
H	9	2	27'-4"	I	R	4	4
I	8	2	27'-7"	J	R2	4	4
J	4	54	5'-10"	K	R3	4	8
K	4	54	6'-1"	L	S	6	12
L	2	30	2'-4"	M	S2	6	12
M	4	8	29'-8"	N	T	6	2
N	4	8	29'-8"	O	U	4	21
O	2	36	2'-6"	P	U	4	21
P	2	16	2'-10"	Q	V	6	6
Q	4	8	2'-9"	R	Y	3	90
R	4	8	2'-11"	S	Z	4	2
S	4	8	2'-11"	T	Z	4	2
T	4	8	2'-11"	U	Z	4	2
U	4	8	2'-11"	V	Z	4	2
V	4	8	2'-11"	W	Z	4	2
W	4	8	2'-11"	X	Z	4	2
X	4	8	2'-11"	Y	Z	4	2
Y	4	8	2'-11"	Z	Z	4	2

**SUMMARY OF QUANTITIES**

	ONE INTERIOR SPAN	ONE END SPAN
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MARK	SIZE	NO.	LENGTH	D	MARK	SIZE	NO.	LENGTH	D
A	5	27	29'-0"	B	L	4	6	3'-8"	B
B	4	32	27'-11"	B	L	4	8	3'-3"	B
C	4	28	28'-11"	B	L	4	8	3'-5"	B
D	4	51	29'-8"	B	L	4	8	3'-8"	B
E	11	12	31'-4"	B	L	4	8	3'-11"	B
E	9	4	31'-10"	B	M	9	6	23'-3"	S
E	8	4	31'-11"	B	N	4	42	3'-9"	B
E	9	2	27'-4"	B	O	4	16	6'-6"	B
E	8	2	27'-7"	B	S	6	16	8'-8"	B
F	4	54	5'-10"	B	S	6	16	5'-0"	S
F	4	54	6'-1"	B	U	4	21	4'-0"	B
G	2	30	2'-4"	B	V	6	8	0'-10"	S
H	4	8	29'-8"	S	Y	3	90	3'-0"	B
I	4	8	29'-8"	S	Z	4	2	20'-10"	S
J	2	36	2'-6"	B	Q	6	16	6'-4"	B
K	2	16	2'-10"	B					
L	4	8	2'-9"	B					
L	4	8	2'-11"	B					

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**STEEL SCHEDULE**

ONE INTERIOR SPAN				ONE END SPAN			
MARK	SIZE	LENGTH	D	MARK	SIZE	LENGTH	D
A	5	27	29'-0"	B	L4	4	6
B	4	32	27'-11"	C	L4	4	6
C	4	28	28'-11"	D	L4	4	6
D	4	51	29'-8"	E	L4	4	6
E	11	12	31'-4"	F	L4	4	6
F	9	4	31'-10"	G	L4	4	6
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H	9	2	27'-4"	I	L4	4	6
I	8	2	27'-7"	J	L4	4	6
J	4	54	5'-10"	K	L4	4	6
K	4	54	6'-1"	L	L4	4	6
L	2	30	2'-4"	M	L4	4	6
M	4	8	29'-8"	N	L4	4	6
N	4	8	29'-8"	O	L4	4	6
O	2	36	2'-6"	P	L4	4	6
P	2	16	2'-10"	Q	L4	4	6
Q	4	8	2'-9"	R	L4	4	6
R	4	8	2'-11"	S	L4	4	6

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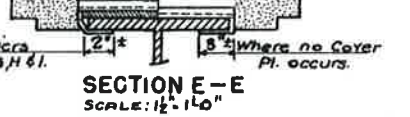
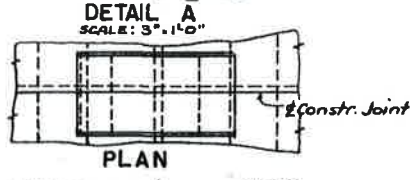
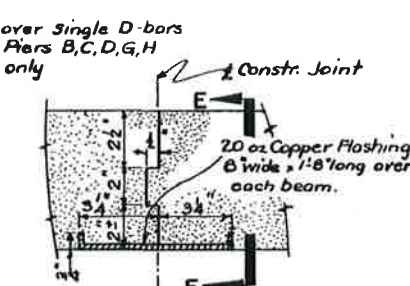
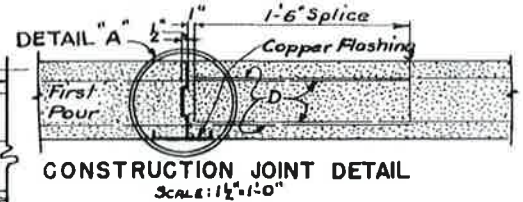
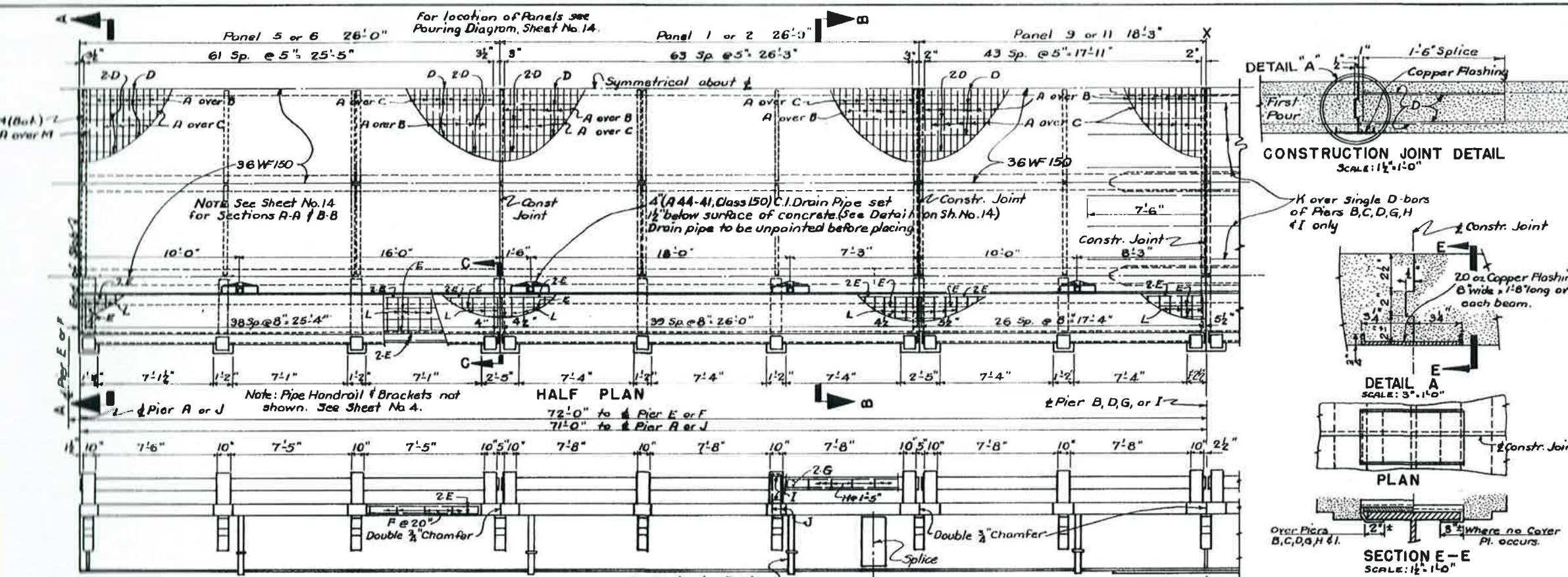
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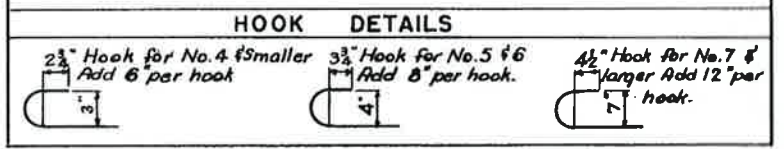
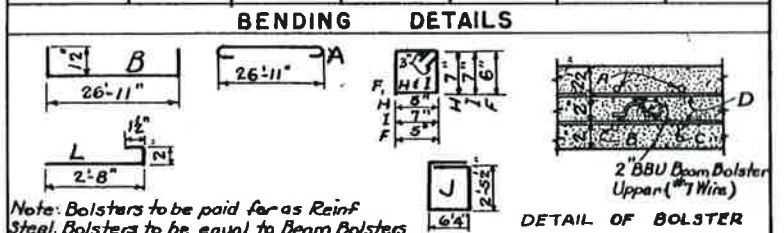
FED. ROAD DIST. NO.	STATE	COUNTY	DOCKET NO.	ROUTE NO.	SHEET NO.	TOTAL SHEETS
3	S.C.	GEORGETOWN	2226.226	701	13	31



BREAK-DOWN OF CONC. QUANTITIES			
		18'-3" PANEL	26'-0" PANEL
SLAB	C.Y.	10.27	14.77
SIDEWALK	C.Y.	2.43	3.55
POSTS & HANDRAIL	C.Y.	.96	1.39
TOTAL	C.Y.	13.66	19.71

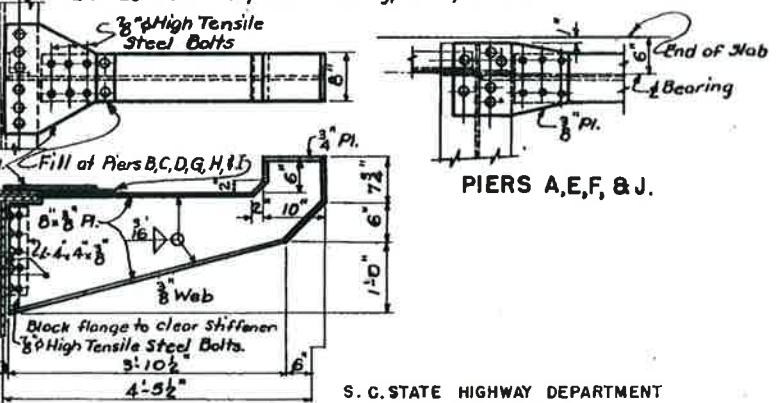
REINFORCING STEEL SCHEDULE									
Mark	No.	Length	D	No.	Length	D	No.	Length	D
A	4	27'-11"	1/2"	6	27'-11"	1/2"	4	27'-11"	1/2"
B	4	28'-11"	1/2"	30	28'-11"	1/2"	22	28'-11"	1/2"
C	4	26'-11"	1/2"	30	26'-11"	1/2"	22	26'-11"	1/2"
D	4	29'-11"	1/2"	39	29'-11"	1/2"	39	29'-11"	1/2"
E	4	26'-5"	1/2"	18	26'-5"	1/2"	18	26'-5"	1/2"
F	2	30'-2"	1/2"	30	30'-2"	1/2"	20	30'-2"	1/2"
G	4	26'-4"	1/2"	8	26'-4"	1/2"	8	26'-4"	1/2"
H	2	36'-2"	1/2"	36	36'-2"	1/2"	24	36'-2"	1/2"
I	2	16'-2"	1/2"	16	16'-2"	1/2"	12	16'-2"	1/2"
J	4	16'-6"	1/2"	16	16'-6"	1/2"	12	16'-6"	1/2"
K	4	—	—	—	—	—	12	15'-0"	—
L	3	80'-3"	1/2"	78	80'-3"	1/2"	54	80'-3"	1/2"
M	4	—	—	—	—	—	—	—	—
Bolsters	77	2'-6"	—	77	2'-6"	—	50	2'-6"	—

QUANTITIES				
PANELS	1,2,3 or 4	5 or 6	7 or 8	9,11 or 13
CONC. CL. CY	20.04	19.58	20.04	13.66
REIN. STEEL LB	3910	3687	3867	2782

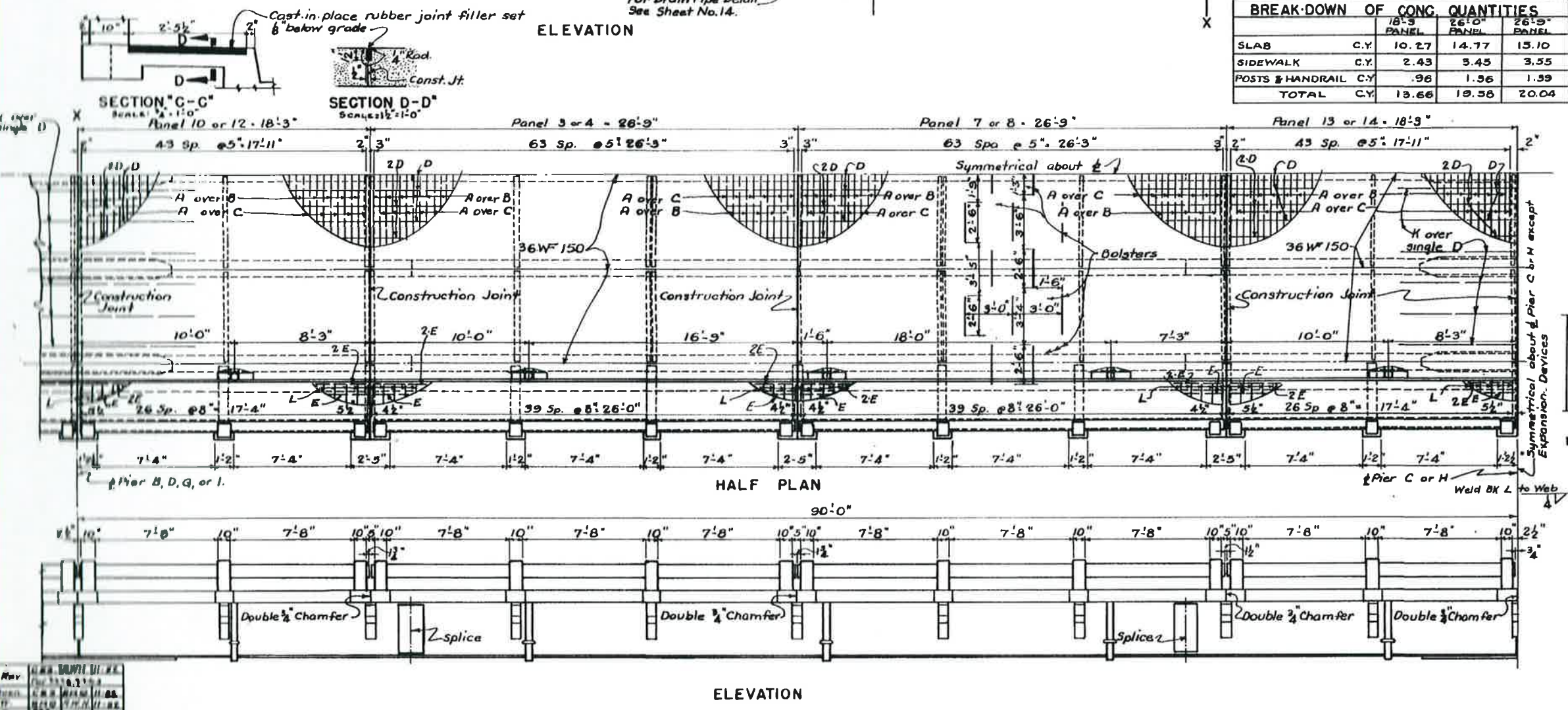


QUANTITIES (ONE 322 FT. SPAN)		
Class "A" Concrete	241.96	C.Y.
Reinforcing Steel	46,951	Lbs.
Structural Steel	307,300	Lbs.

NOTES:  
For General Notes, see Sheet No. 15.  
Coppers shall be 0.025" in thickness (approx. 20 oz) and shall comply with the latest A.S.T.M. Spec. B152 for Type ETP, hot rolled and annealed.



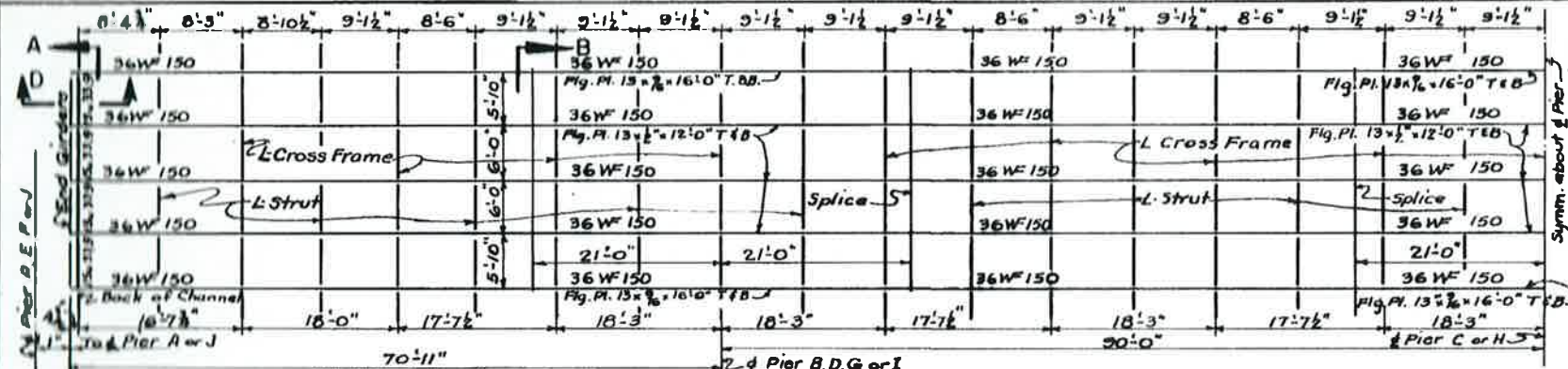
S. C. STATE HIGHWAY DEPARTMENT  
COLUMBIA  
322 FT. SUPERSTRUCTURE  
FOR MAIN BRIDGE NO. 2 OVER  
GREAT PEE DEE RIVER  
AT YAUHANNAH  
S. C. DOCKET NO. 2226.226.3 ROUTE NO. 701.  
GEORGETOWN-HORRY COS. DATE OCTOBER 1932.  
SCALE: 1/4" = 1'-0" UNLESS NOTED



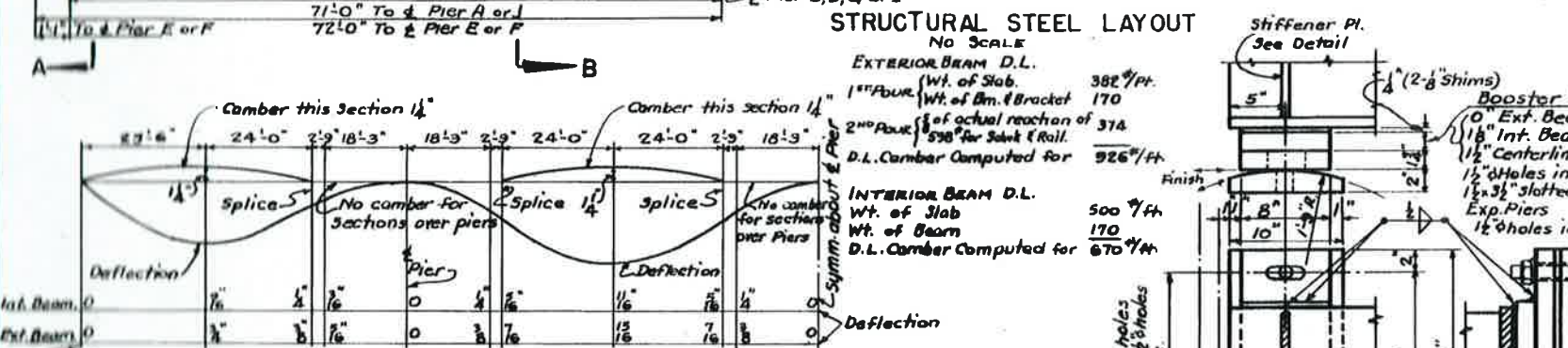
Rev.	By	Date	Description
1	J. H. H.	10-1-32	Initial Design
2	J. H. H.	10-1-32	Revised Design
3	J. H. H.	10-1-32	Final Design



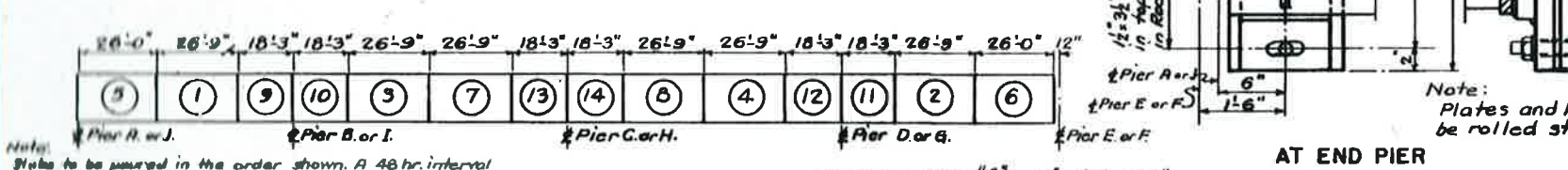
FED. ROAD DIV. NO.	STATE	COUNTY	DOCKET NO.	ROUTE NO.	SHEET NO.	TOTAL SHEETS
3	S.C.	GEORGETOWN	2226.226.3	701	14	31



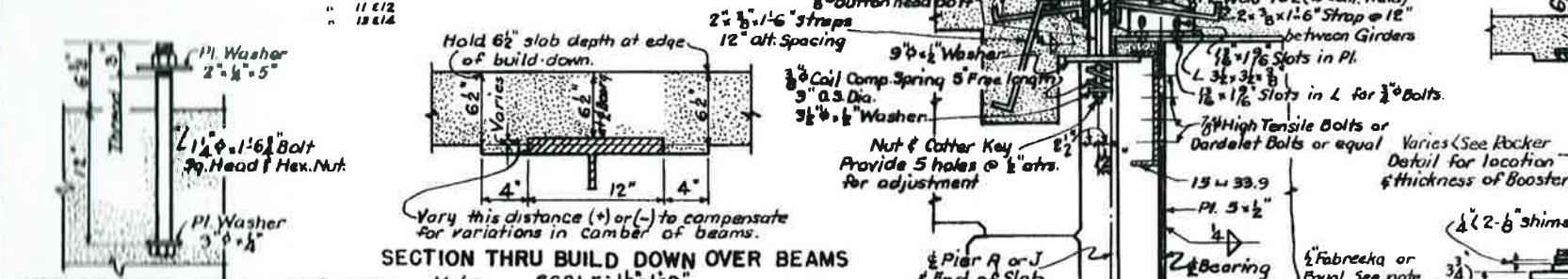
STRUCTURAL STEEL LAYOUT  
NO SCALE



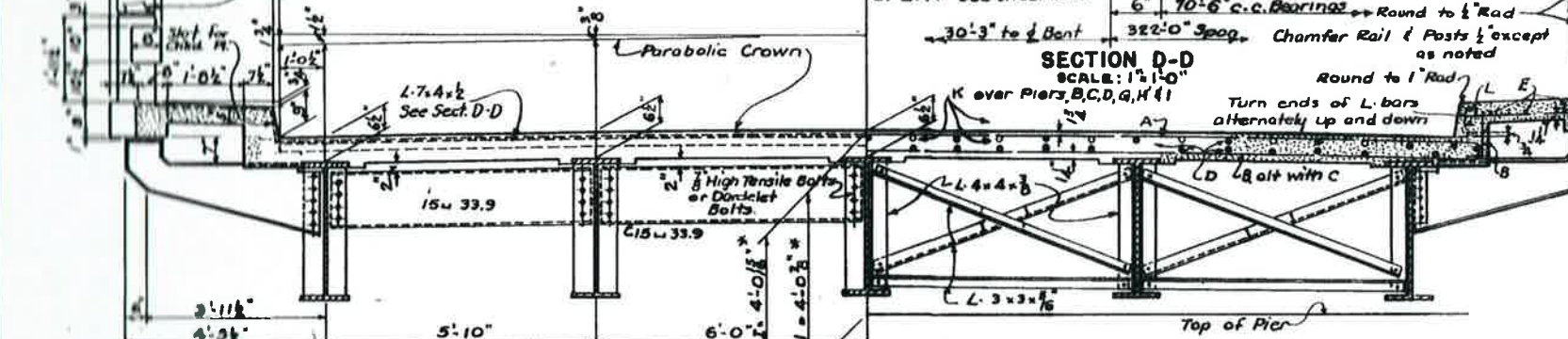
DEAD LOAD DEFLECTION & CAMBER DIAGRAM  
(NO CAMBER IN 42 FT. SECTION OVER PIERS. CAMBER OTHER SECTIONS 1/4")  
NO SCALE.



POURING DIAGRAM  
NO SCALE

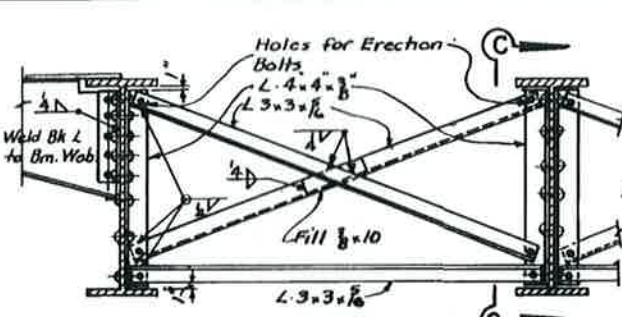


ANCHOR BOLT DETAIL 322' SPAN  
SCALE: 1/2" = 1'-0"

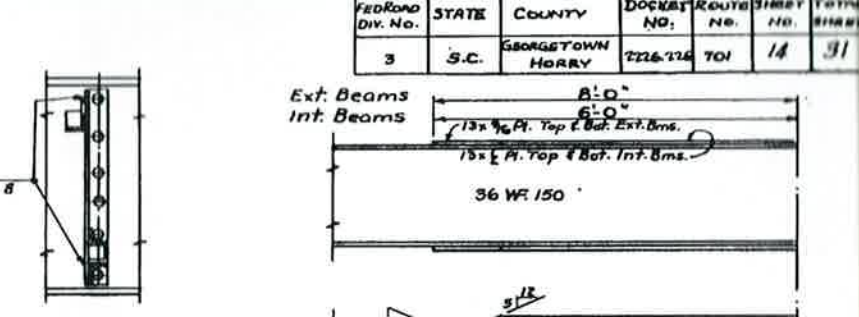


HALF SECTION "A-A"  
SCALE: 1/2" = 1'-0"

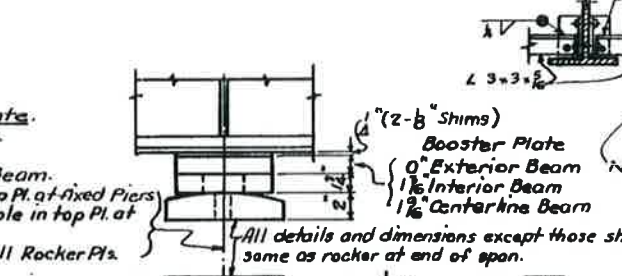
HALF SECTION "B-B"  
SCALE: 1/2" = 1'-0"



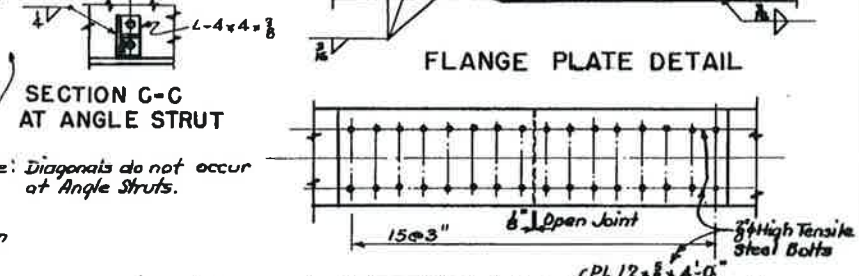
INT. CROSS FRAME DETAIL  
SCALE: 3/4" = 1'-0"



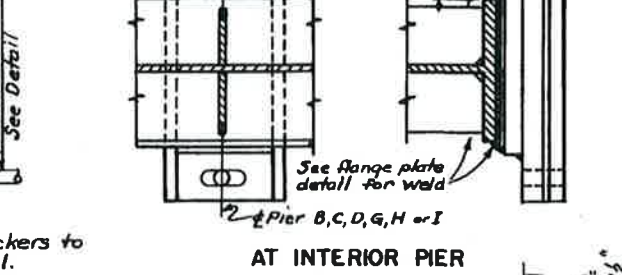
SECTION C-C  
AT ANGLE CROSS FRAME



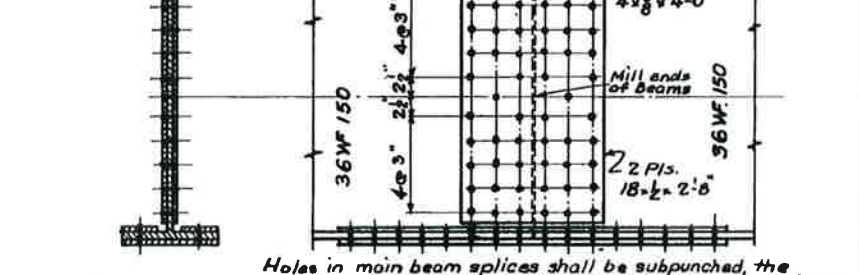
SECTION C-C  
AT ANGLE STRUT



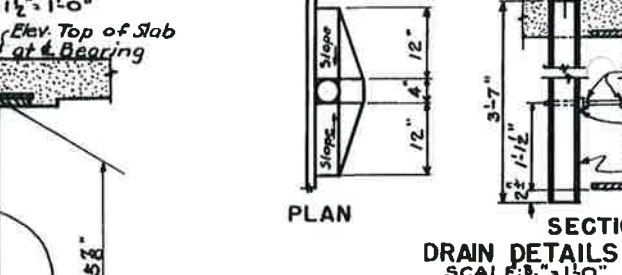
FLANGE PLATE DETAIL



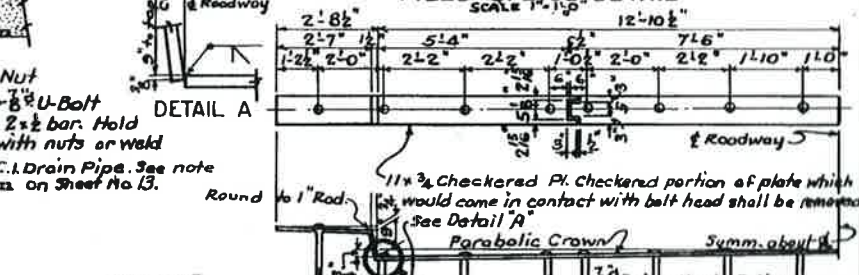
AT END PIER



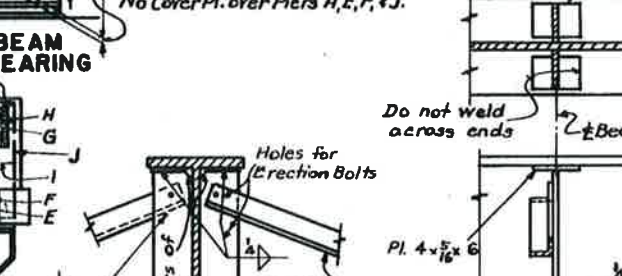
AT INTERIOR PIER



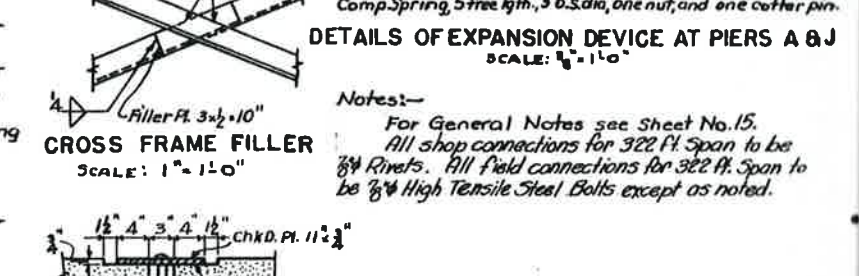
ROCKER DETAIL  
SCALE: 1/2" = 1'-0"



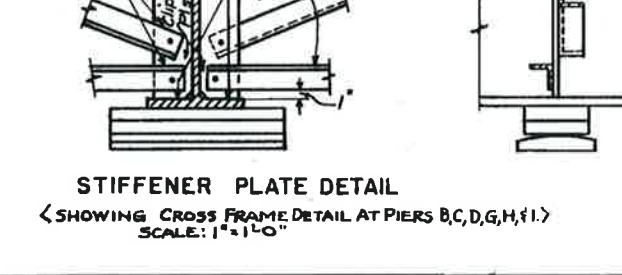
FIELD SPLICE DETAIL  
SCALE: 1/2" = 1'-0"



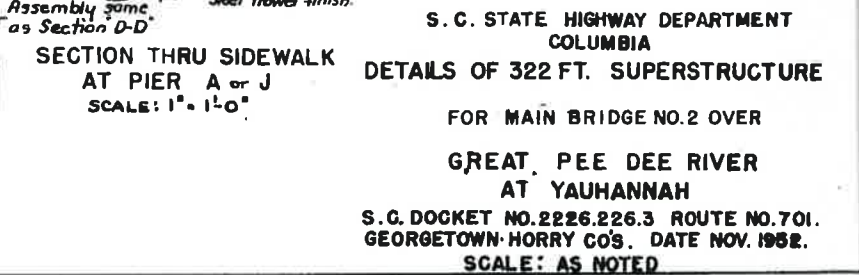
SKETCH FOR COMPUTING BEAM  
SEAT ELEVATION AT BEARING



DETAILS OF EXPANSION DEVICE AT PIERS A & J  
SCALE: 1/2" = 1'-0"



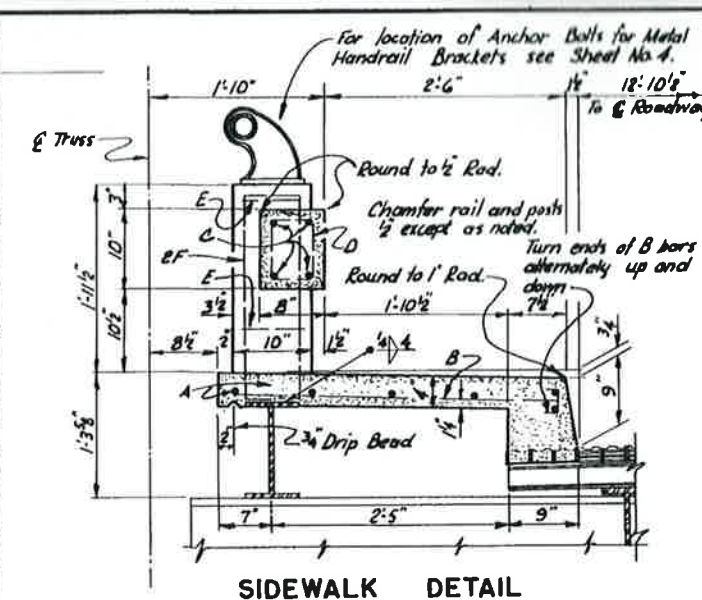
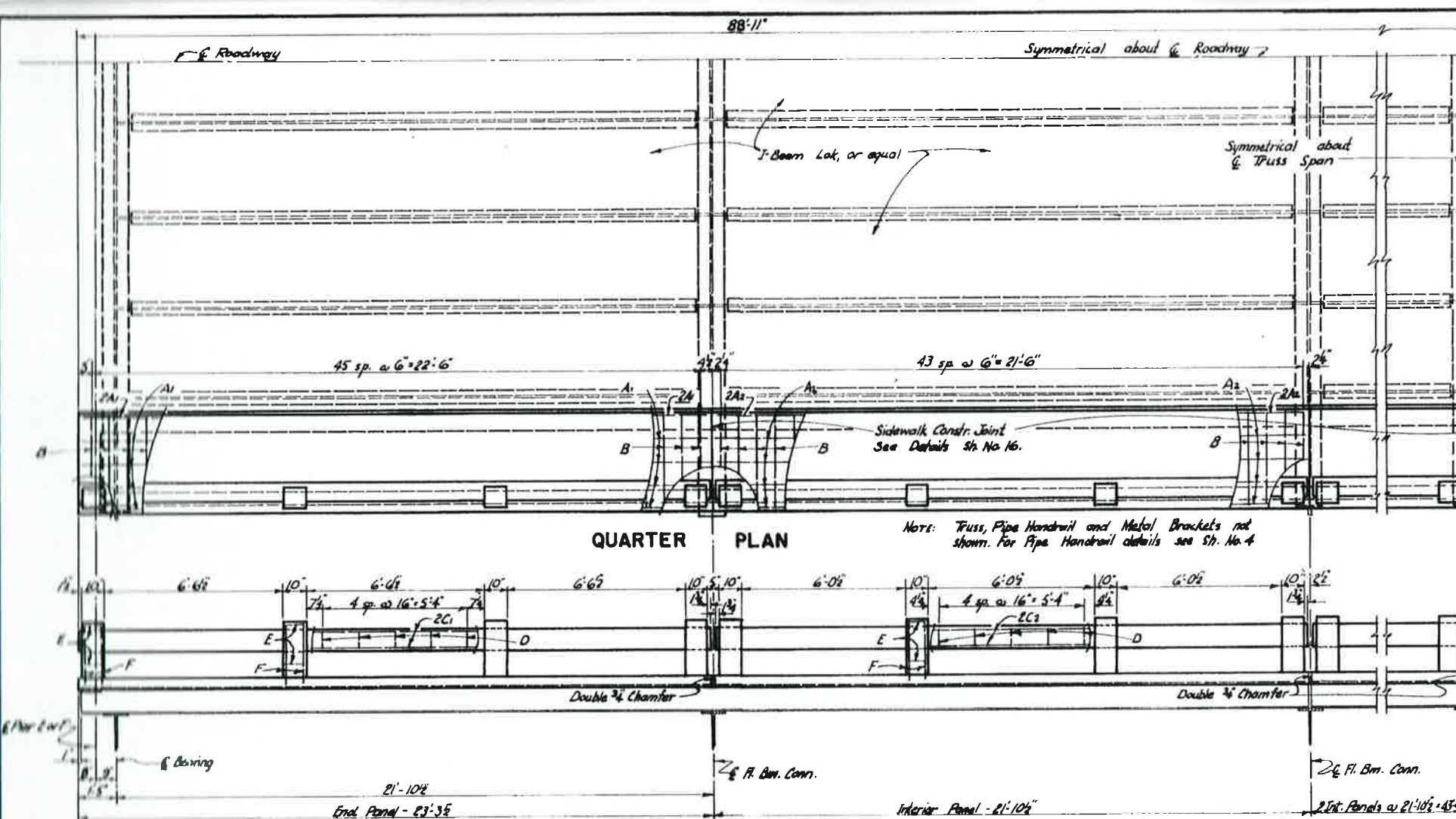
STIFFENER PLATE DETAIL  
(SHOWING CROSS FRAME DETAIL AT PIERS B, C, D, G, H, I, J)  
SCALE: 1/2" = 1'-0"



CROSS FRAME FILLER  
SCALE: 1/2" = 1'-0"

S. C. STATE HIGHWAY DEPARTMENT  
COLUMBIA  
DETAILS OF 322 FT. SUPERSTRUCTURE  
FOR MAIN BRIDGE NO. 2 OVER  
GREAT PEE DEE RIVER  
AT YAUHANNAH  
S. C. DOCKET NO. 2226.226.3 ROUTE NO. 701.  
GEORGETOWN-HORRY COS. DATE NOV. 1932.  
SCALE: AS NOTED





FED. RD. DIV. NO.	STATE	COUNTY	DOCKET NO.	ROUTE NO.	SHEET NO.	TOTAL SHEET
3	S. C.	GEORGETOWN HORRY	22252263	701	15	31

REINFORCING STEEL SCHEDULE					BENDING DETAILS	
MARK	SIZE NO.	NO. REQ'D	LENGTH	D		
A1	4	24	23'-0"	S		12" 3'-3" 90° D E F
A2	4	72	21'-6"	S		
B	3	712	3'-6"	B		
C1	4	16	22'-10"	S		
C2	4	48	21'-5"	S		
D	2	240	2'-6"	B		
E	2	128	2'-10"	B		
F	4	128	5'-9"	B		

QUANTITIES (ONE 175 FT TRUSS SPAN)	
Class "A" Concrete	30.77 C.Y.
Reinforcing Steel	3923 Lbs.
New Structural Steel	10 100, 700 Lbs.
Removing, Transporting, Widening and Re-erecting 175 Ft. Truss	1 Each
Roadway Floors	535 Sq. Yds.
Pipe Handrail	357 L.F.
Painting <del>Old</del> Steel	Necessary L.S.

① Includes all new structural steel except Pipe Handrail & Brackets.

**NOTES:**

The existing 175' truss shall be removed, widened, moved to the new location and re-erected. The existing floor, stringers, floor beams, handrail, portals and any frames shall be removed. The existing stringers and connections shall be salvaged and reused. The entire truss shall be widened and modified to comply with the existing and later Design Data: A.A.S.H.O. Specifications 1949, with revisions thru 1950.

Live Load = 15-212-44  
 $f_s (\text{Boat}) = 2,000 \text{ in}^2$ ;  $f_s (\text{Struct. - New Steel}) = 18,000 \text{ in}^2$ ;  $f_s (\text{Struct. - Old Steel and New Steel in combination with Old Steel}) = 16,360 \text{ in}^2$ ;  $f_c = 1200 \text{ in}^2$ ;  $n = 10$ ;  $u = 300 \text{ in}^2$ ;  $v = 225 \text{ in}^2$

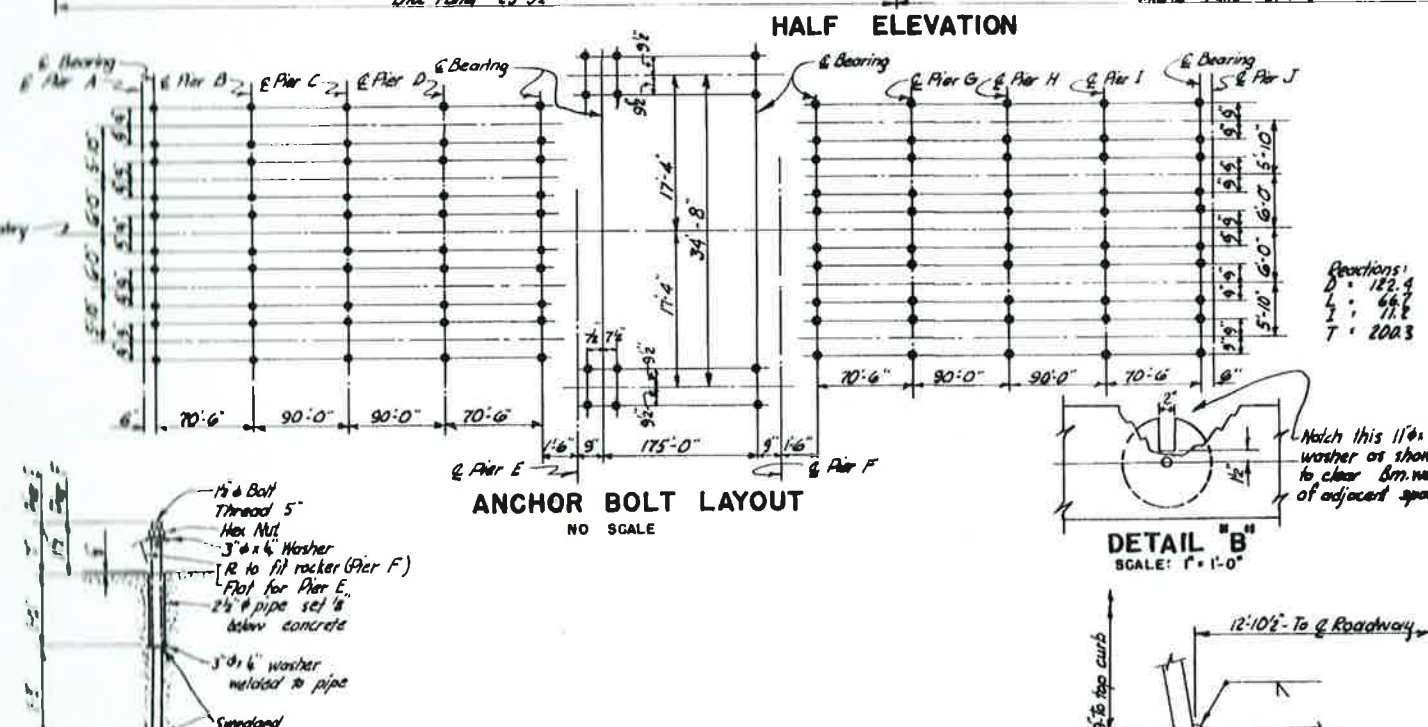
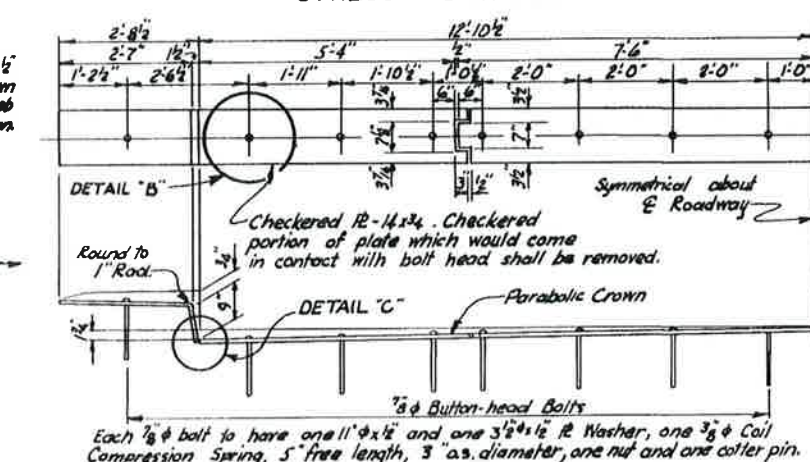
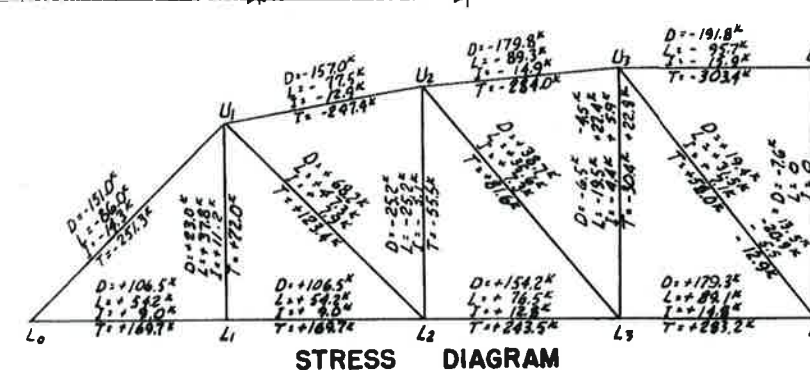
lateral, and in addition some new bottom lateral, shall be installed. New floor, sidewalk, posts, rail, floor beams, portals, top lateral, expansion devices and

No deck concrete to be poured until superstructure framework has been struck.  
No construction joints other than those shown will be permitted.  
Handrail posts shall be vertical.

High Tensile Steel Bolts, nuts, and washers shall meet all the requirements of Specifications for Assembly of Structural Joints Using High Tensile Steel Bolts as approved Jan. 31, 1961 by the Research Council on Riveted and Bolted Structural Joints of the Engineering Foundation.

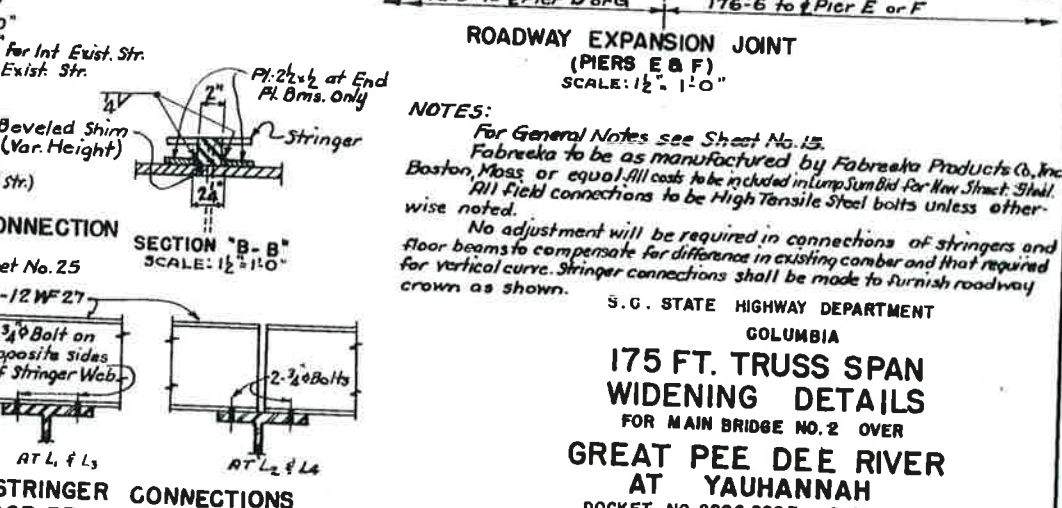
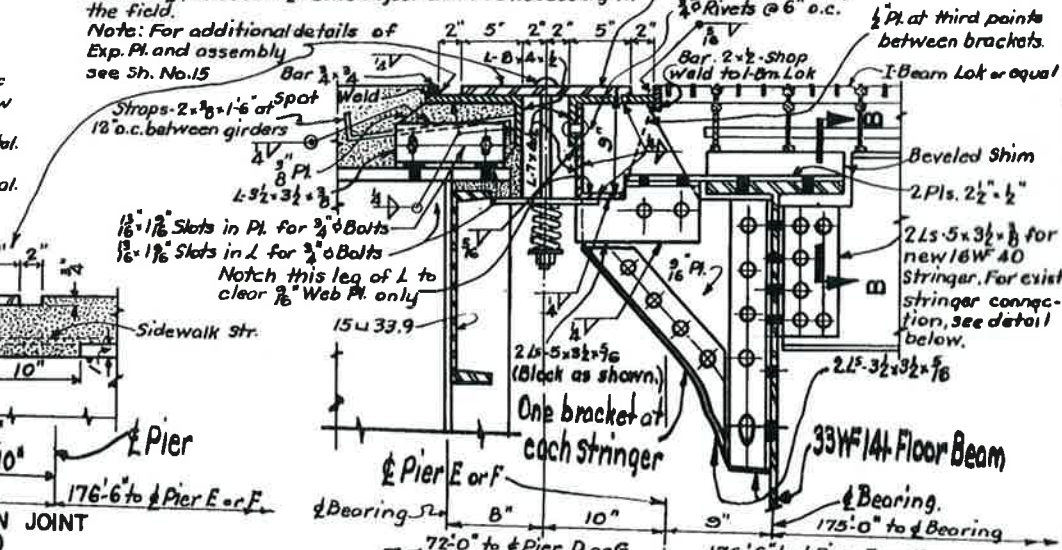
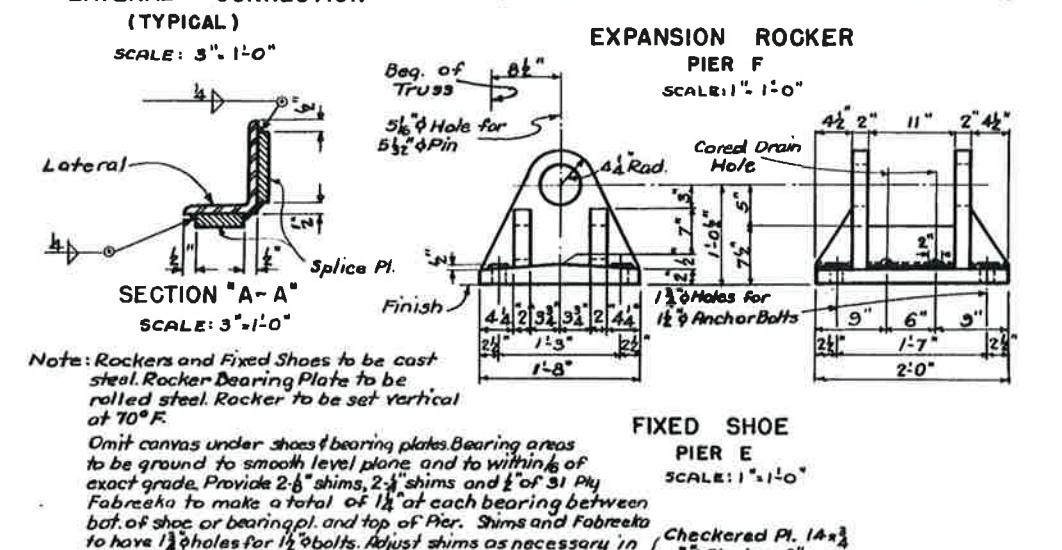
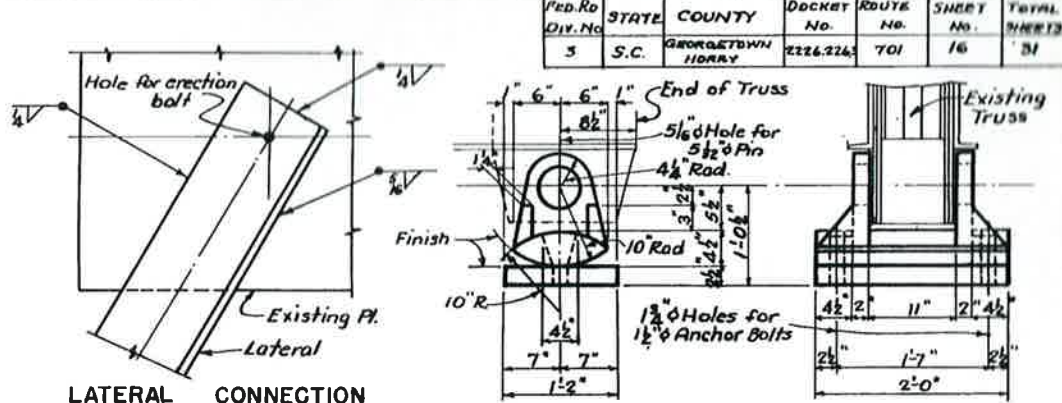
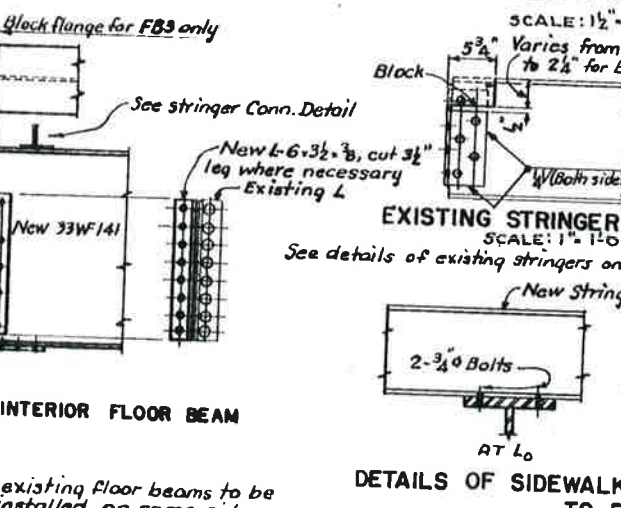
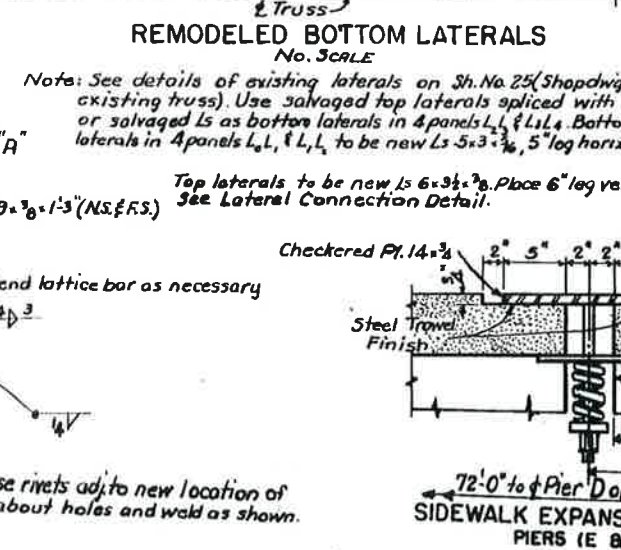
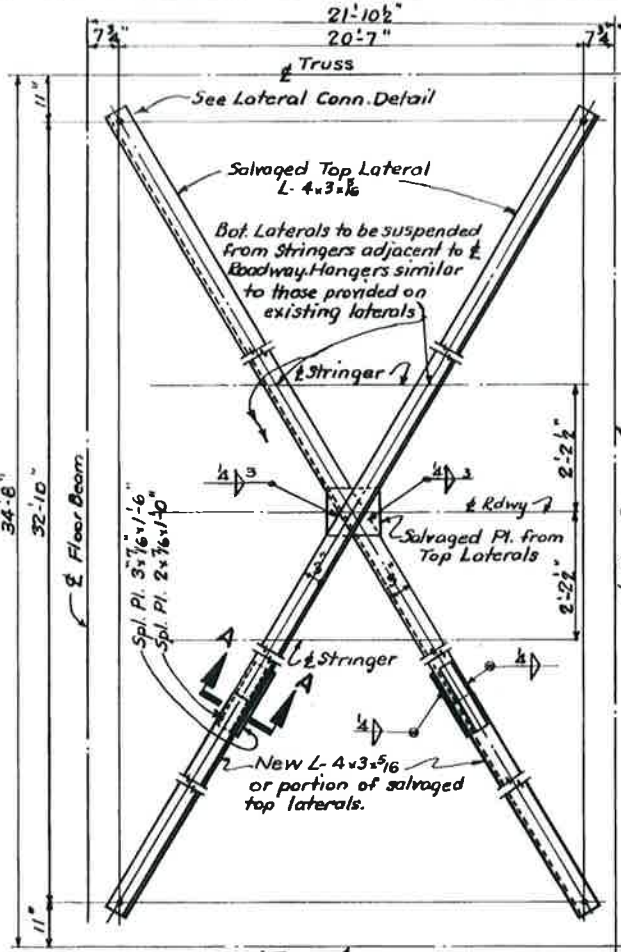
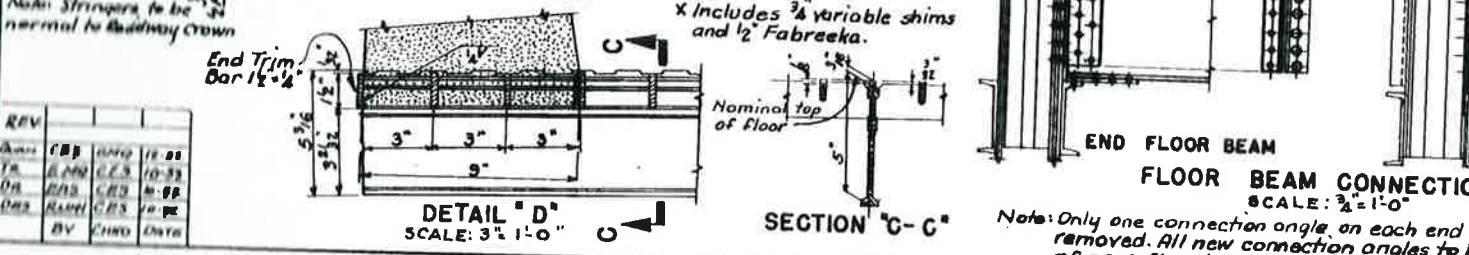
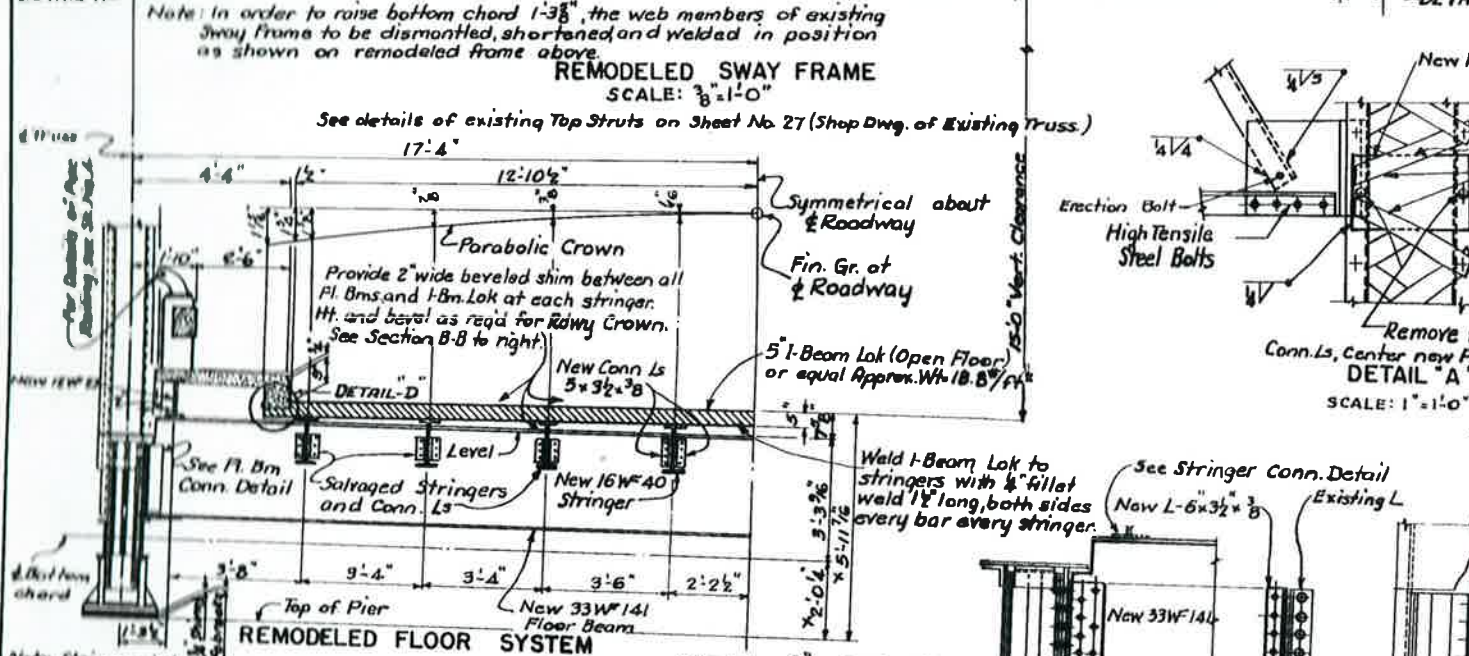
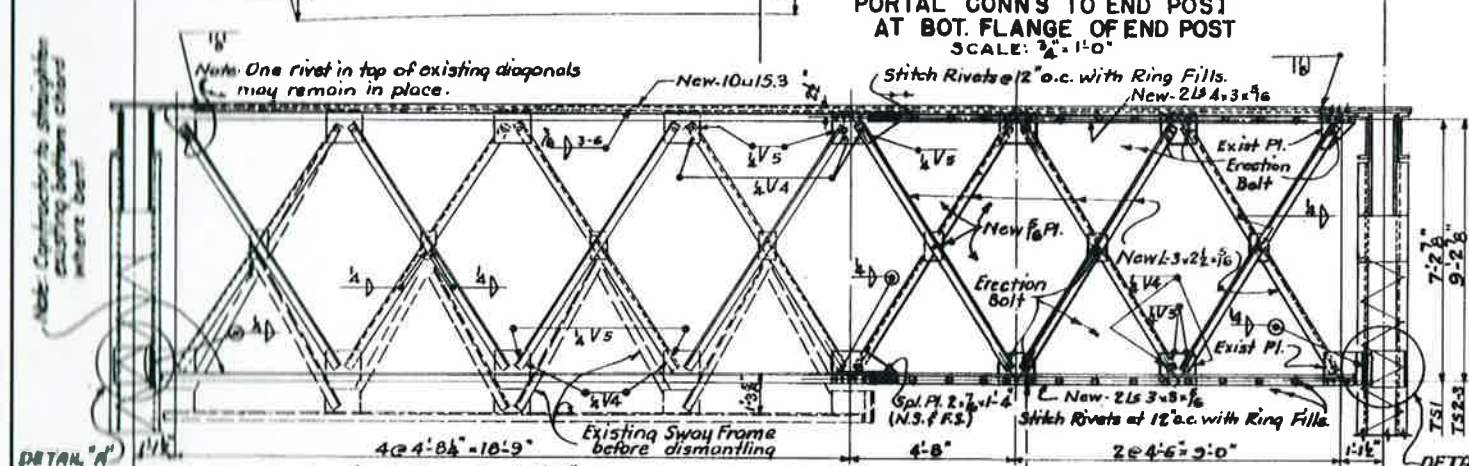
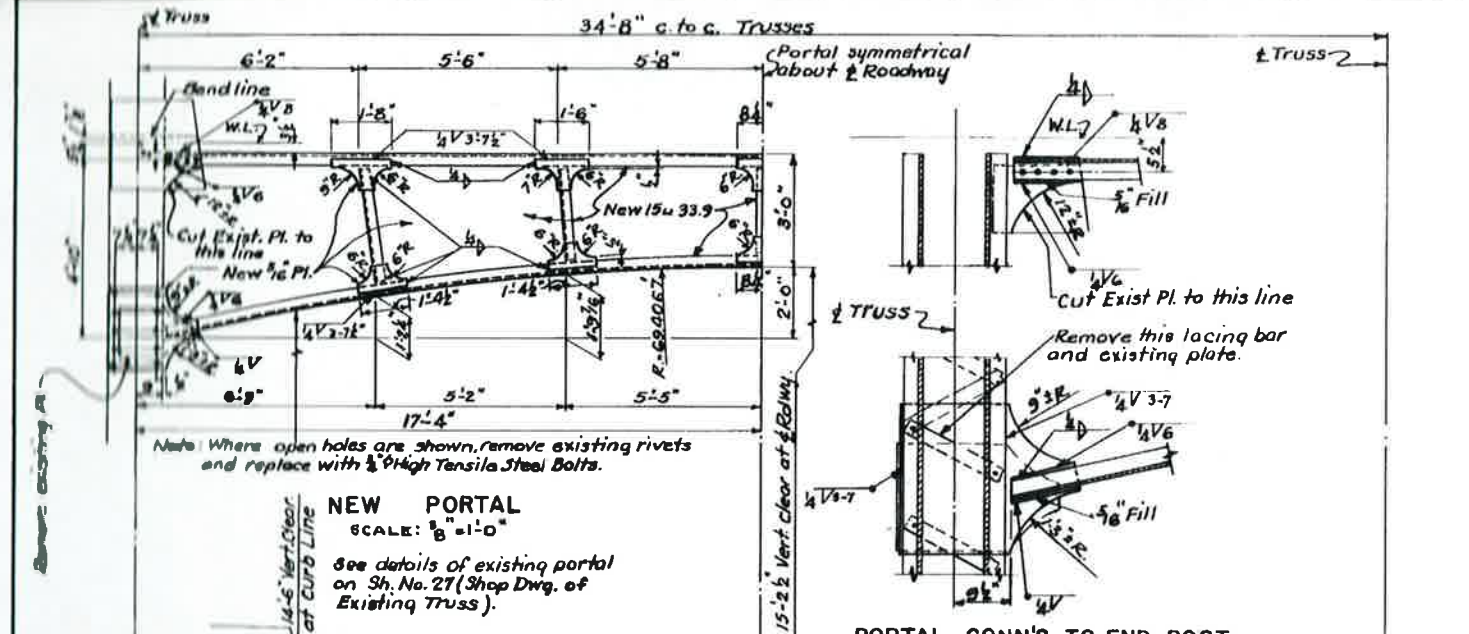
S. C. STATE HIGHWAY DEPARTMENT  
COLUMBIA  
175 FT. TRUSS SPAN  
WIDENING DETAILS  
FOR MAIN BRIDGE NO. 2 OVER  
GREAT PEE DEE RIVER  
AT YAUHANNAH

DOCKET NO 2226.2263 ROUTE NO 70  
GEORGETOWN-HORRY COUNTIES OCT 1952  
SCALE: 1" = 1'-0" OR AS NOTED



REV	1	1	
NAME	CES	CHS	PL 1
TR	110	CES	8.22
DR	CES	CES	8.32
DES	CHS	CES	8.32
	BY	CHS	DATE

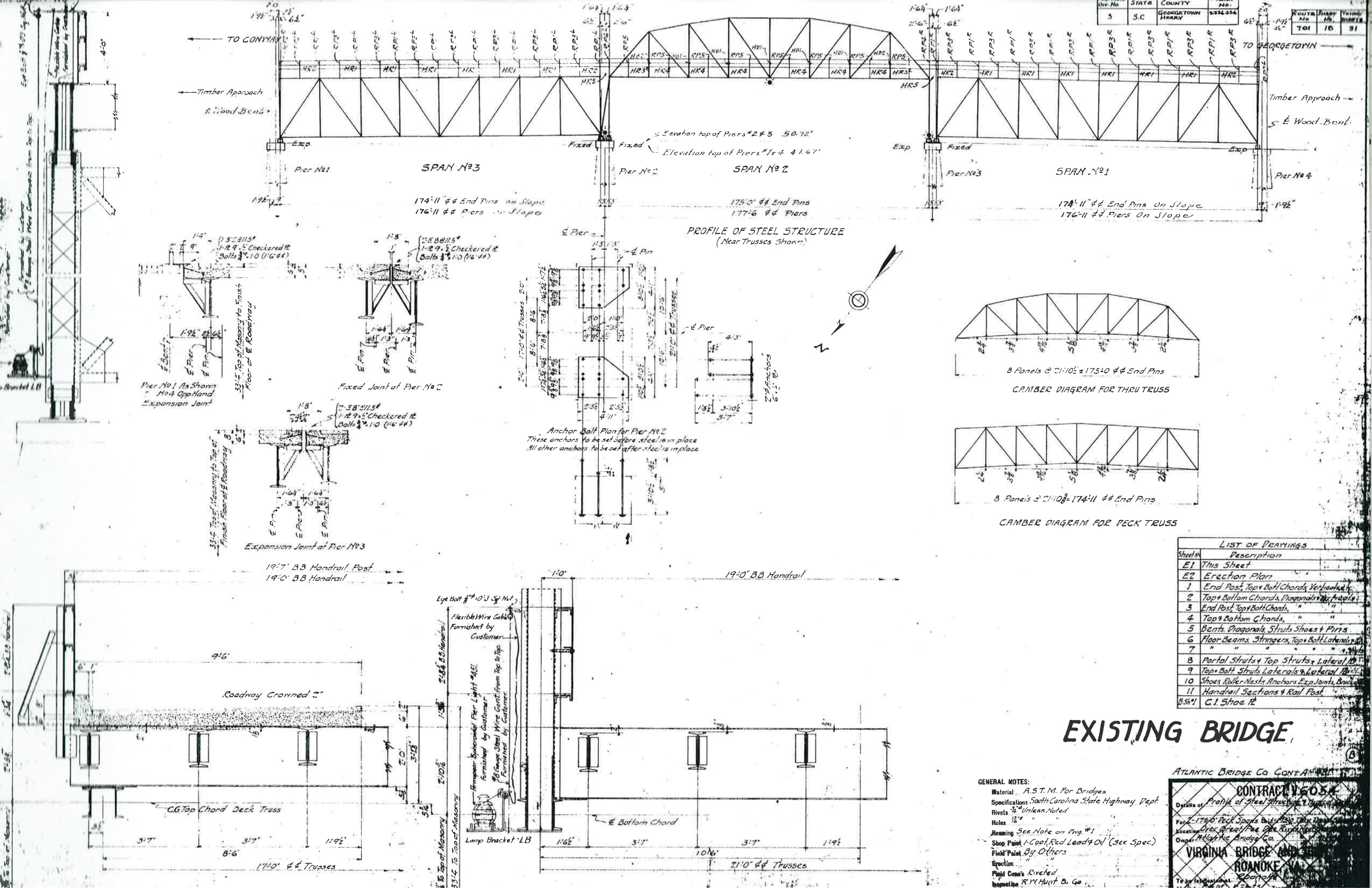






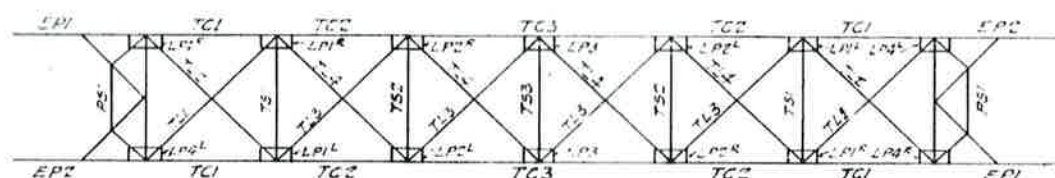
STEEL TABLE				
Mark	Nº	Size	Length	Spacing
A	352	5' 0"	19' 8"	12' of oc
B	178	12' 0"	20' 6"	12' of oc
C	138	7' 0"	31' 0"	as shown
D	24	4' 0"	31' 0"	as shown



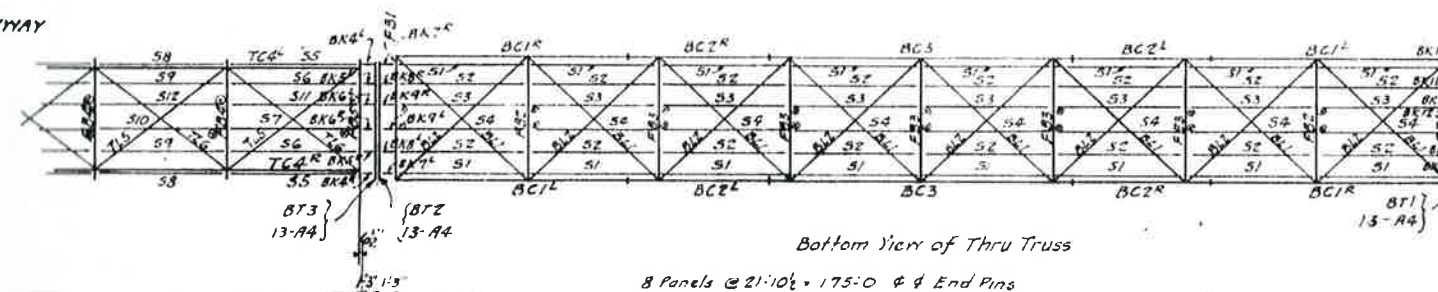




FED. ROAD DIST. NO.	STATE	COUNTY	DIST. NO.	ROUTE NO.	SHEET NO.	TOTAL SHEETS
3	S.C.	GEORGETOWN HERRY	222.124	701	19	31

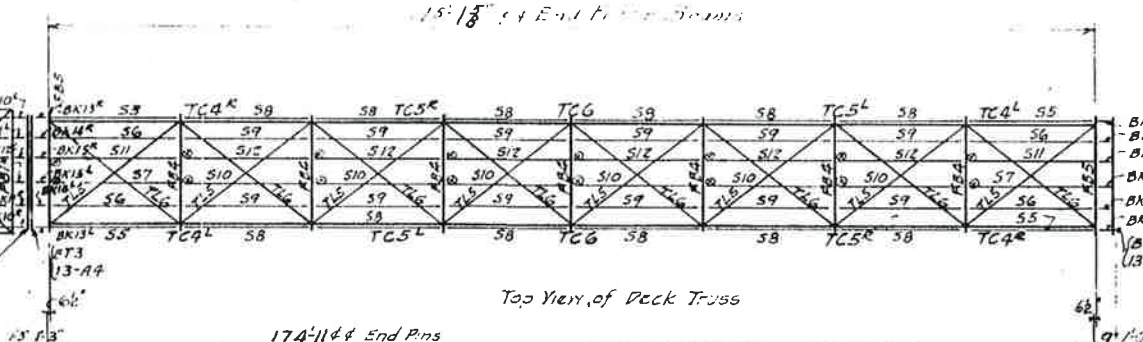


Top view of Thru Truss



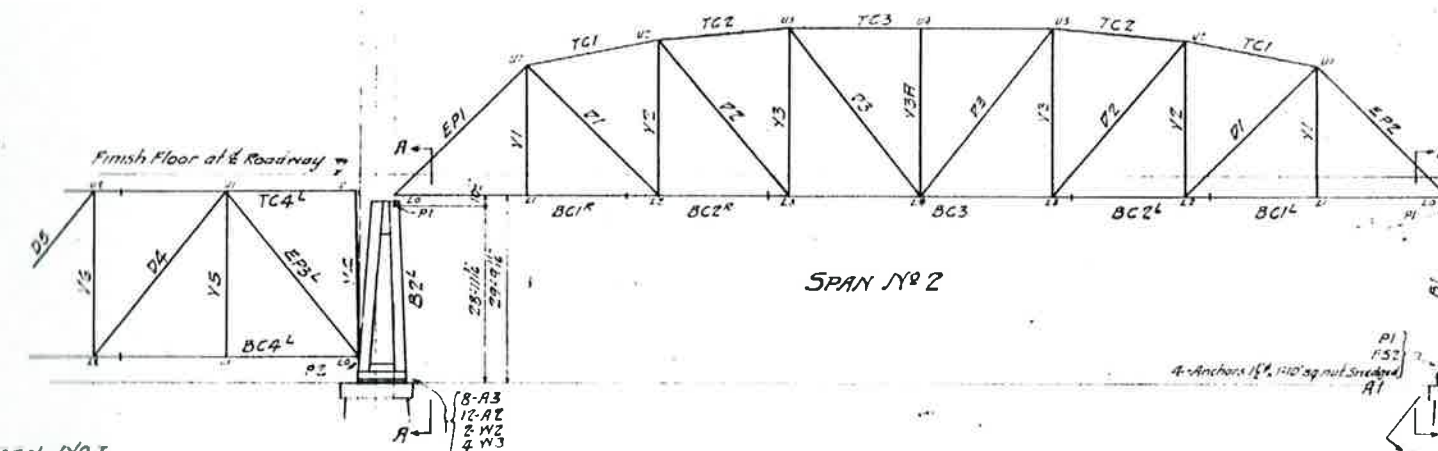
Bottom view of Thru Truss

8 Panels @ 21'-10" = 175'-0" & 4 End Pins



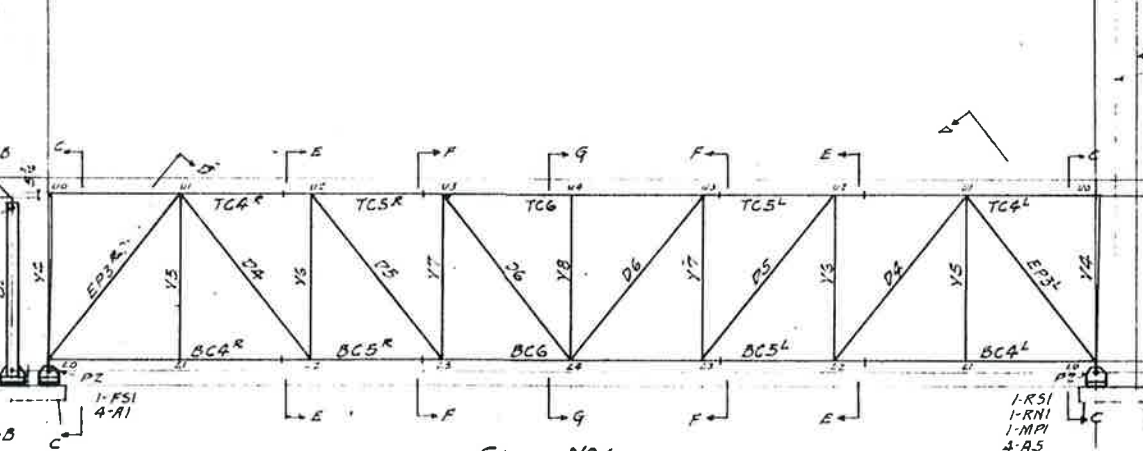
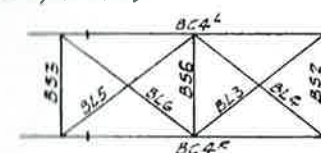
Top view of Deck Truss

174'-11 1/4" End Pins

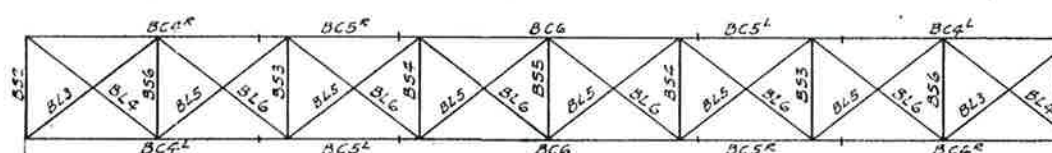


SPAN No. 2

SPAN No. 3  
(Same as Span No. 1)



SPAN No. 1



FOR HANDRAIL, RAIL POST, GENERAL LOCATION OF SPANS  
AND TYPICAL DETAILS SEE DRAWING No. E1

**EXISTING BRIDGE**

GENERAL NOTES:

- Material
- Specifications
- Rivets
- Holes
- Roaming
- Shop Paint
- Field Paint
- Erection
- Field Conn's
- Inspection

For General Notes See Sheet #E1

Atlantic Bridge Co. Cont. A-A81

**CONTRACT V.6854**

Details of Erection

For 175'-0" Thru Truss Span + 27'-5 1/2" Deck Span

Location Over Great Neck Marine, Georgetown, Herry Co., Virginia

Owner, Atlantic Bridge Co.

**VIRGINIA BRIDGE AND IRON CO.**

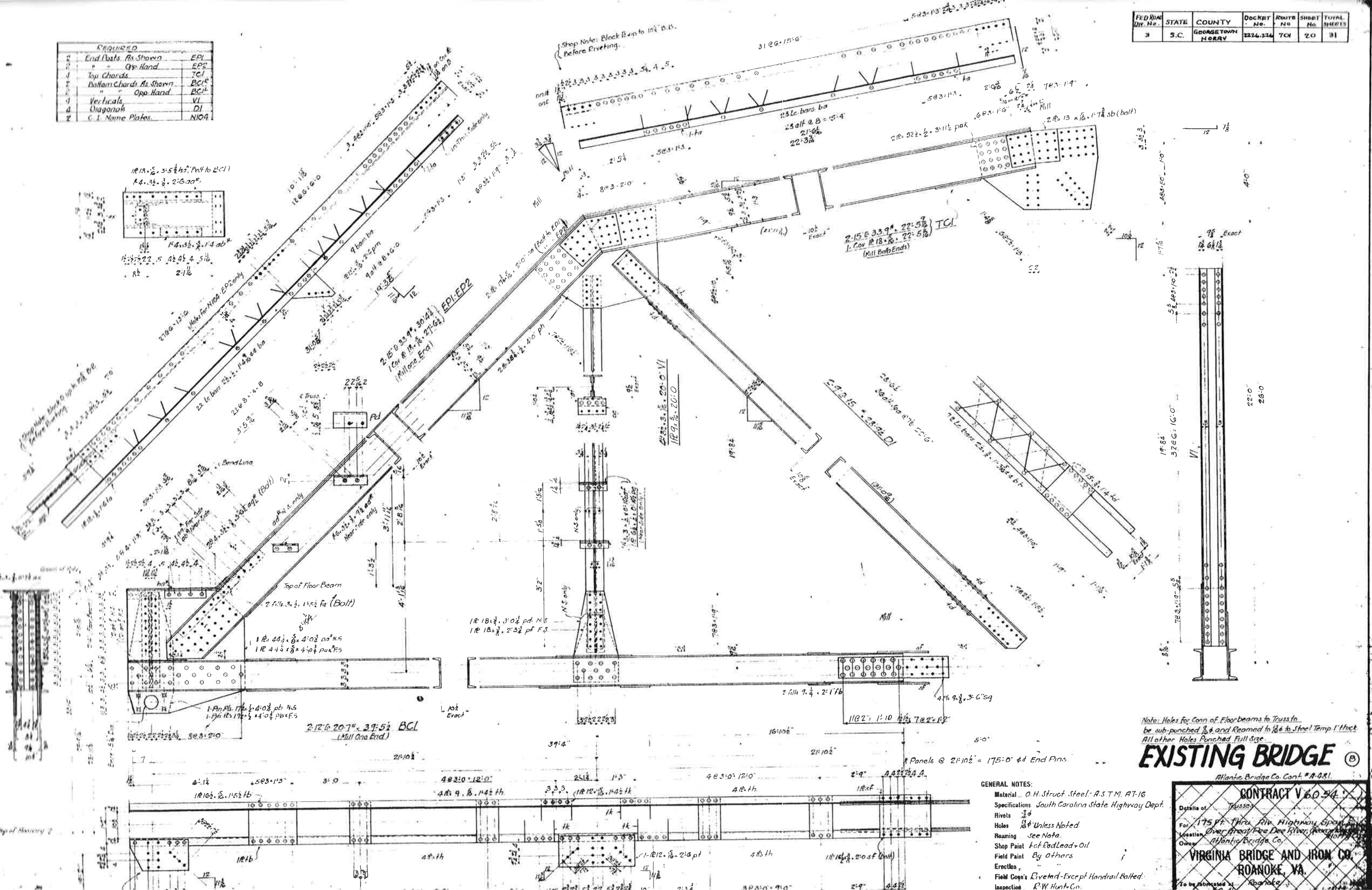
ROANOKE, VA.

To be fabricated at Roanoke



REQUIRED		
2	End Posts As Shown	EPI
2	" " Op. Hand	EP2
1	Top Chords	TC1
2	Bottom Chords As Shown	BC1
2	" " Op. Hand	BC2
1	Verticals	VI
1	Diagonals	DI
2	C. I. Name Plates	N104

FED. ROAD DIV. NO.	STATE	COUNTY	DOCKET NO.	ROUTE NO.	SHEET NO.	TOTAL SHEETS
3	S.C.	GEORGETOWN	1224.124	701	20	31



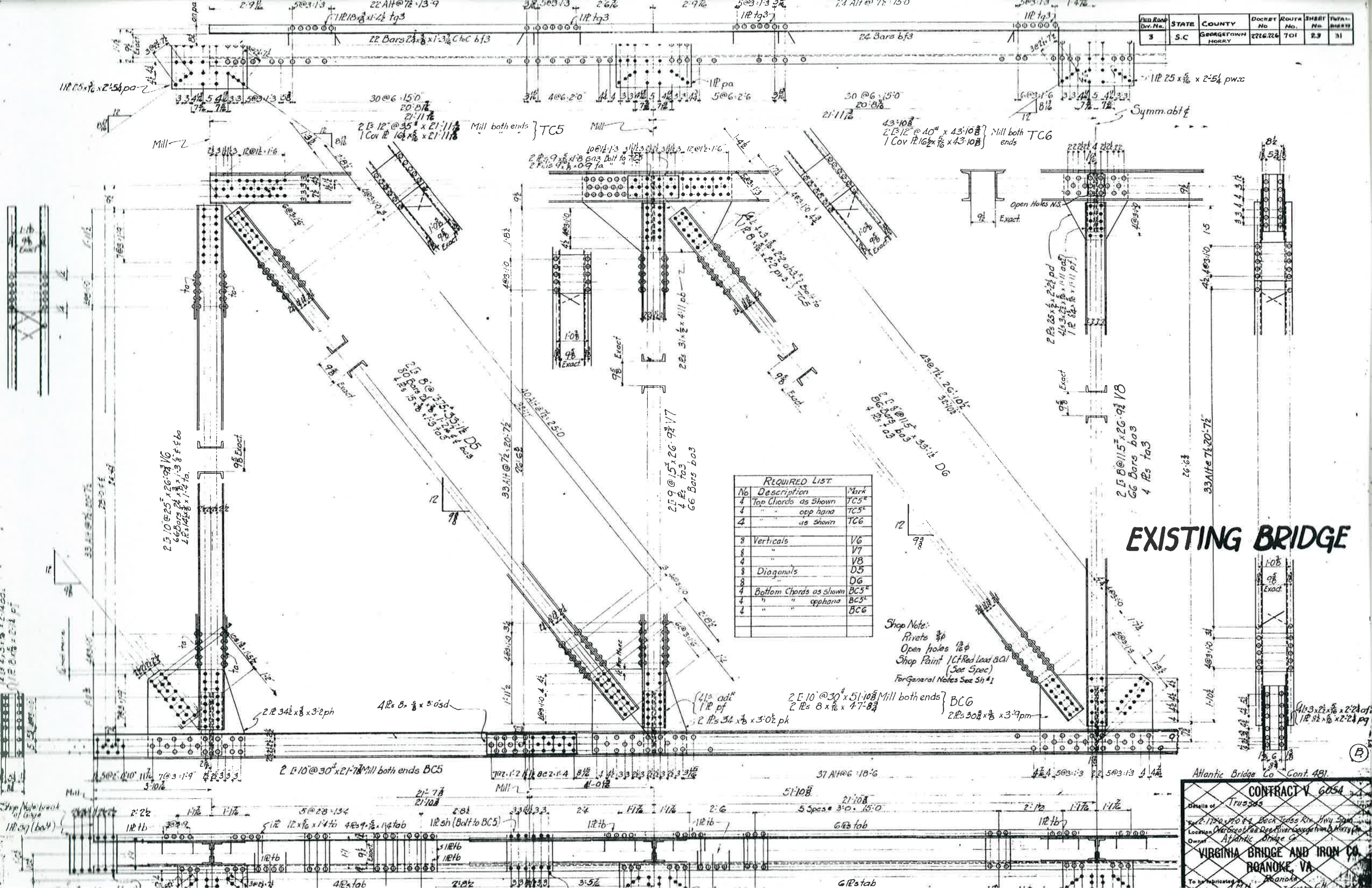


















Atlantic Bridge Co Cont: 9431

CONTRACT V 6054

Details of Floor Joists: Members Generally

Eor. 1-135 FF thru Ee Highway Span.

Location Over Pea Lake Road Georgetown & Norfolk Co

Owner Atlantic Bridge Co

VIRGINIA BRIDGE AND IRON CO.  
ROANOKE, VA.

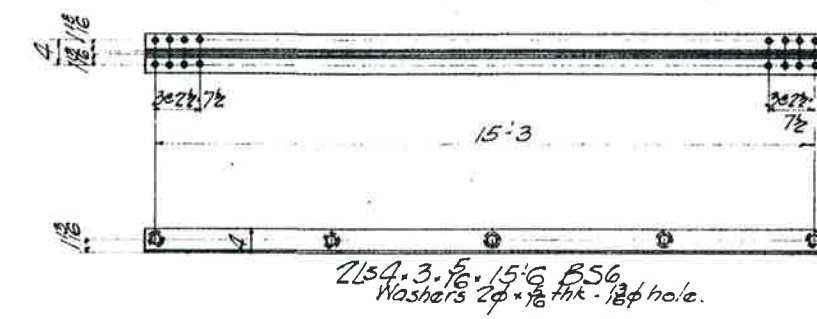
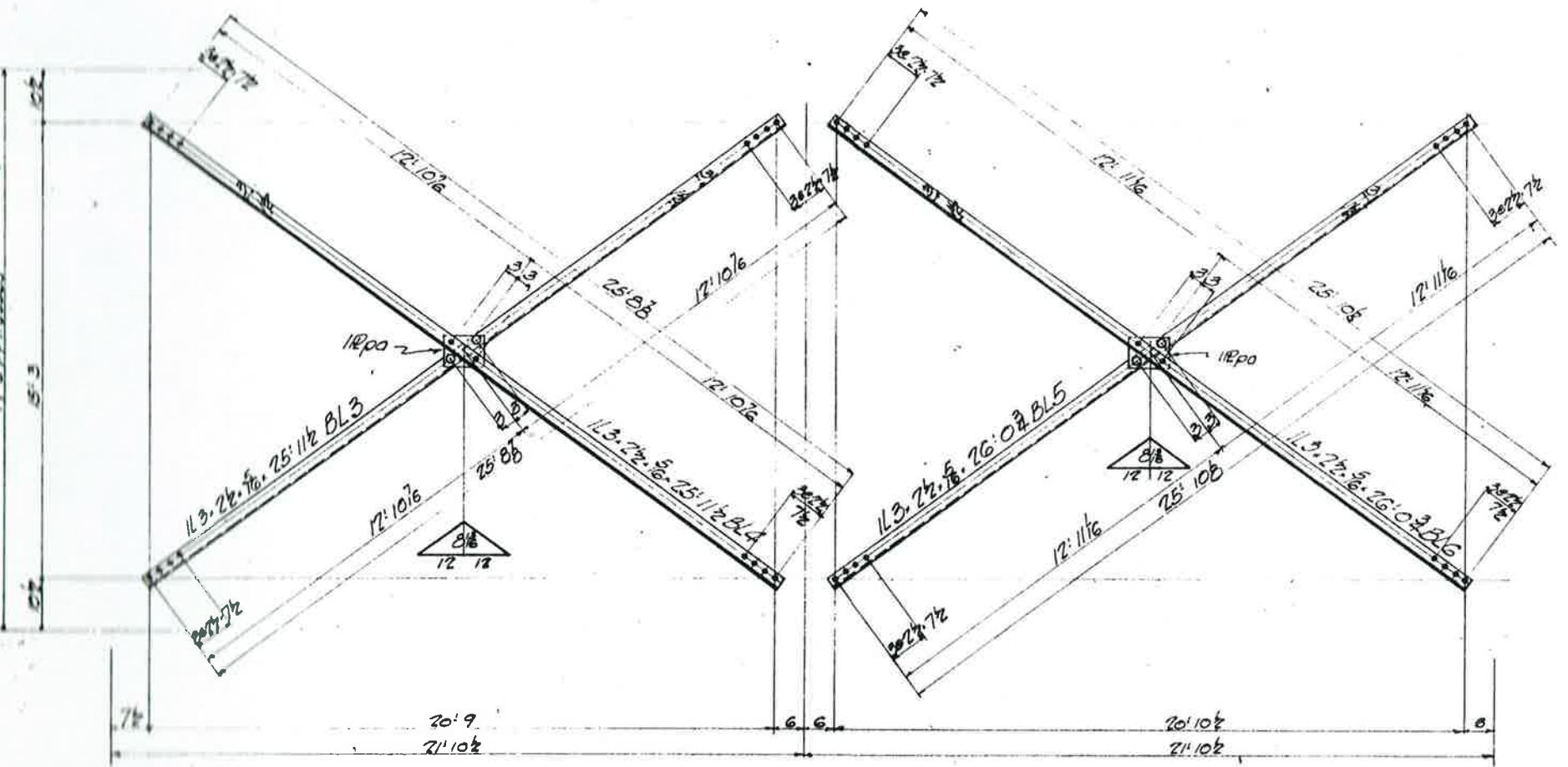
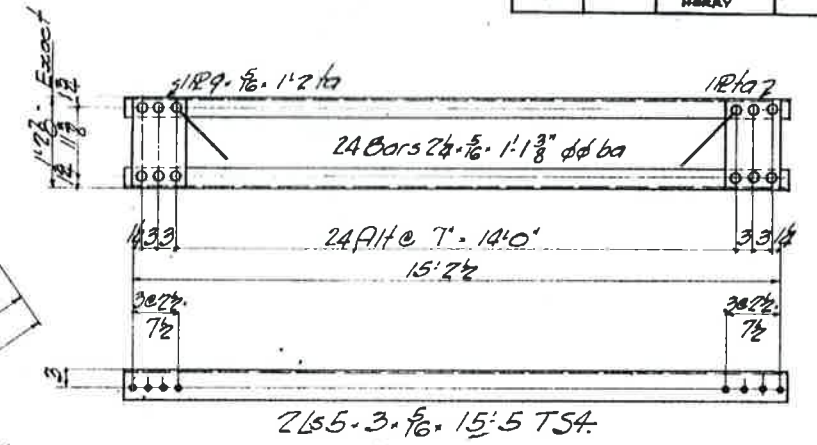
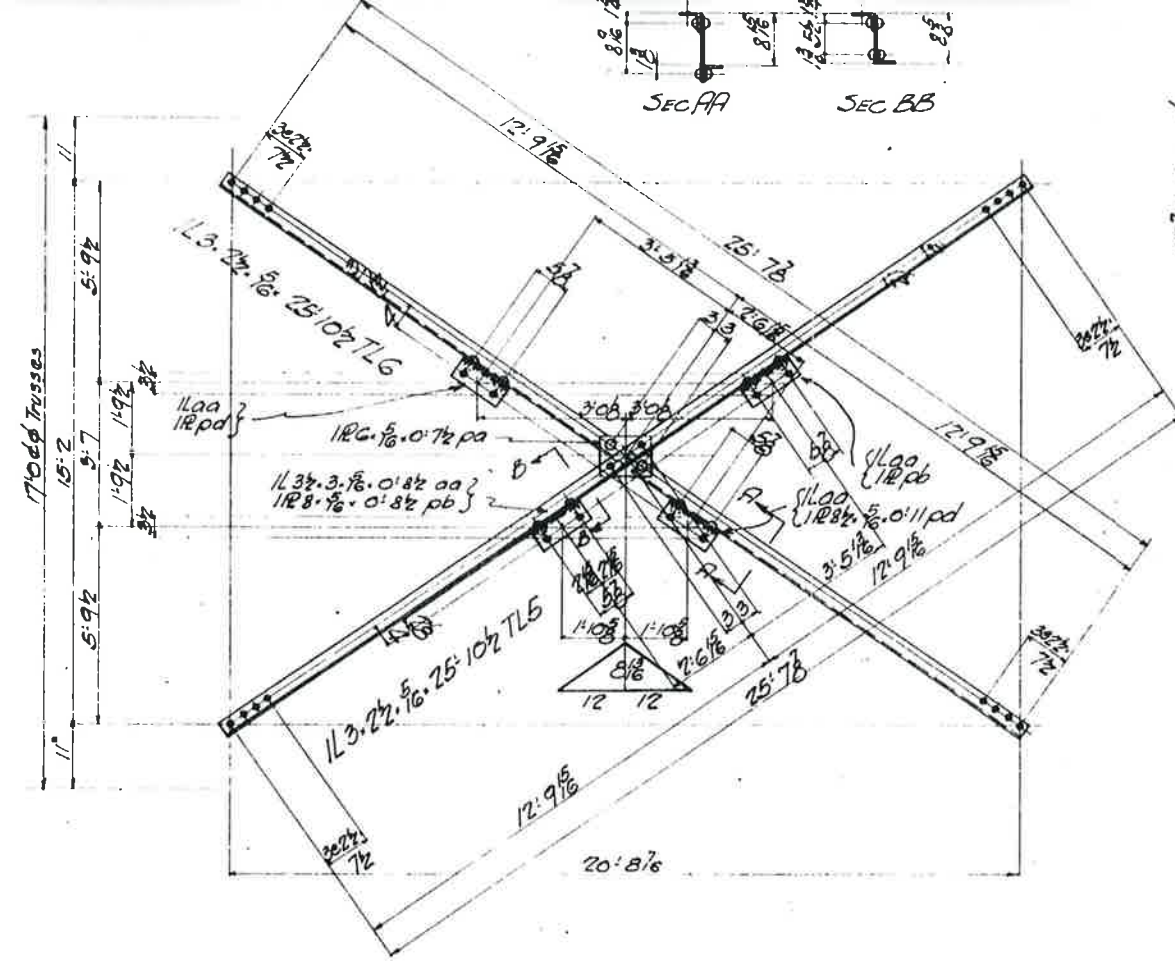
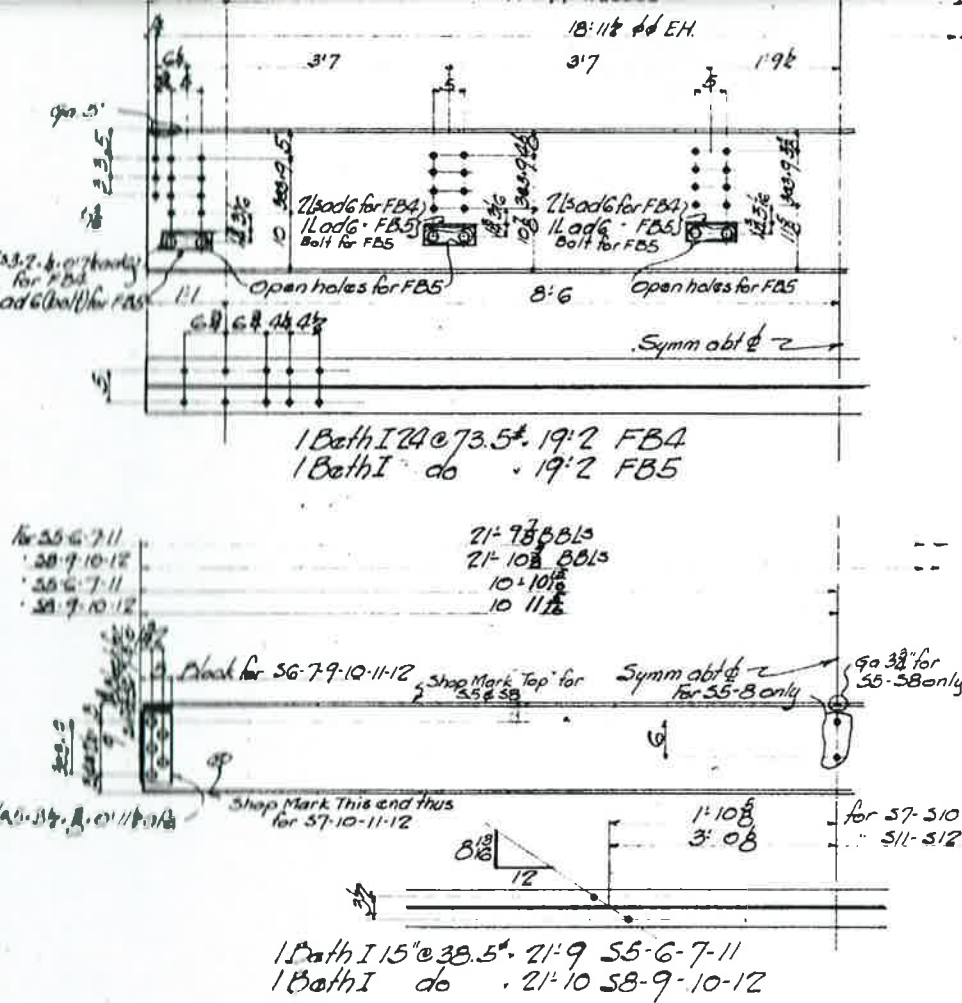
Roanoke

To be fabricated at  
Shop by C.M.S. & S.H.

Traced by H.H.H.



FED. ROAD DEV. NO.	STATE	COUNTY	DOCKET NO.	ROUTE NO.	SHEET NO.	TOTAL SHEETS
5	S.C.	GREENSWOOD HARRIS	2226-224	701	26	31



REQUIRED LIST		
No	Description	Mark
14	Floor Beams	FB4
4	"	FB5
16	Top Laterals	TL5
16	"	TL6
8	Stringers	S5
8	"	S6
4	"	S7
24	"	S8
24	"	S9
12	"	S10
4	"	S11
12	"	S12
4	Bottom Laterals	BL3
4	"	BL4
12	"	BL5
12	"	BL6
4	Bottom Struts	BS6
8	Struts	TS4

EXISTING BRIDGE

GENERAL NOTES:  
Material  
Specifications  
Rivets  
Holes  
Roaming  
Shop Paint  
Field Paint  
Erection  
Field Conn's  
Inspection

Atlantic Bridge Co Cont A481

CONTRACT V 6054

Details Floor Beams Top Laterals Bottom Laterals Stringers & Struts For 17' 0" x 20' 0" Deck Truss Kinnelburg

Location 2nd Spill Race Line East of Kinnelburg

Owner Atlantic Bridge Co

VIRGINIA BRIDGE AND IRON CO

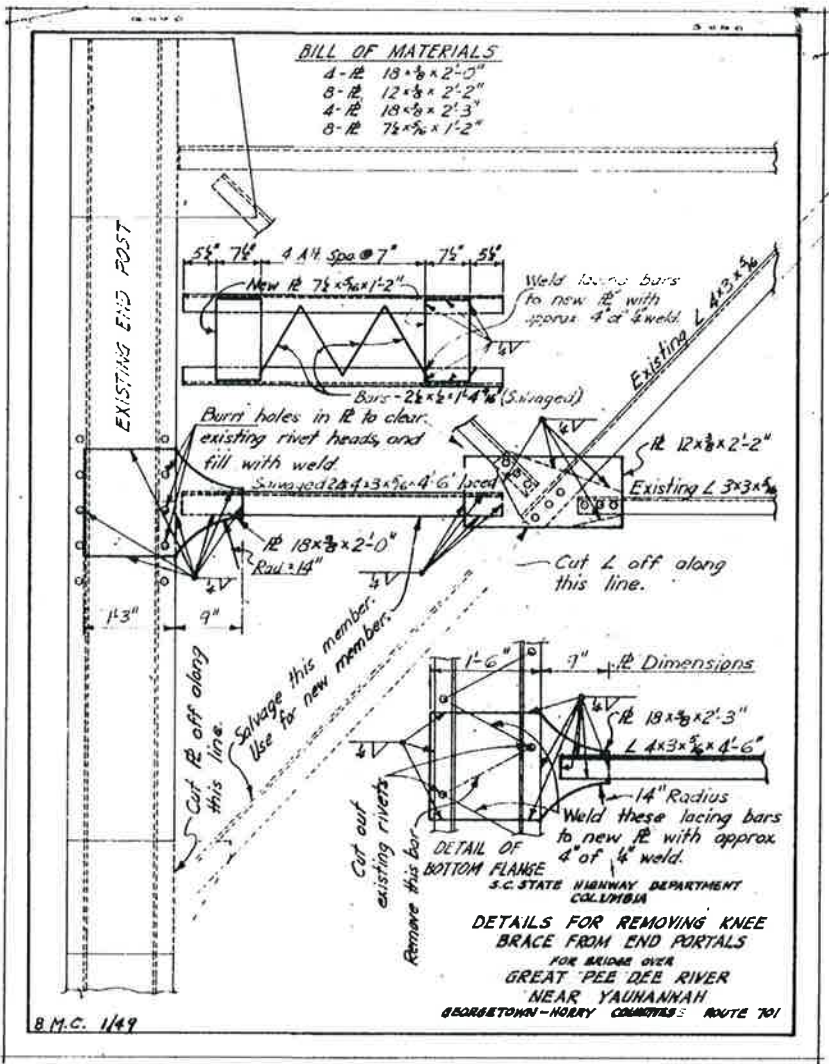
ROANOKE, VA







FED. ROAD Div. No.	STATE	COUNTY	DIST. No.	ROUTE No.	SHEET NO.	TOTAL SHEETS
3	S.C.	GEORGETOWN Horry	2226-224	701	28	31



# EXISTING BRIDGE

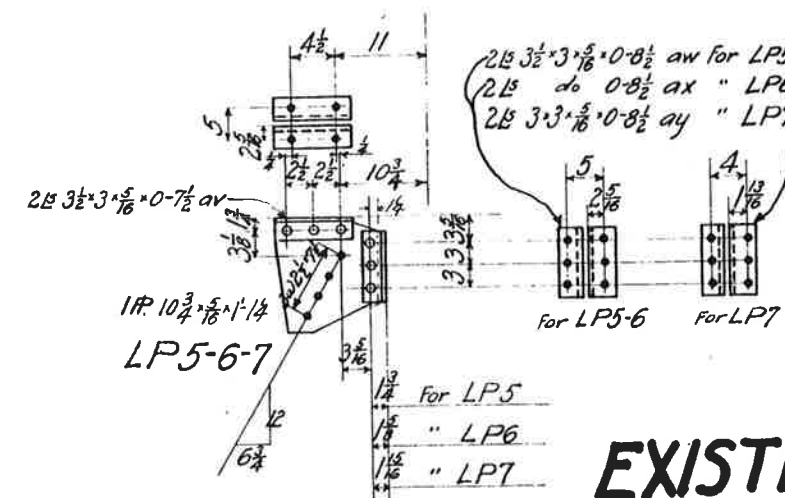
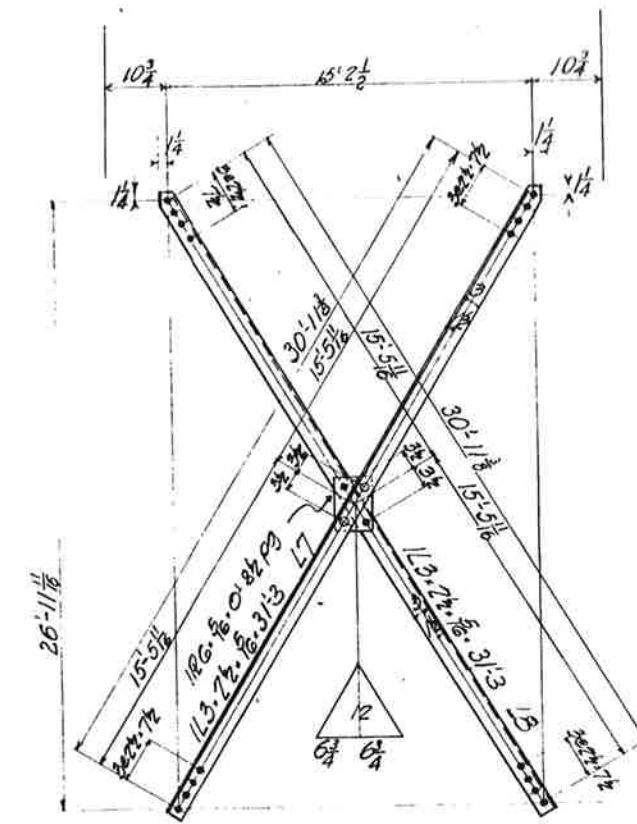
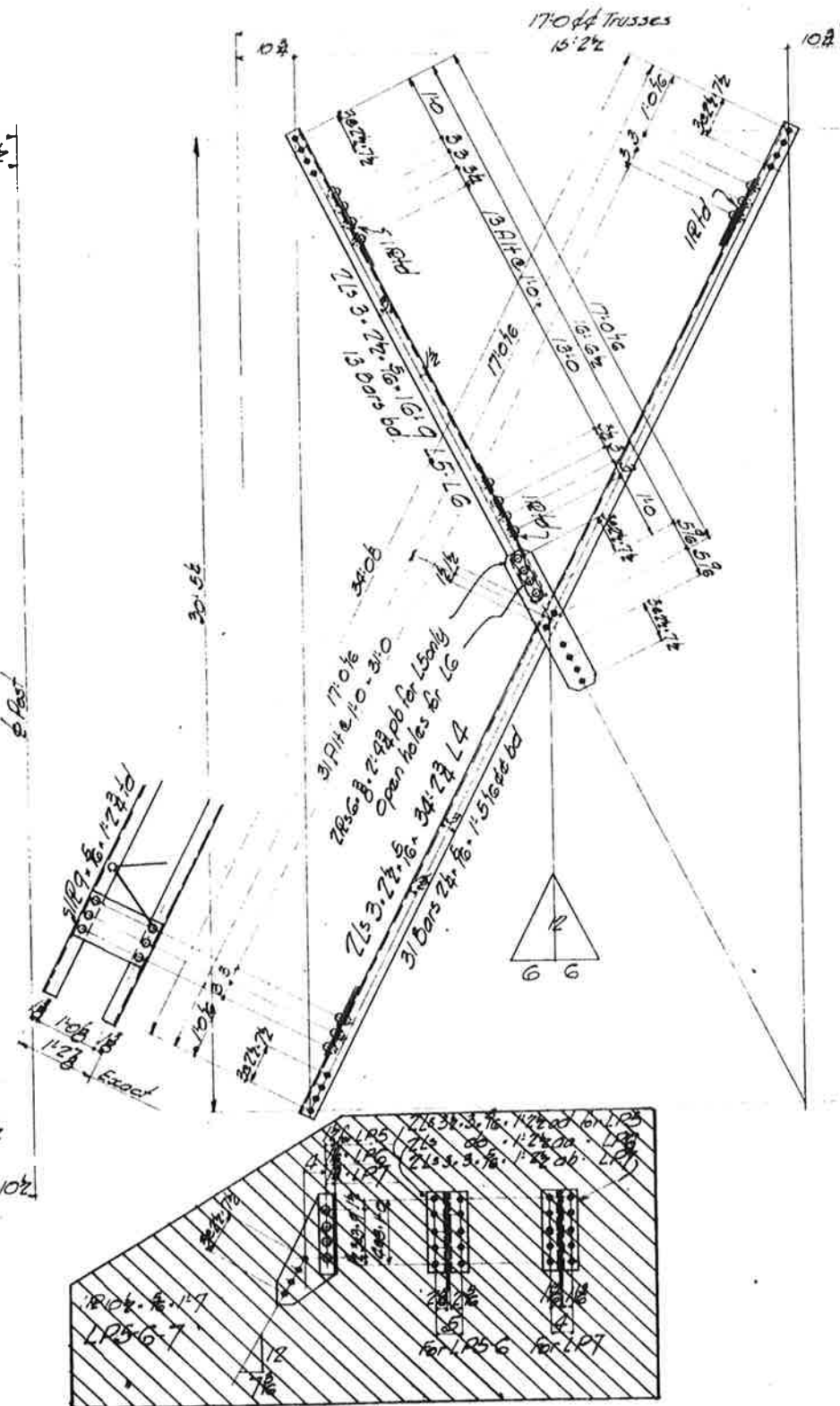
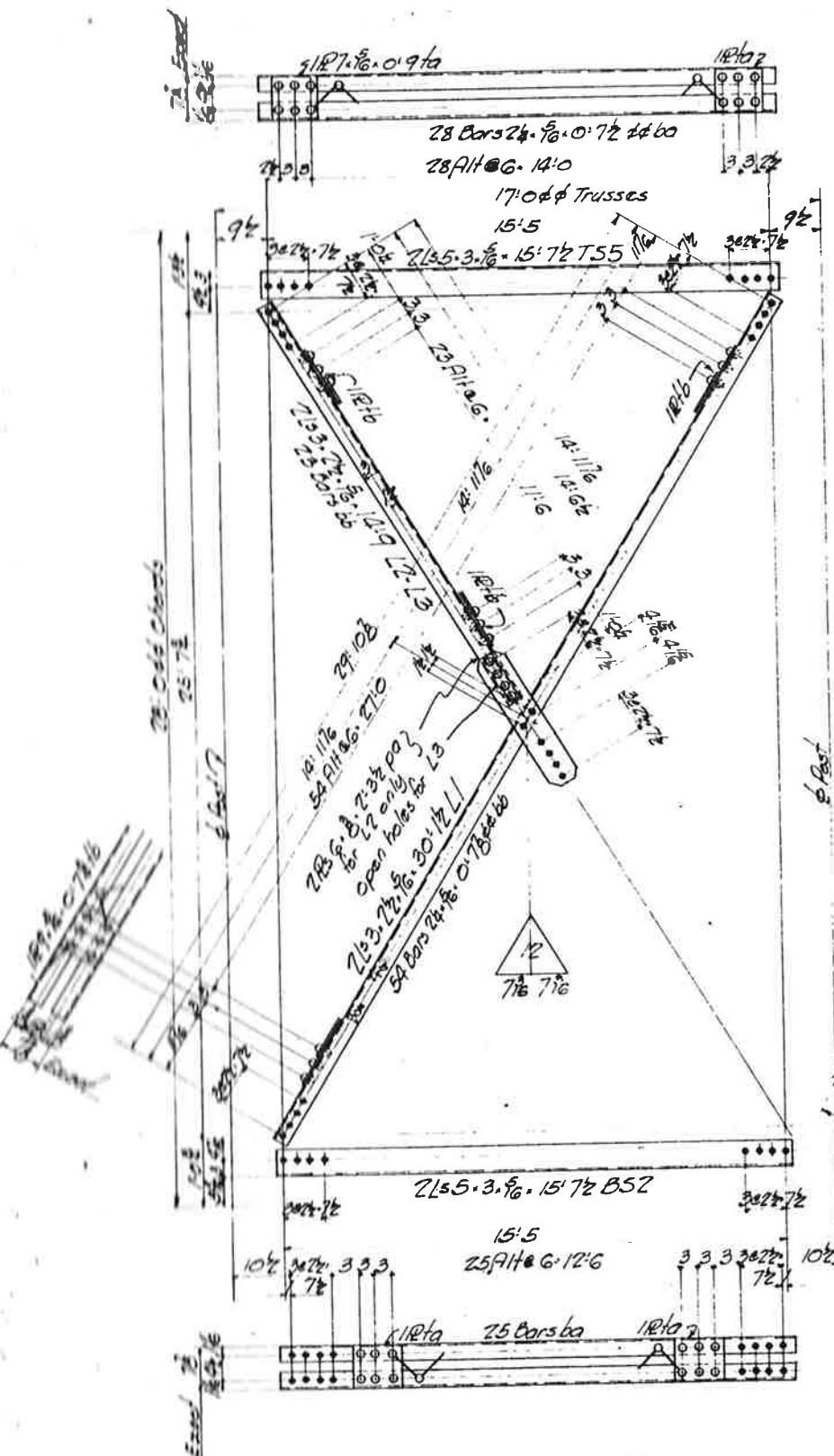
S. C. STATE HIGHWAY DEPARTMENT  
COLUMBIA

DETAILS FOR REMOVING KNEE  
FROM END PORTAL  
FOR BRIDGE OVER

GREAT PEE DEE RIVER  
NEAR YAUHANNAH

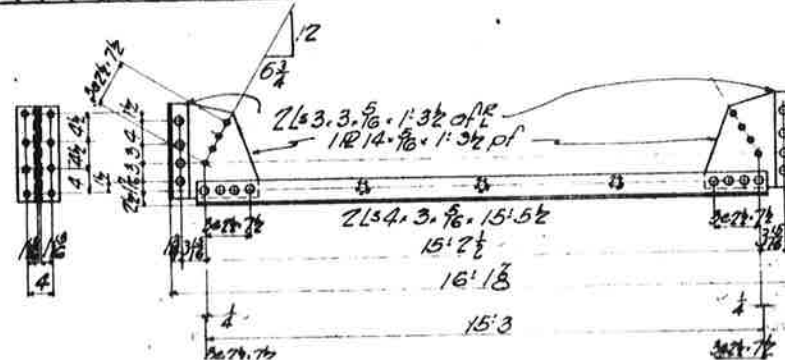
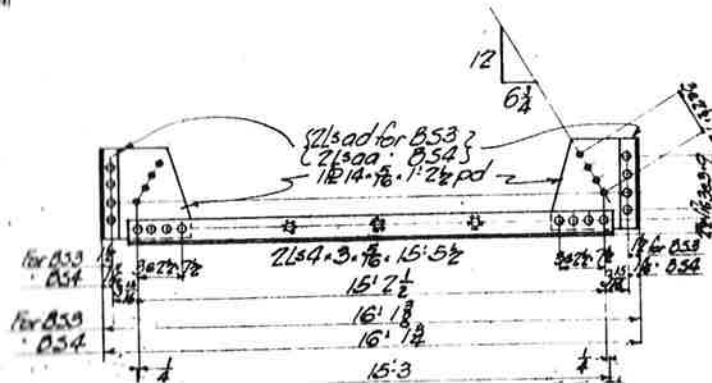
GEORGETOWN-HORRY COUNTIES ROUTE No. 701





REQUIRED LIST		
No	Description	Mark
4	Top Struts	T30
4	Bottom Struts	B52
4	Lateral	L1
4	"	L2
4	"	L3
4	"	L4
4	"	L5
4	"	L6
10	"	L7
10	"	L8
8	Lateral Plates	LP5
8	"	LP6
4	Bottom Struts	LP7
4	"	B53
2	"	B54

**EXISTING BRIDGE**



Note: Washers 2" x 1/2" x 1/2" holes.

**GENERAL NOTES:**

- Material
- Specifications
- Rivets
- Holes
- Reaming
- Shop Paint (Ch. Red Lead or Oil) (See Spec)
- Field Paint
- Erection
- Field Conn's
- Inspection

Atlantic Bridge Co. Cont. R4B1

**CONTRACT V 6004**

Route of Lateral Struts & Lateral Plates

For 17' 0" x 17' 0" Deck Truss R4B1

Location of Deck Truss R4B1

Owner: Atlantic Bridge Co.

**VIRGINIA BRIDGE AND IRON**

**ROANOKE, VA.**

To be fabricated at Roanoke, Va.

Notes: See Sht. 1

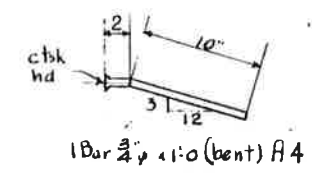
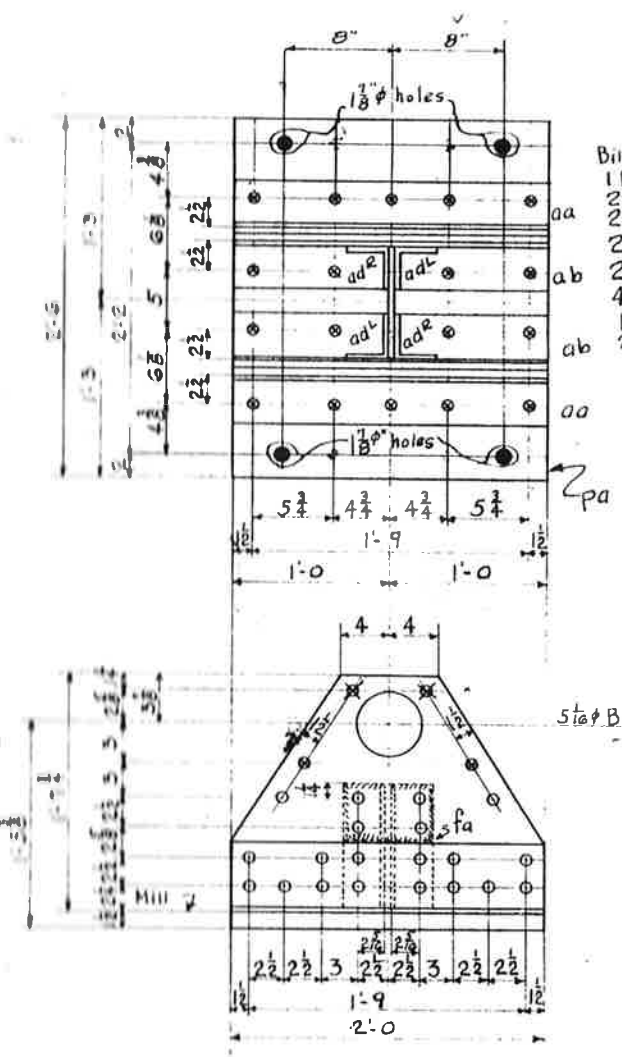
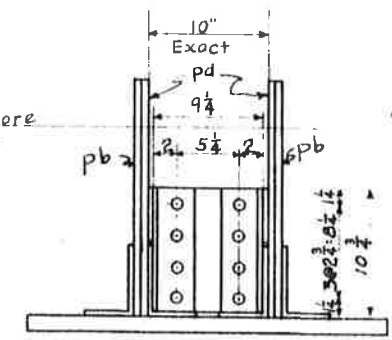
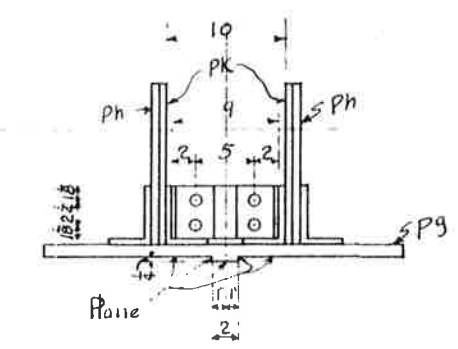
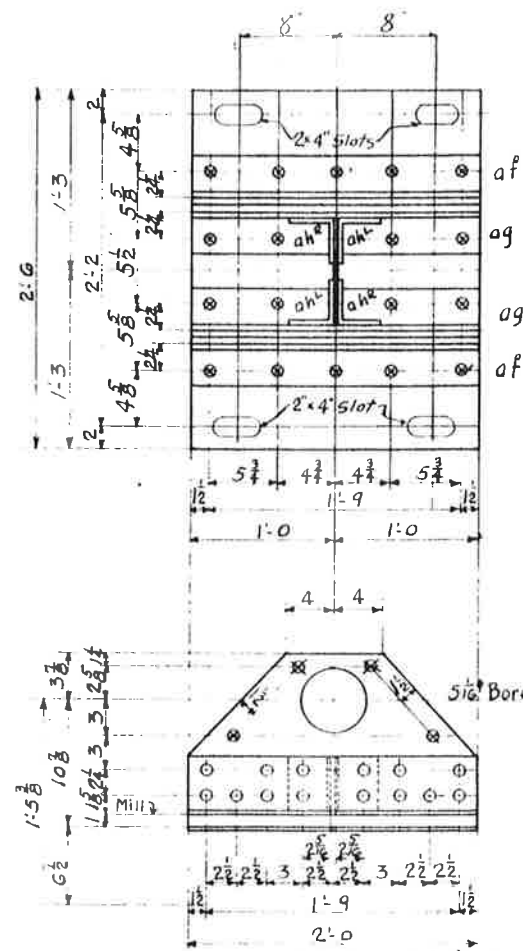
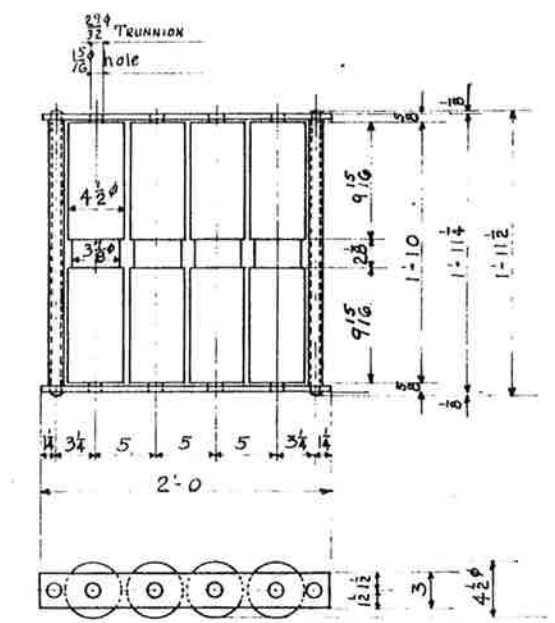


Material for Roller Nest RNI  
 4 Rollers 4 1/2" x 1 1/4" Turn from 4 3/4"  
 2 Bars 3 1/2" x 2 1/2" ba.  
 2 Pcs Gas Pipe 1" x 1 1/4" lg ma.  
 2 Bars 3/4" x 2 1/2" bb Rivet.

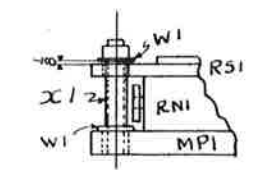
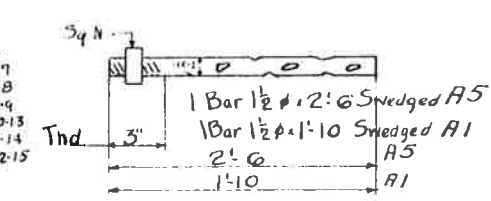
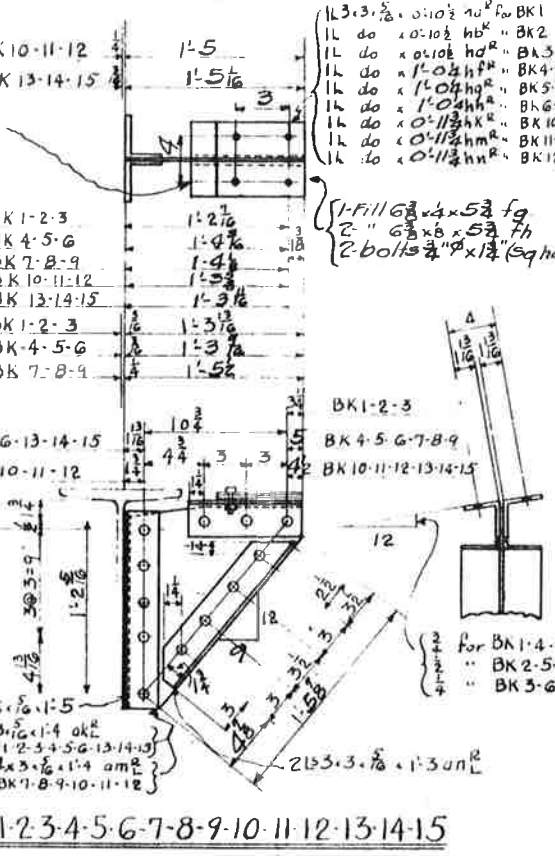
FED ROAD Div. No.	STATE	COUNTY	PROJECT No.	ROUTE No.	SHEET No.	TOTAL SHEETS
3	S. C.	GEORGETOWN Horry	1026-226	701	80	81

Bill of Material for One Fixed Shoe FSI.  
 1 R 24 x 1 1/2 x 2 1/2 pa  
 2 R 24 x 1 1/2 x 1 1/2 pb Mill one end.  
 2 R 24 x 1 1/2 x 1 1/2 pd " " "  
 2 L 6 x 4 x 1/2 x 2 1/2 aa  
 2 L 6 x 4 x 1/2 x 2 1/2 ab  
 4 L 3 1/2 x 3 1/2 x 1/2 x 2 1/2 ad } Diaphragm  
 1 R 9 x 1/2 x 1 1/2 pf  
 2 Fills 5 x 1/2 x 1 1/2 fa

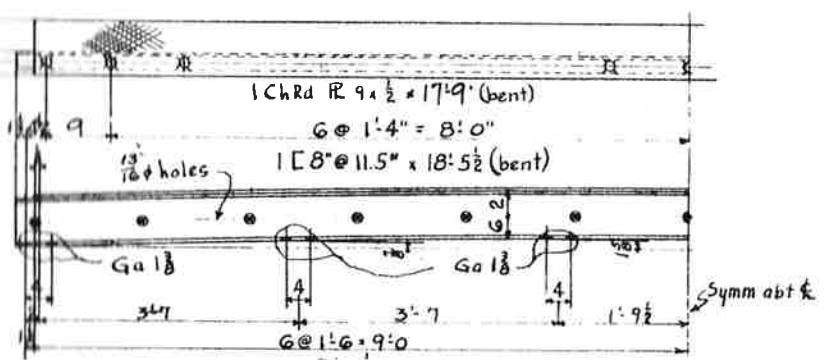
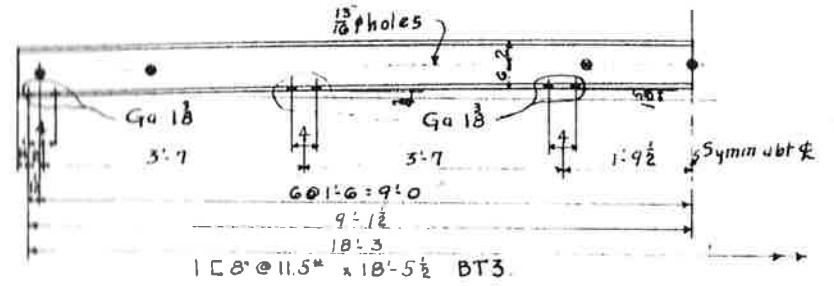
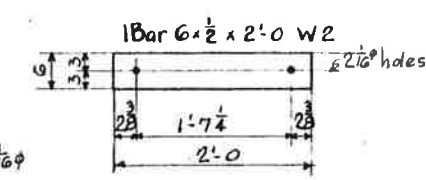
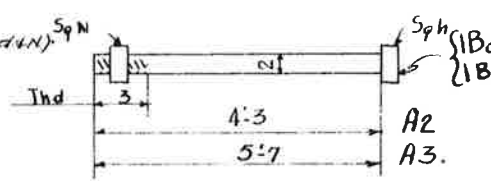
Material for One Roller Shoe RSI.  
 1 R 24 x 1 1/2 x 2 1/2 pg Plane from 1 1/2  
 2 R 24 x 1 1/2 x 1 1/2 ph Mill one end  
 2 R 24 x 1 1/2 x 1 1/2 pk  
 2 L 5 x 3 1/2 x 1/2 x 2 1/2 af  
 2 L 5 x 3 1/2 x 1/2 x 2 1/2 ag  
 4 L 3 1/2 x 3 1/2 x 1/2 x 2 1/2 ah } Diaphragm  
 1 Bar 4 1/2 x 1/2 x 1 1/2 paa



1 L 3 x 3/4 x 1/2 hp BK1  
 1 L do 1/2 x 1/2 hs BK2  
 1 L do 1/2 x 1/2 ht BK3  
 1 L do 1/2 x 1/2 hv BK4-7  
 1 L do 1/2 x 1/2 hw BK5-8  
 1 L do 1/2 x 1/2 hx BK6-9  
 1 L do 1/2 x 1/2 hy BK10-13  
 1 L do 1/2 x 1/2 ha BK11-14  
 1 L do 1/2 x 1/2 hb BK12-15



Sketch of Roller and Shoe.



No	Description	Mark
2	Fixed Shoes	FSI
4	Roller Shoes	RSI
4	Roller Nests	RNI
32	Washers 3/4" x 1/2" x 1/2" hole	W1
16	Pos 1/2" Gas Pipe x 0.5" lg	X1
16	Anchors	A1
12	"	A2
8	"	A3
2	Washers	W2
4	"	W3
3	Expansion Joints	BT1
1	"	BT2
2	"	BT3
78	Anchors	A4
2	Brackets as shown	BK1
2	" opp hand	BK1L
2	" as shown	BK2
2	" opp hand	BK2L
2	" as shown	BK3
2	" opp hand	BK3L
1	" as shown	BK4
1	" opp hand	BK4L
1	" as shown	BK5
1	" opp hand	BK5L
1	" as shown	BK6
1	" opp hand	BK6L
1	" as shown	BK7
1	" opp hand	BK7L
1	" as shown	BK8
1	" opp hand	BK8L
1	" as shown	BK9
1	" opp hand	BK9L
1	" as shown	BK10
1	" opp hand	BK10L
16	Anchors	A5

No	Description	Mark
1	Bracket opp hand	BK1L
1	" as shown	BK12R
1	" opp hand	BK12L
1	" as shown	BK13R
1	" opp hand	BK13L
1	" as shown	BK14R
1	" opp hand	BK14L
1	" as shown	BK15R
1	" opp hand	BK15L
16	Anchors	A5

# EXISTING BRIDGE

GENERAL NOTES:  
 Material  
 Specifications  
 Rivets 3/4"  
 Holes 7/8" unless noted  
 Reaming  
 Shop Paint 1st Red Lead & Oil (See Spec)  
 Field Paint  
 Erection  
 Field Conn's

ATLANTIC BRIDGE CO. No. 4671

CONTRACT V 6054

Shoes, Roller Nests, Anchors, Expansion Joints, Brackets

For Location: Over Grant Res. Pk. Rte. 1, Georgetown & Horry Co. S.C.

Owner: Atlantic Bridge Co.

VIRGINIA BRIDGE AND IRON CO.

ROANOKE, VA.



