

INDEX OF SHEETS

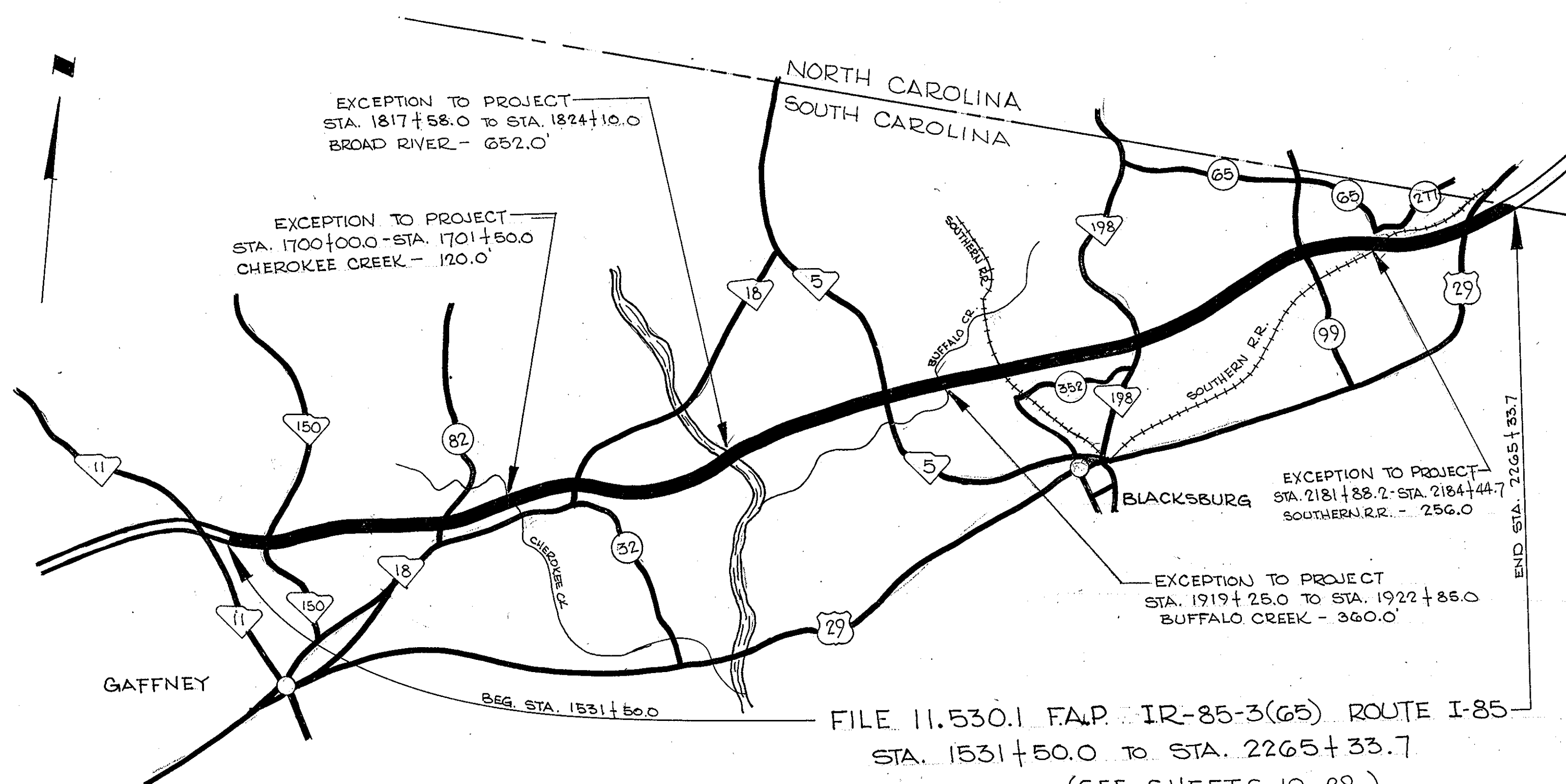
| | |
|-------------|-----------------------|
| SHEET No. 1 | TITLE |
| " 2-3-4 | TYPICAL SECTIONS |
| " 5-5F | GUARD RAIL STD. |
| " 6 | PIPE UNDERDRAIN |
| " 7 | HAY BALES |
| " 8 | MEDIAN DITCH STD. |
| " 9 | CATCH BASIN (TYPE 12) |
| 10-29 | PLAN SHEETS |
| 30-32 | TRAFFIC CONTROL |
| 33-60 | PAVEMENT MARKINGS |

SOUTH CAROLINA
STATE HIGHWAY DEPARTMENT
COLUMBIA

PLAN AND PROFILE OF PROPOSED
STATE HIGHWAY
CHEROKEE COUNTY
FILE 11.530.1 F.A.P. IR-85-3(65)
ROUTE I-85
FROM: ROUTE 11 TO: N.C. STATE LINE

| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | F.A.P. PROJ. NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|------------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | IR-85-3(65) | I-85 | 1 | 60 |

| SUMMARY OF ESTIMATED QUANTITIES | | IR-85-3(65) | UNIT |
|---|----------|-------------|------|
| TRAFFIC CONTROL | NEC. | | L.S. |
| BREAKING AND RETAINING OF EXISTING CONCRETE GUTTER | 7,100 | | S.Y. |
| REMOVAL OF EXISTING CONCRETE GUTTER | 7,100 | | S.Y. |
| UNCLASSIFIED EXCAVATION | 68,750 | | C.Y. |
| ASPHALT CEMENT IN PAVING MIXTURE | 5,699 | | TONS |
| ASPHALTIC CONCRETE BINDER COURSE (TYPE 1) | 39,400 | | TONS |
| ASPHALTIC CONCRETE BINDER COURSE (TYPE 2) | 41,700 | | TONS |
| ASPHALTIC CONCRETE SURFACE COURSE (TYPE 3) | 27,400 | | TONS |
| ASPHALTIC CONCRETE SURFACE COURSE FOR DITCH PAVING (TYPE 3) | 1,125 | | TONS |
| NYLON FILAMENT MATTING (FOR DITCH PAVING-8' WIDE) | 3,750 | | S.Y. |
| FIBERGLASS ROVING (FOR DITCH PAVING-8' WIDE) | 3,750 | | S.Y. |
| BITUMINOUS SURFACING (SINGLE TREATMENT) TYPE 3 | 243,000 | | S.Y. |
| BITUMINOUS SURFACING (SINGLE TREATMENT) TYPE 5 | 496,000 | | S.Y. |
| PART DEPTH ASPHALT PATCHING | 27,500 | | S.F. |
| FULL DEPTH ASPHALT PATCHING | 6,000 | | S.Y. |
| CRUSHER RUN STONE (CR 14) | 1,800 | | TONS |
| EMULSIFIED ASPHALT FOR CRACK SEALING | 10,500 | | GAL. |
| 15" R.C. PIPE CULVERT (CLASS III) | 268 | | L.F. |
| 18" R.C. PIPE CULVERT (CLASS III) | 412 | | L.F. |
| 24" R.C. PIPE CULVERT (CLASS III) | 120 | | L.F. |
| 30" R.C. PIPE CULVERT (CLASS III) | 144 | | L.F. |
| 18" R.C. PIPE (CLASS III) BEND (30°) | 1 | | EACH |
| JUNCTION BOX (30"x30") | 2 | | EACH |
| CATCH BASIN (TYPE NO. 12) | 34 | | EACH |
| CATCH BASIN (TYPE NO. 12) CATCH BASIN (TYPE NO. 9) CONVERTED | 103 | | EACH |
| END ANCHOR (TYPE A) CABLE END CONVERTED | 42 | | EACH |
| STEEL BEAM GUARD RAIL | 36,912.5 | | L.F. |
| ADJUSTED STEEL BEAM GUARD RAIL | 4,515.0 | | L.F. |
| REMOVAL OF EXISTING GUARD RAIL | 17,012.5 | | L.F. |
| END ANCHOR (TYPE A) | 49 | | EACH |
| END ANCHOR (TYPE B) | 35 | | EACH |
| 4" CORRUGATED PIPE UNDERDRAIN | 34,400 | | L.F. |
| HAY BALES | 1,100 | | EACH |
| SEEDING (MULCHED) | 352.856 | | MSY. |
| FERTILIZER (4-12-12) | 36.42 | | TONS |
| LIME | 72.84 | | TONS |
| NITROGEN | 3,496 | | LBS. |
| 4" WHITE BROKEN LINES (LANE LINES, THERMOPLASTIC LENGTH ONLY-GAP EXCLUDED) 125 MIL. | 37,780 | | L.F. |
| 4" WHITE SOLID LINES (PAVEMENT EDGELINES) 90 MIL. | 183,325 | | L.F. |
| 8" WHITE SOLID LINES (GORE MARKINGS) 125 MIL. | 6,595 | | L.F. |
| 4" YELLOW SOLID LINES (PAVEMENT EDGELINES) 90 MIL. | 182,520 | | L.F. |
| 12" WHITE SOLID LINES (STOP LINES, TRANSVERSE LINES) 125 MIL. | 485 | | L.F. |
| WHITE SINGLE ARROWS (STRAIGHT) 125 MIL. | 15 | | EACH |
| RAISED MARKERS (INCLUDING GROOVES) | 2,430 | | EACH |
| RAISED MARKERS (NO GROOVES) | 20 | | EACH |
| * GUARD RAIL POST (ADDITION LENGTHS) | 1000 | | L.F. |



FILE 11.530.1 F.A.P. IR-85-3(65) ROUTE I-85
STA. 1531+50.0 TO STA. 2265+33.7
(SEE SHEETS 10-29)

SCALE: 1" = 5,280 FEET

| | | |
|-------------------------|--------|-------|
| Net Length of Roadway | 13.725 | Miles |
| Net Length of Bridges | | Miles |
| Net Length of Project | 13.725 | Miles |
| Length of Exceptions | 0.263 | Miles |
| Gross Length of Project | 13.988 | Miles |

| | |
|---|-----------------|
| Sta. 1736+89.4 = Sta. 1736+41.1 = +48.3' | TOTAL = +471.2' |
| Sta. 1873+30.2 = Sta. 1868+12.2 = +48.0' | |
| Sta. 2066+85.2 = Sta. 2067+29.2 = -44.0' | |
| Sta. 2238+100.2 = Sta. 2238+50.2 = -50.0' | |

Note: All workmanship and materials on this project conform with South Carolina State Highway Department standard specifications for highway construction dated 1973.

R.H.A.

3 DAYS BEFORE DIGGING IN
SOUTH CAROLINA
CALL 1-800-922-0983
PALMETTO UTILITY LOCATION SERVICE

| CONVENTIONAL SIGNS | |
|-------------------------------------|------------------------------|
| State Line | Triangular Signs |
| County Line | Power Poles |
| City or Town Line | Telephone or Telegraph Poles |
| Property Line | Marsh |
| Fence | Trees |
| Retaining Wall | Brush |
| Existing Road | Stumps |
| Line and ROW Lines of Proposed Road | Buildings |
| Railroad | Bridge |
| Level of Embankment | Concrete Box Culvert |
| Guard Rail | Pipe Culvert |
| Point of Intersection (P.I.) | Drop Inlet and Culvert |
| | Hub on Center Line |

| LEGEND | |
|------------------|-----|
| PROPOSED PROJECT | --- |
| OTHER ROADS | --- |

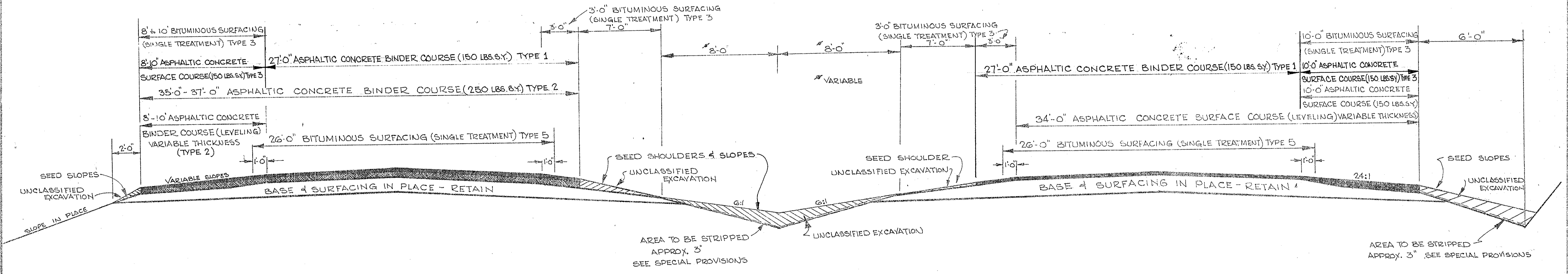
RECOMMENDED BY:
NK York 4/2/82
Rural ENGINEER / DATE
SUBMITTED BY:
John Jaccan 4-2-82
ROAD ENGINEER/DESIGN / DATE
APPROVED BY:
[Signature] 4-7-82
STATE HIGHWAY ENGINEER / DATE

DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
APPROVED FOR DIVISION ADMINISTRATOR
BY: _____ DATE _____
DISTRICT ENGINEER

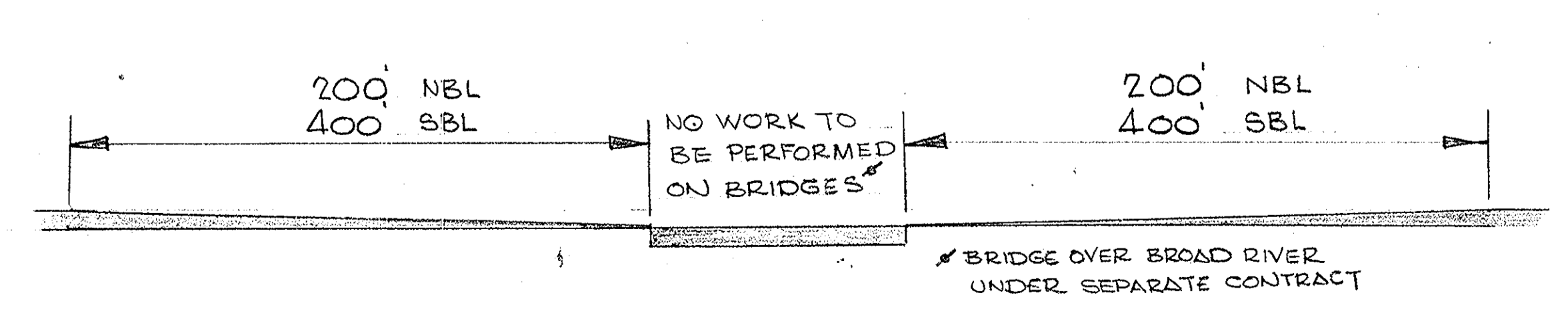
| FED. ROAD DIV. NO. | STATE | COUNTY | FILE | PROJECT | ROUTE | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|----------|-------------|-------|-----------|--------------|
| 3 | SC | CHEROKEE | 11,530.1 | 1R-85-3(65) | 1-85 | 2 | 60 |

SBL

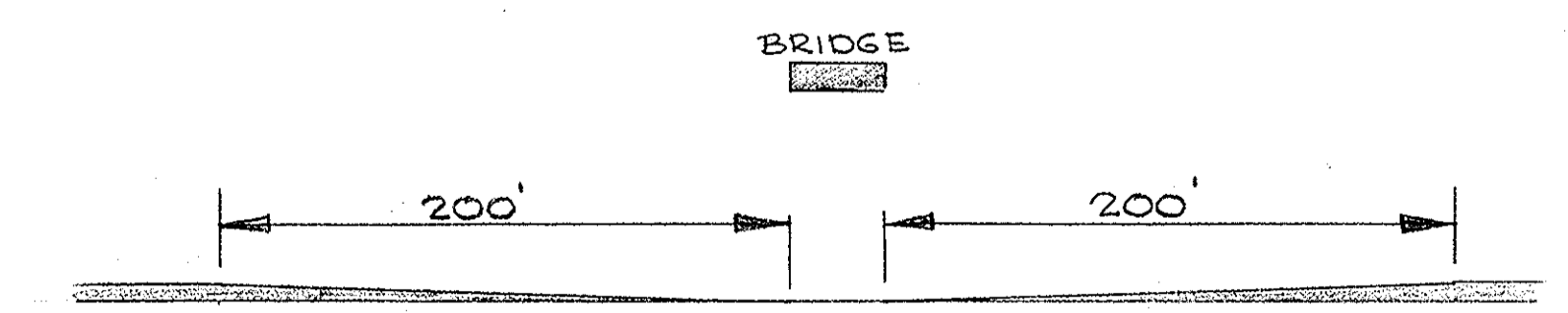
NBL



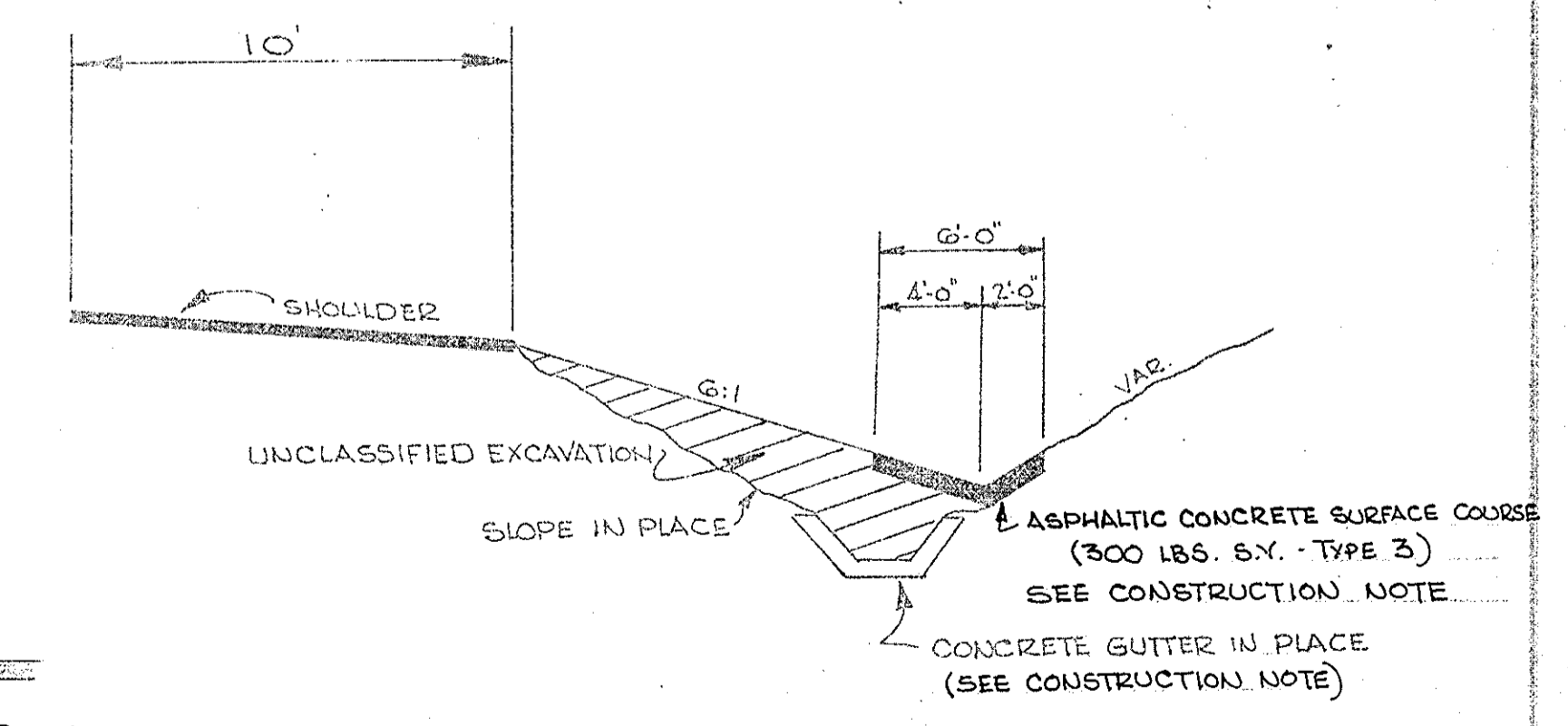
USE THIS SECTION STA. 1531+50.0 TO STA. 2265+33.7



METHOD FOR TAPERING TREATMENT AT BRIDGES

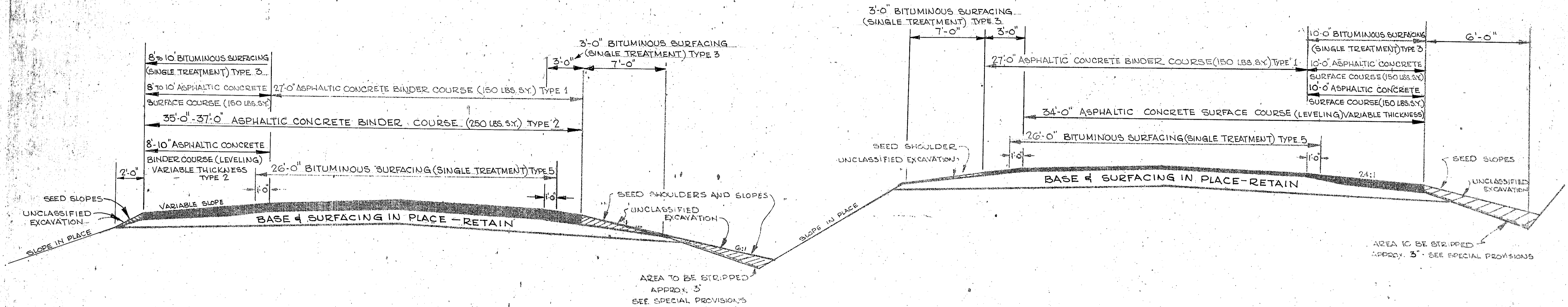


METHOD FOR TAPERING BINDER TYPE 2 AT UNDERPASS ON ROAD S-99 (SOUTHBOUND LANE ONLY)

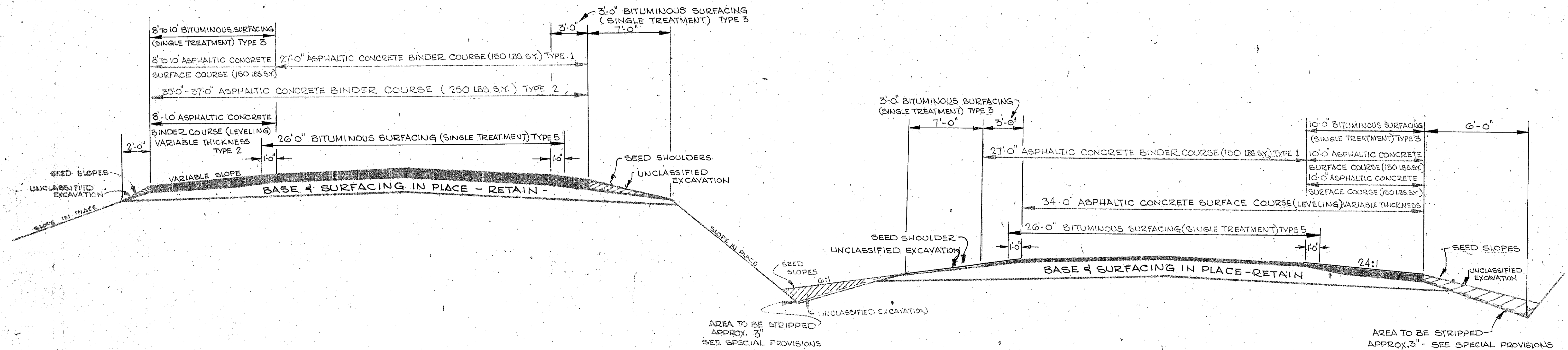


NOT TO SCALE

| FED. ROAD DIV. NO. | STATE | COUNTY | FILE | PROJECT | ROUTE | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|----------|------------|-------|-----------|--------------|
| 3 | SC. | CHEROKEE | 11,530.1 | 12-85-3(6) | I-85 | 3 | 60 |

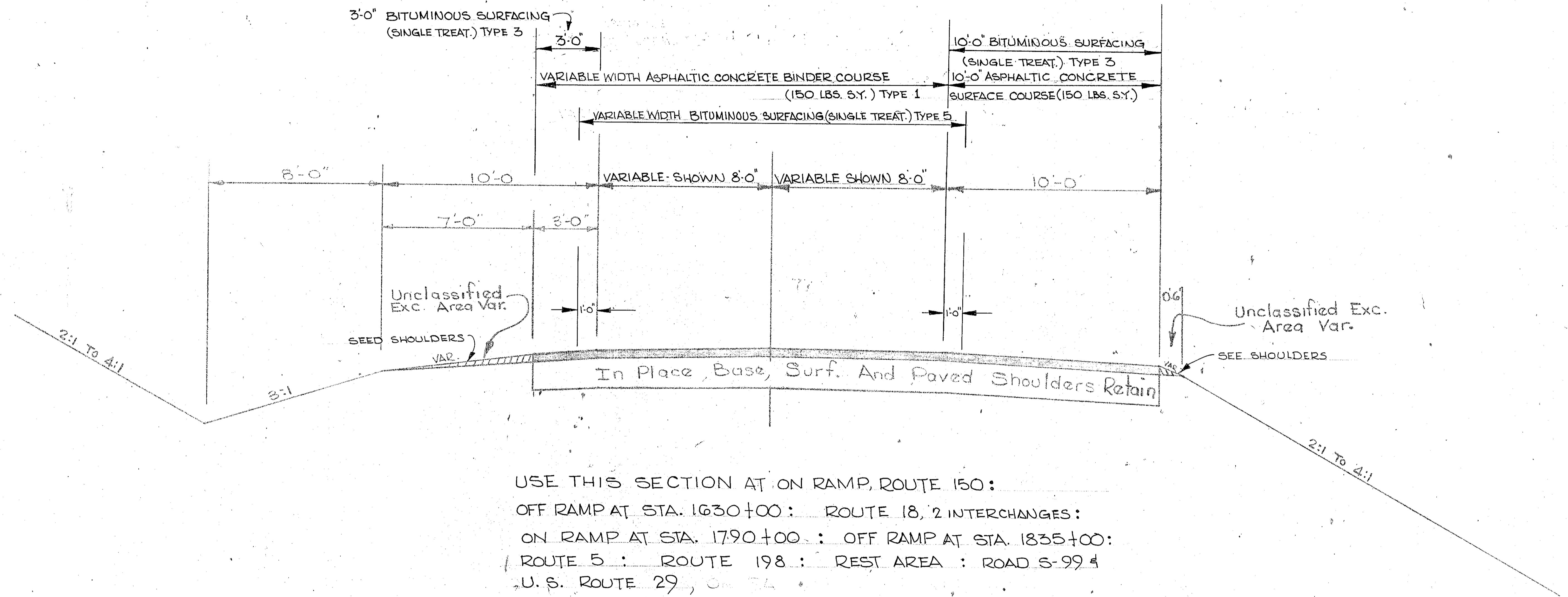


USE THIS SECTION WHERE APPLICABLE

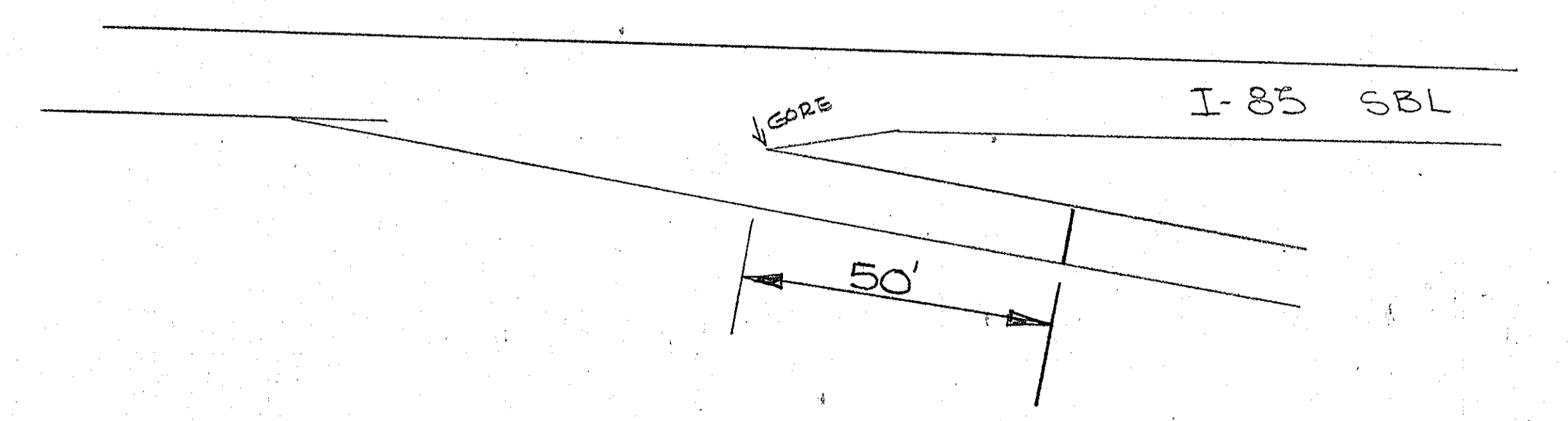


USE THIS SECTION WHERE APPLICABLE

| FED. ROAD DIST. NO. | STATE | COUNTY | FILE NO. | PROJ. NO. | RT. No. | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------|----------|-----------|---------|-----------|--------------|
| 3 | SC | CHEROKEE | 11.530.1 | 12-853(6) | 1-85 | 4 | 60 |



USE THIS SECTION AT ON RAMP, ROUTE 150:
 OFF RAMP AT STA. 1630+00: ROUTE 18, 2 INTERCHANGES:
 ON RAMP AT STA. 1790+00: OFF RAMP AT STA. 1835+00:
 ROUTE 5: ROUTE 198: REST AREA: ROAD S-99 &
 U.S. ROUTE 29, 0-24



END ASPHALTIC CONCRETE BINDER COURSE (TYPE 2)
 50' BEYOND GORE ON SOUTHBOUND LANE

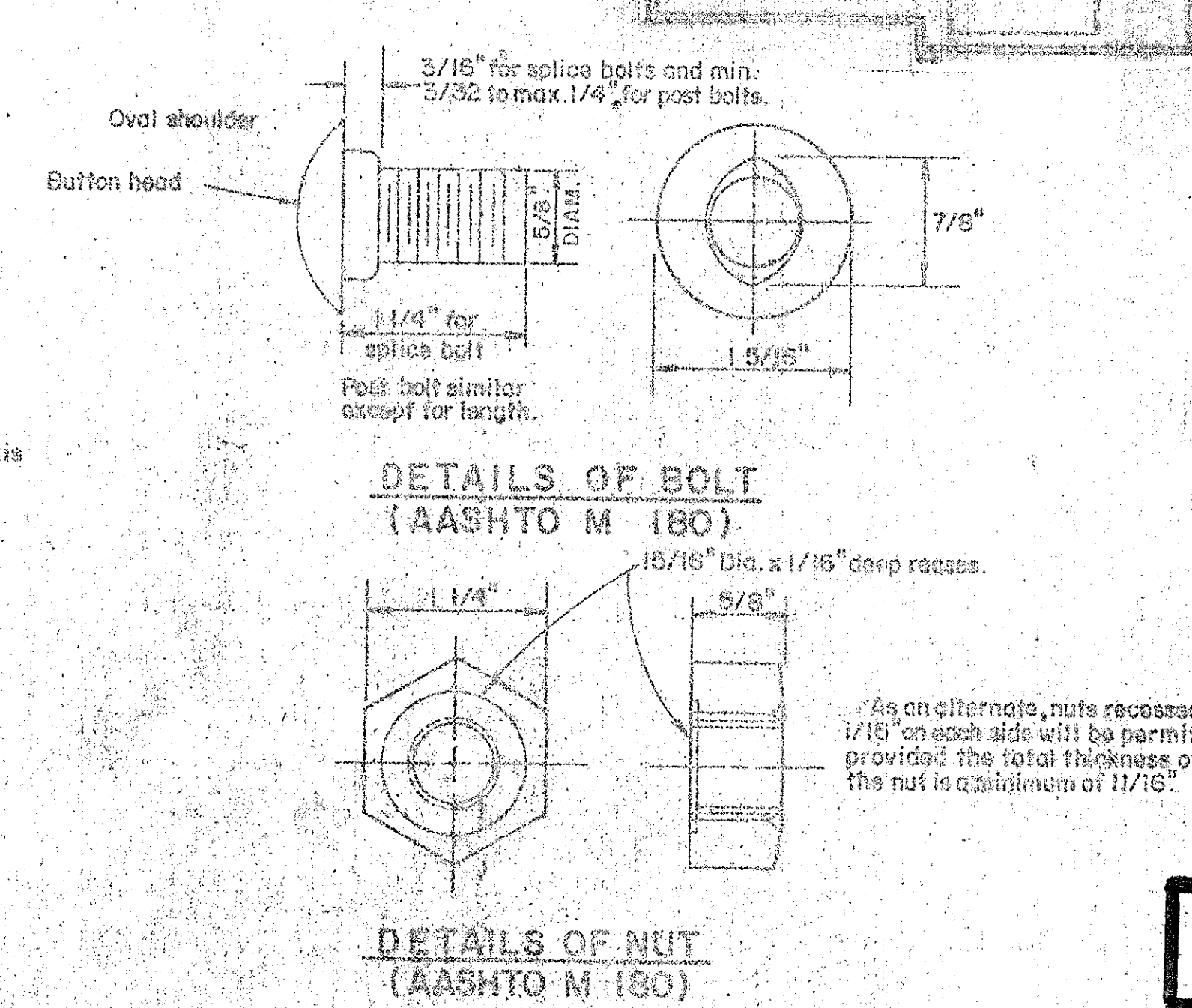
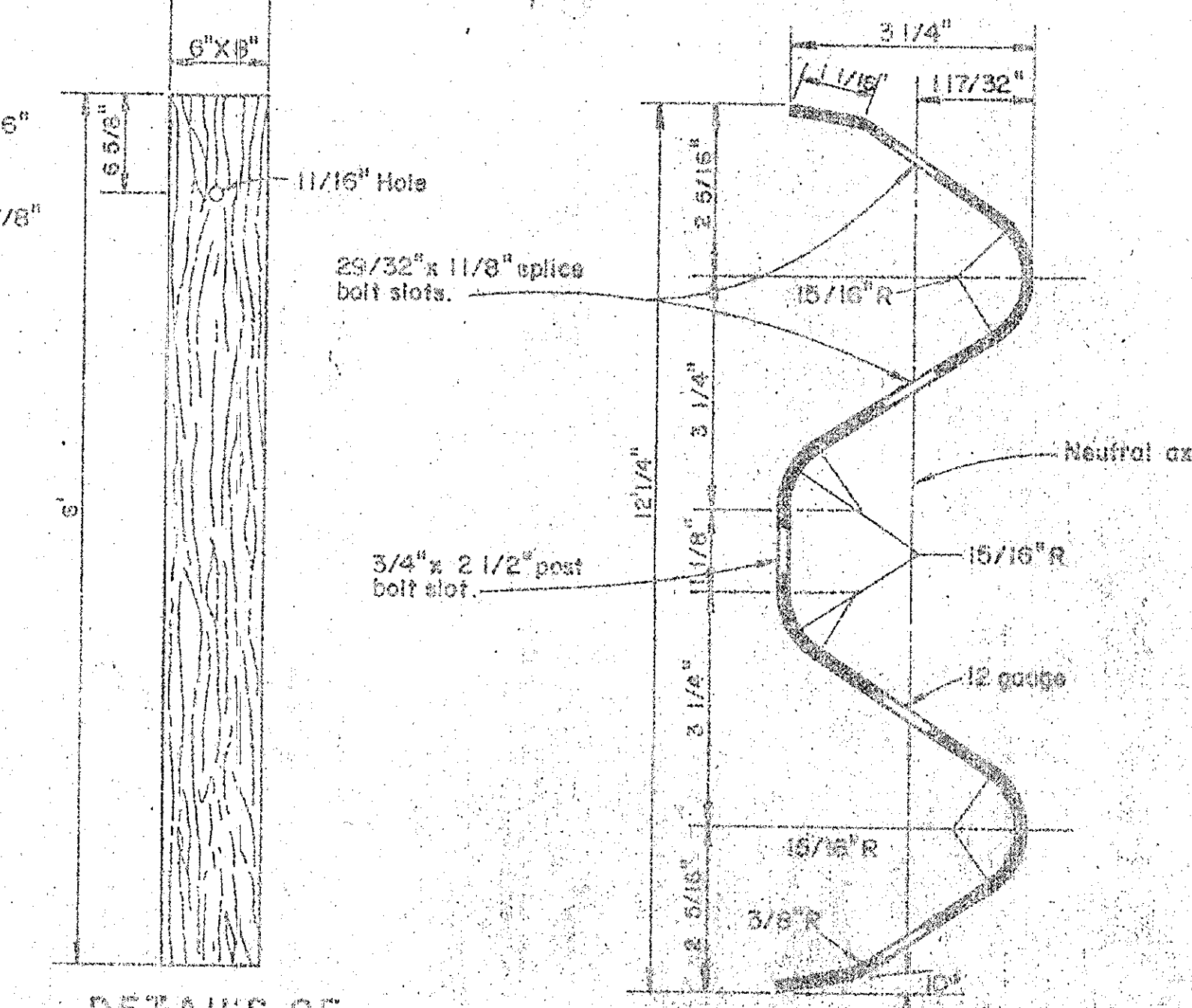
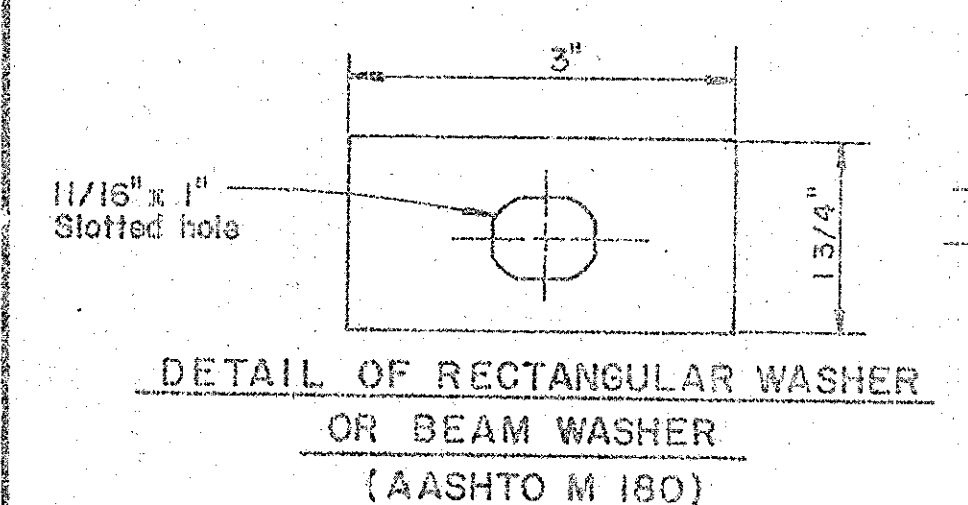
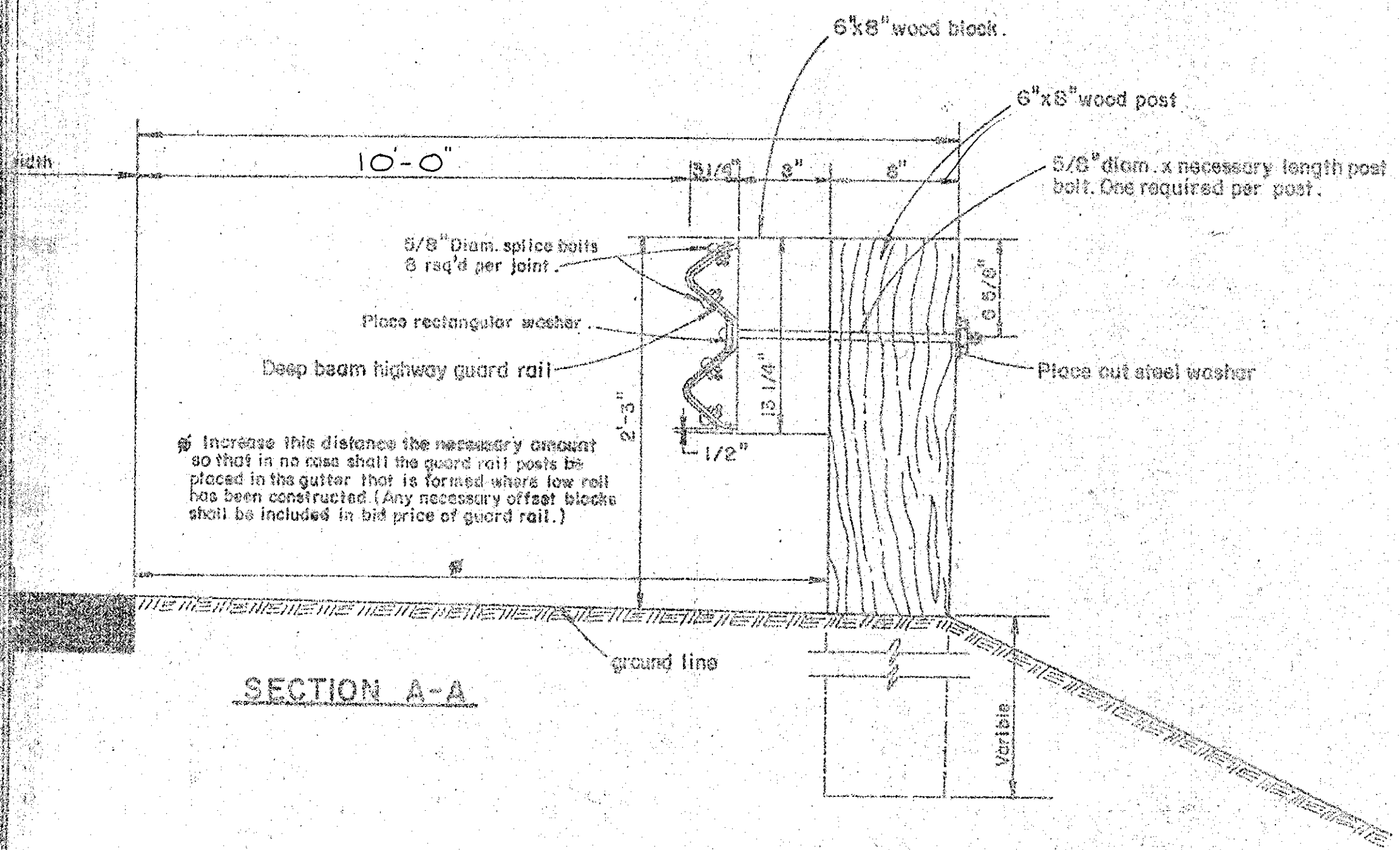
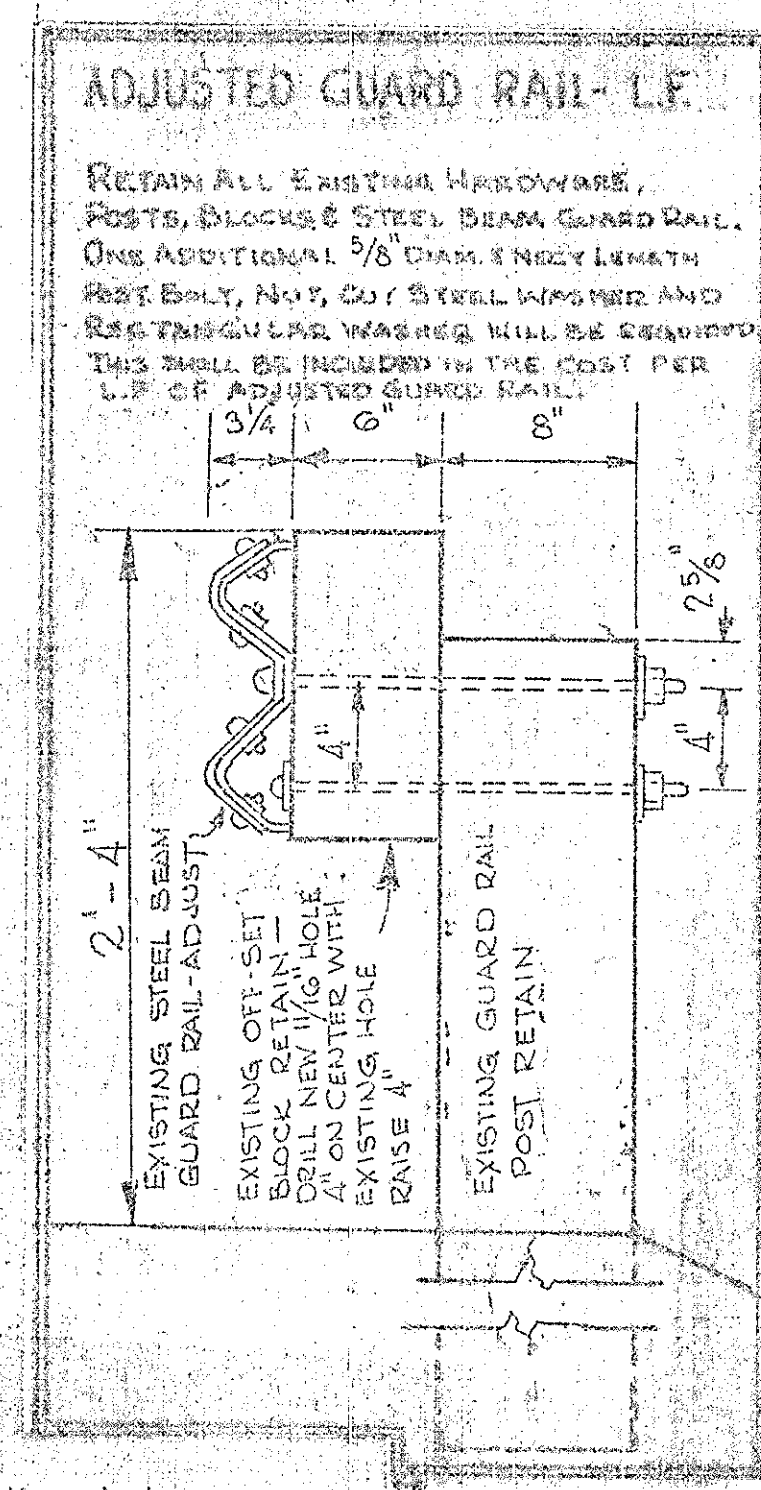
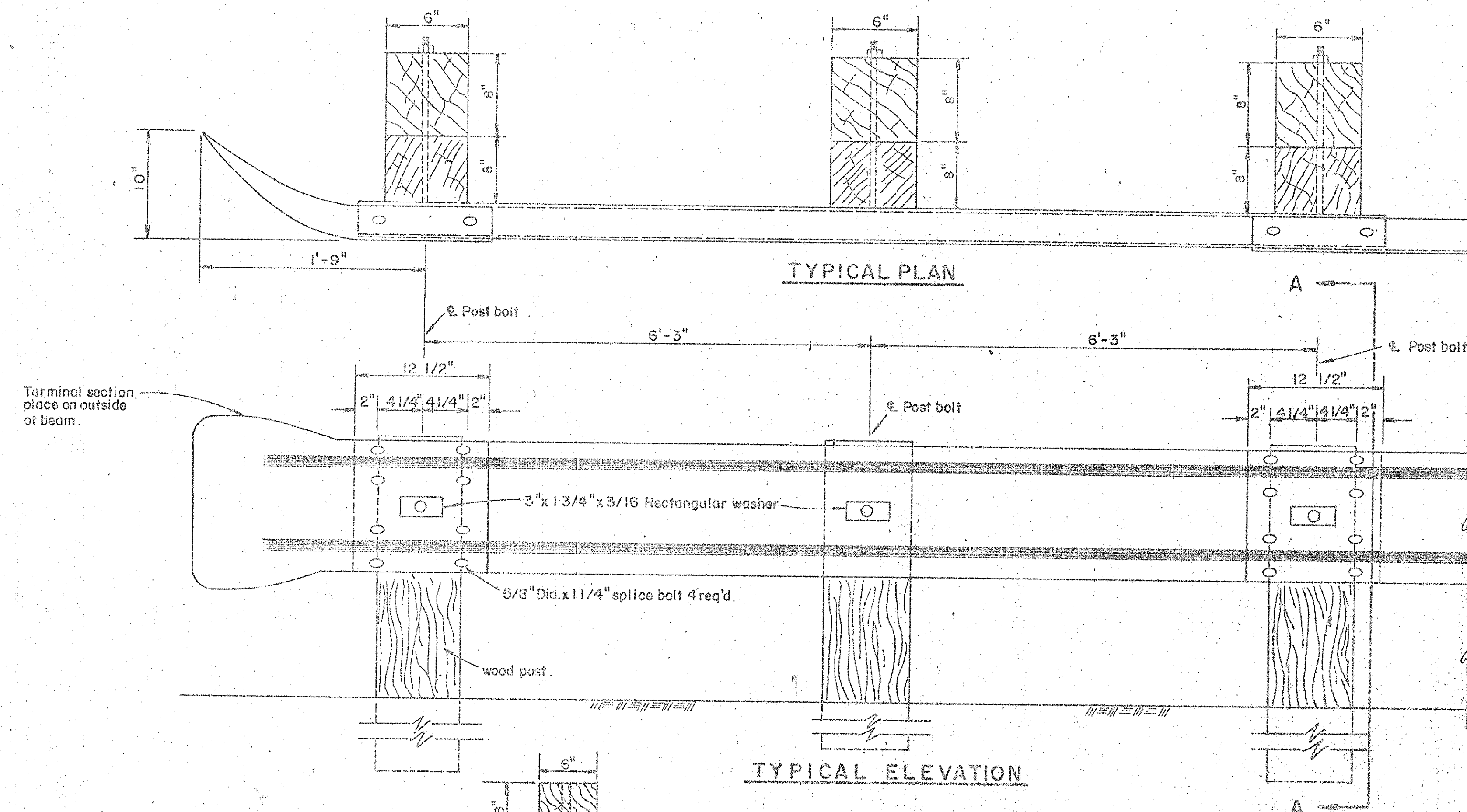
GENERAL NOTES:

Steel beam guard rail shall comply with the requirements of AASHTO M.180 and shall be Class "A" (O.105) types 1 or 2.

Posts shall be 6"x8"x6'-0" and blocks shall be 6"x8"x13 1/4" meeting the requirements of sections 706 and 805 of the standard specifications the variance in the size of the posts shall be no greater than 1/4" and the post shall be 3'-9" in the ground and extended 2'-3" above the edge of the shoulder. Post and blocks shall be treated in accordance with section 707 of the standard specifications with 12 lbs./cu. ft. retention of creosote or 0.6 lbs./cu. ft. of pentachlorophenol or 0.6 lbs./cu. ft. of chromated copper arsenate. Combinations of preservatives will not be used except when permitted by the engineer.

Where laps in rail are necessary they shall be placed in the same direction as the flow of traffic. Rail elements shall conform to the requirements of section 805 of the departments standard specification. If desirable, guard rail sections may be furnished and installed in 25'-0" sections instead of 12'-6" sections as shown on plans. Where beam rail of the guard rail is to be erected on the curves of 150 ft. radius or less, the plates shall be pre-curved in the shop to fit the required radius.

The unit price bid for guard rail shall include all cost of furnishing and placing post, blocks, and also of furnishing, galvanizing and placing the metal guard rail (including post bolts, nuts, and washers necessary for splices and for fastening rail to posts) as called for on plans. The anchor section shall be paid for under separate bid item. (Pay per each installed) This item to include all cost of furnishing and placing conc. anchor, anchor plate, swaged fitting and stud with cable, galv. rod, 1/2"x3"x10 steel plate 3w/ 17x4'-6" long, cable clips, and other hardware necessary for cable anchor. (See 805 std. specification.)



Index for Guard Rail Standards

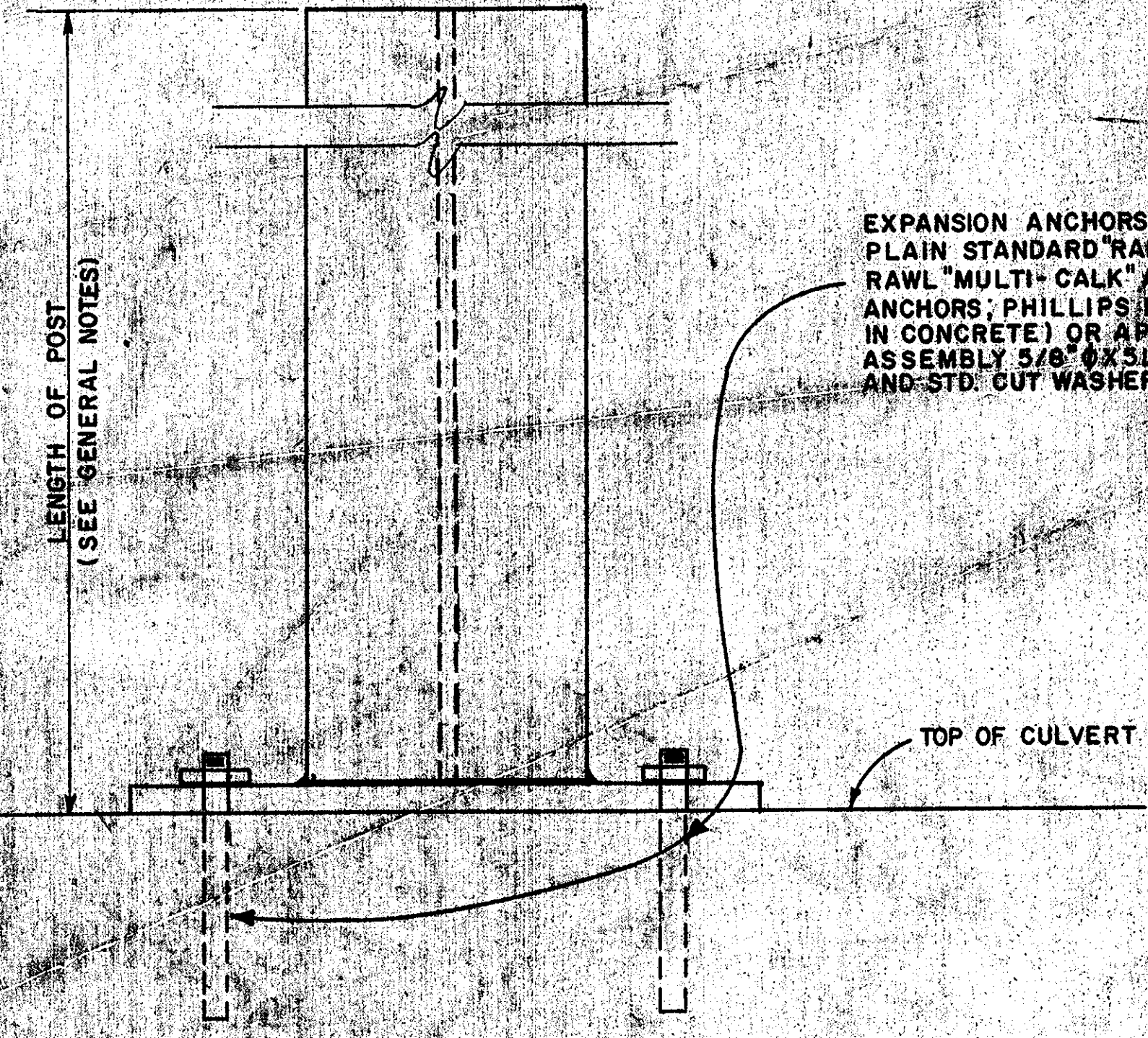
- GR-1 Hardware sheet showing - Guard rail, post, washers, nuts and bolts.
- GR-2-B Detail showing anchor section.
- GR-2A Detail showing expansion joint.
- GR-3 Detail showing guard rail at bridges and offsets for approach anchor sections.
- GR-4 Detail showing guard rail around bridge abutments in median.
- GR-5 Detail showing special end shoe to fit anchor lug at bridge ends.
- GR-6 Detail showing guard rail tie to bridge without anchor lug at bridge ends.
- GR-7 Detail showing conc. anchor block in cut slope and standard guard rail at frontage roads or secondary roads at bridges and culverts.
- GR-1-A Detail showing metal guard rail post and base plate.

NOTE

Note: Use 6"x8" square wooden post & offset blocks on all Interstate project.

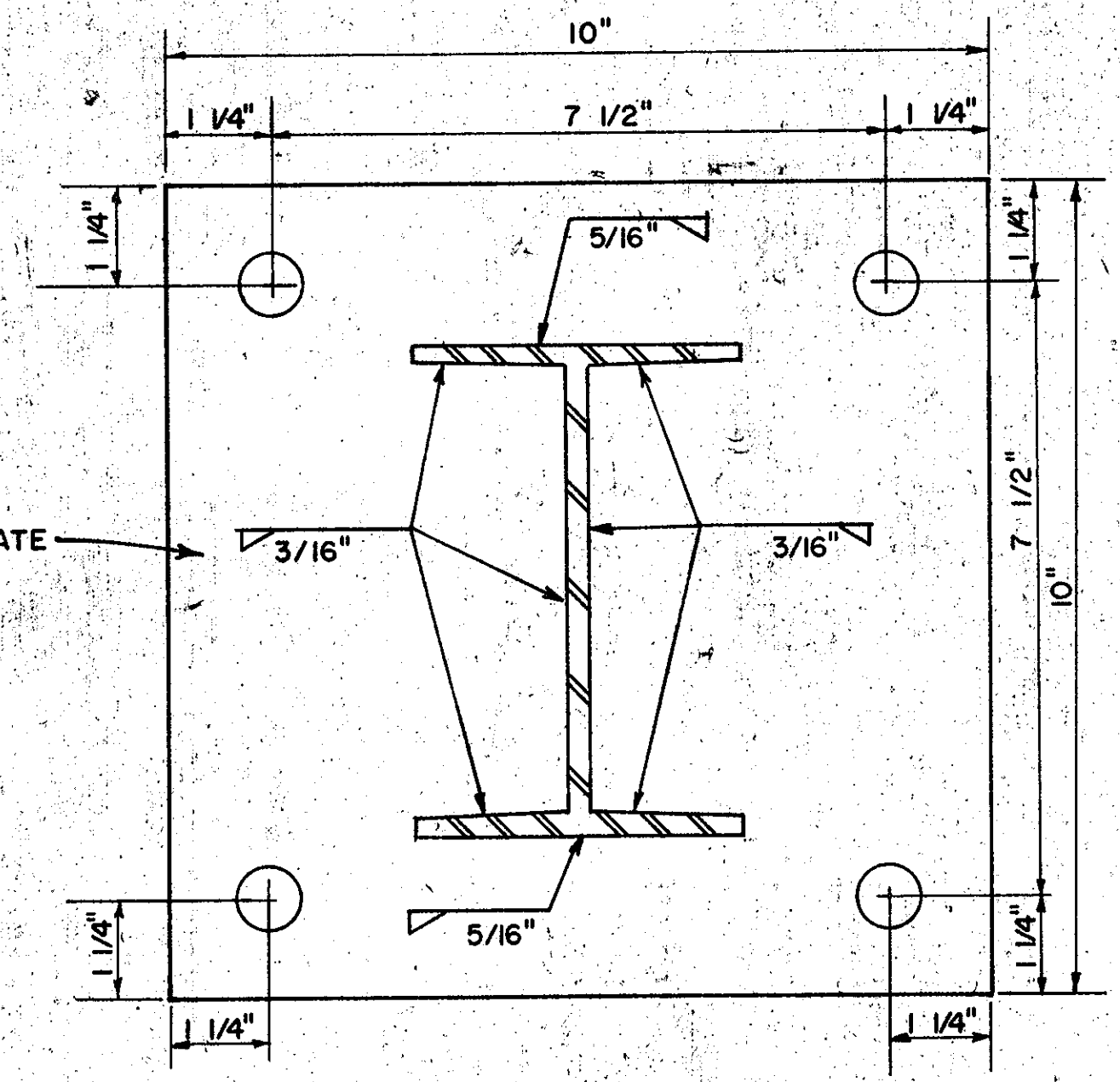
| DATE | REVISIONS | BY |
|----------|---|--------|
| | Rectangular washer | |
| 10/10/68 | Rail element | J.D.M. |
| | Post or splice bolt | |
| 3/6/69 | Size of post and spacer block | |
| | Terminal section (Law const. office) | J.D.M. |
| 12/15/69 | Guard rail term anchor section | J.D.M. |
| 2/9/70 | Cable end anchor or sec. (B.P.R.) | J.D.M. |
| 11/2/72 | Rect. washer or beam washer | J.S.S. |
| 6/23/74 | Rev. post and spacer block | J.D.M. |
| 1/18/82 | Rev. post & block (Letter 1-11-82 S.H.O.) | J.D.M. |

STANDARD METAL GUARD RAIL



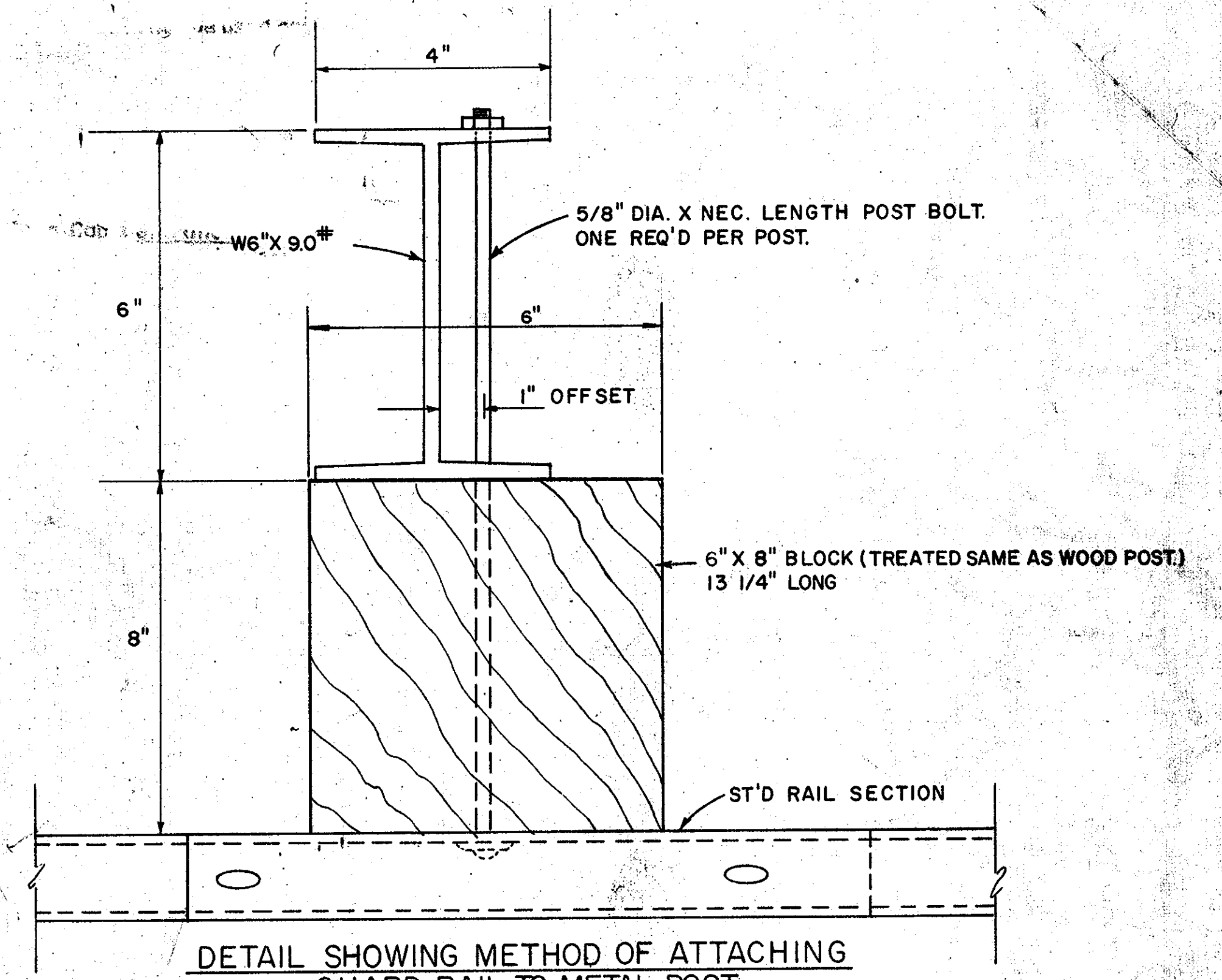
DETAIL OF METAL POST

EXPANSION ANCHORS MAY BE EITHER PLAIN STANDARD "RAM" ANCHOR, TYPE IA; RAWL "MULTI-CALK" ANCHOR; "STAR SLUG" ANCHORS; PHILLIPS RED HEAD R-58 (2 3/8" IN CONCRETE) OR APPROVED EQUAL. BOLT ASSEMBLY 5/8" X 3 1/2" BOLT WITH HEX. HEAD AND STD. CUT WASHER.



10" X 10" X 5/8" BASE PLATE (3/4" Ø HOLES)

DETAIL OF BASE PLATE



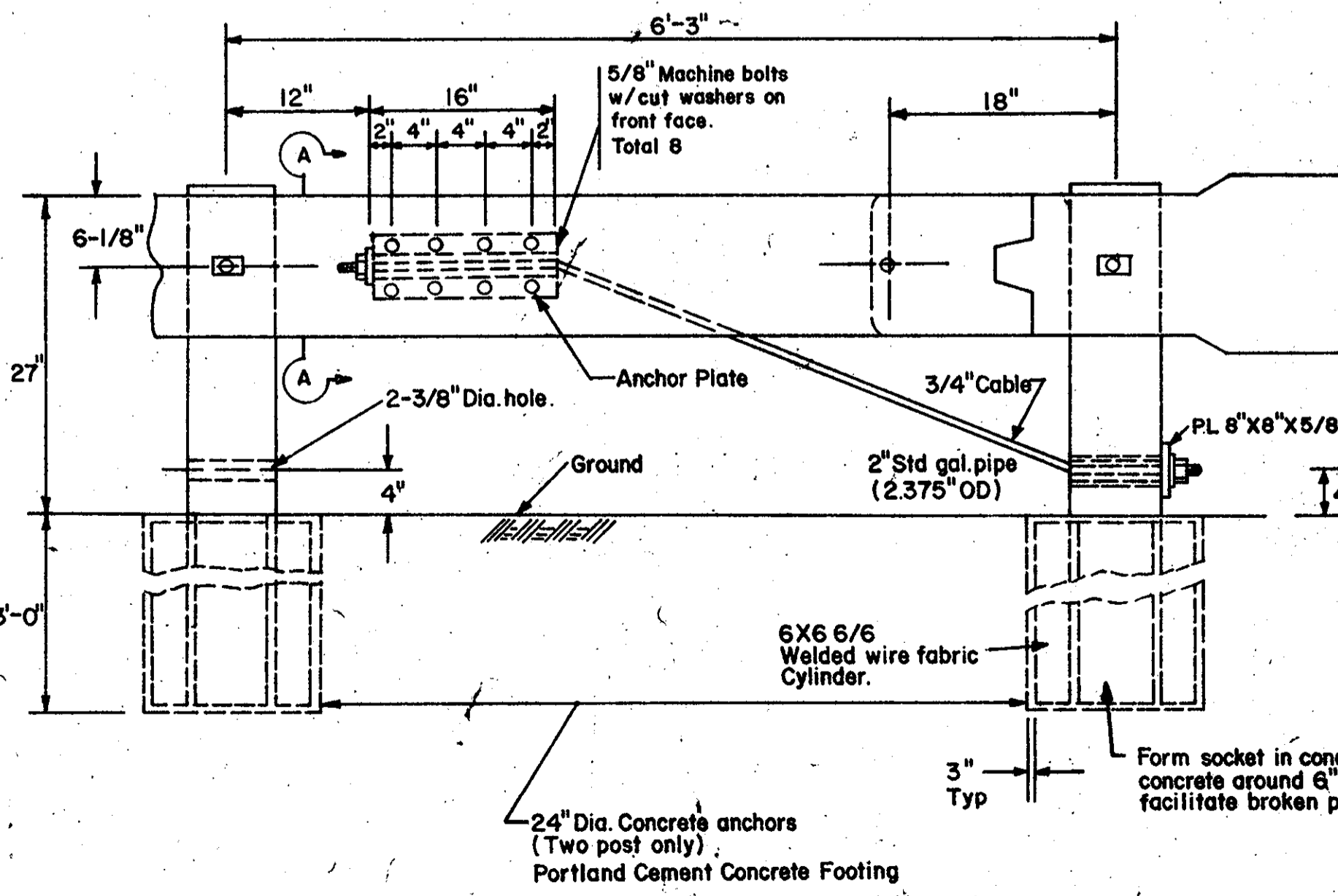
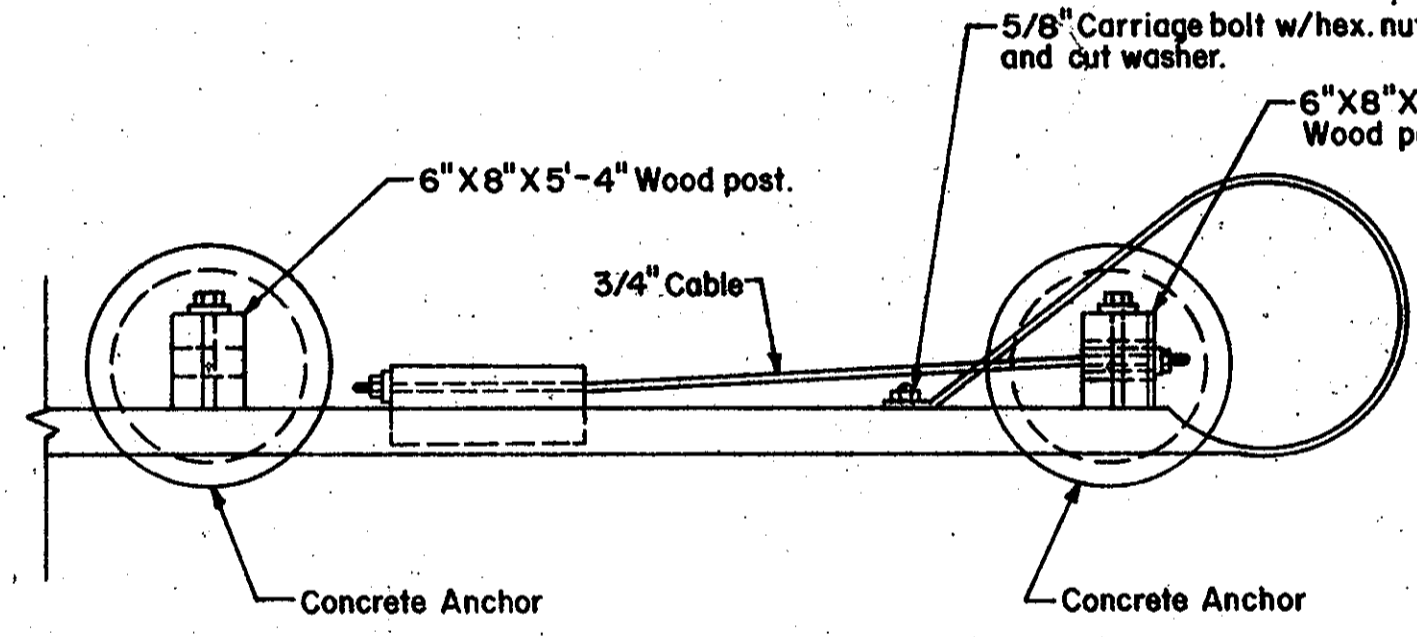
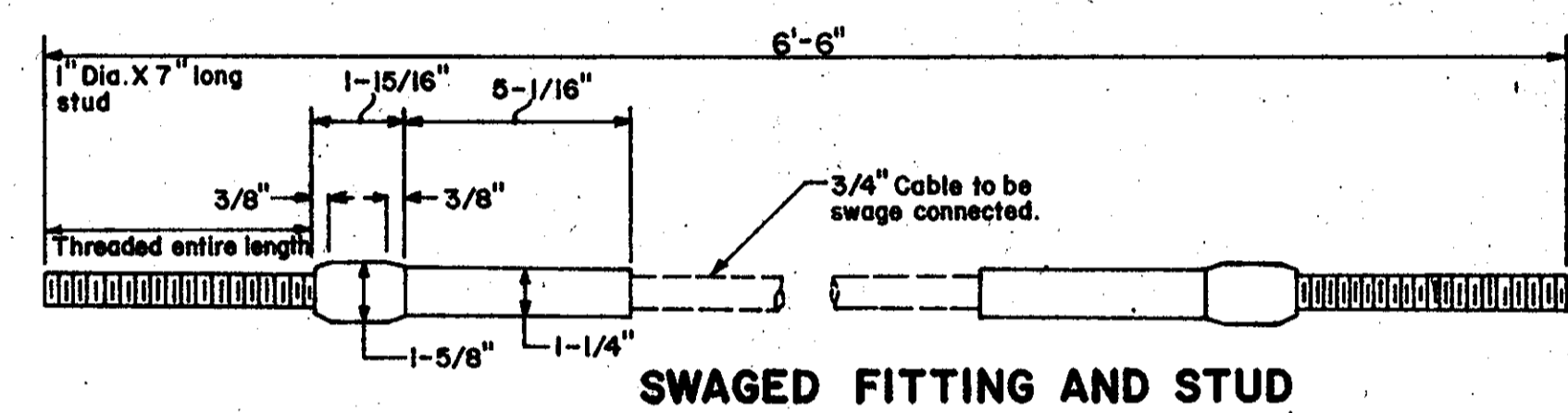
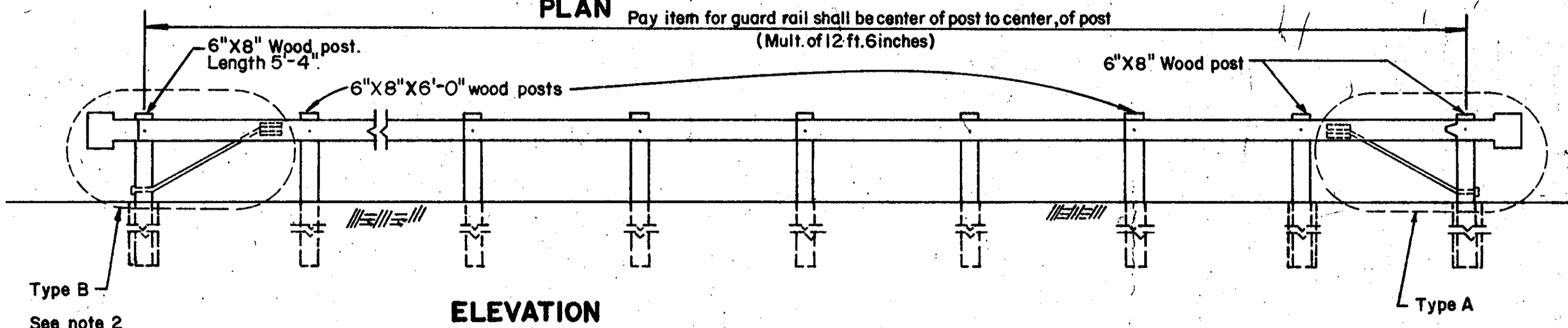
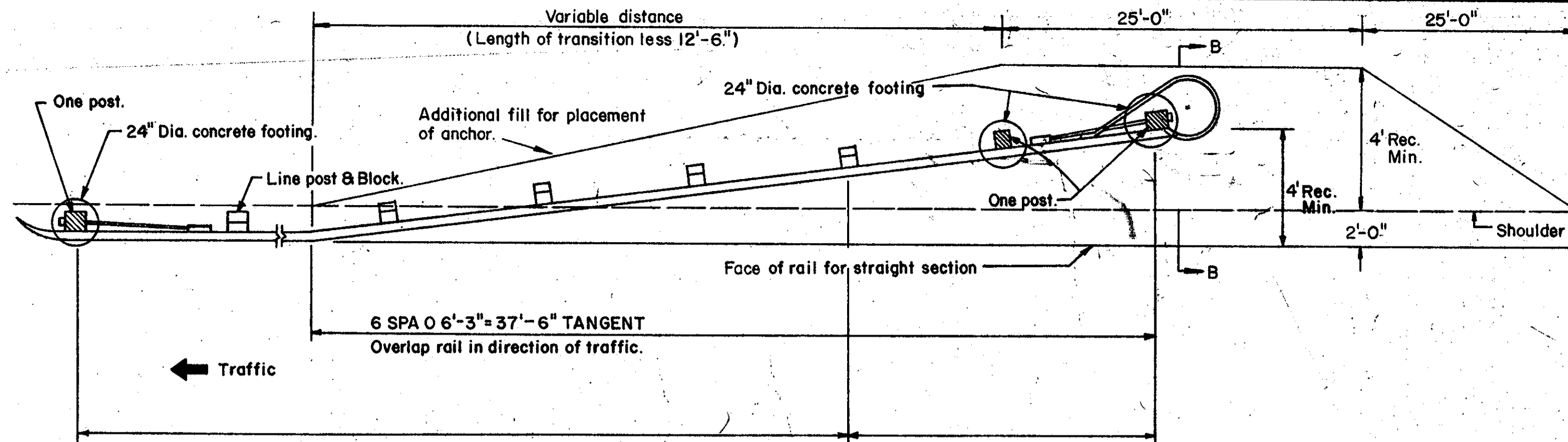
DETAIL SHOWING METHOD OF ATTACHING GUARD RAIL TO METAL POST

GENERAL NOTES:

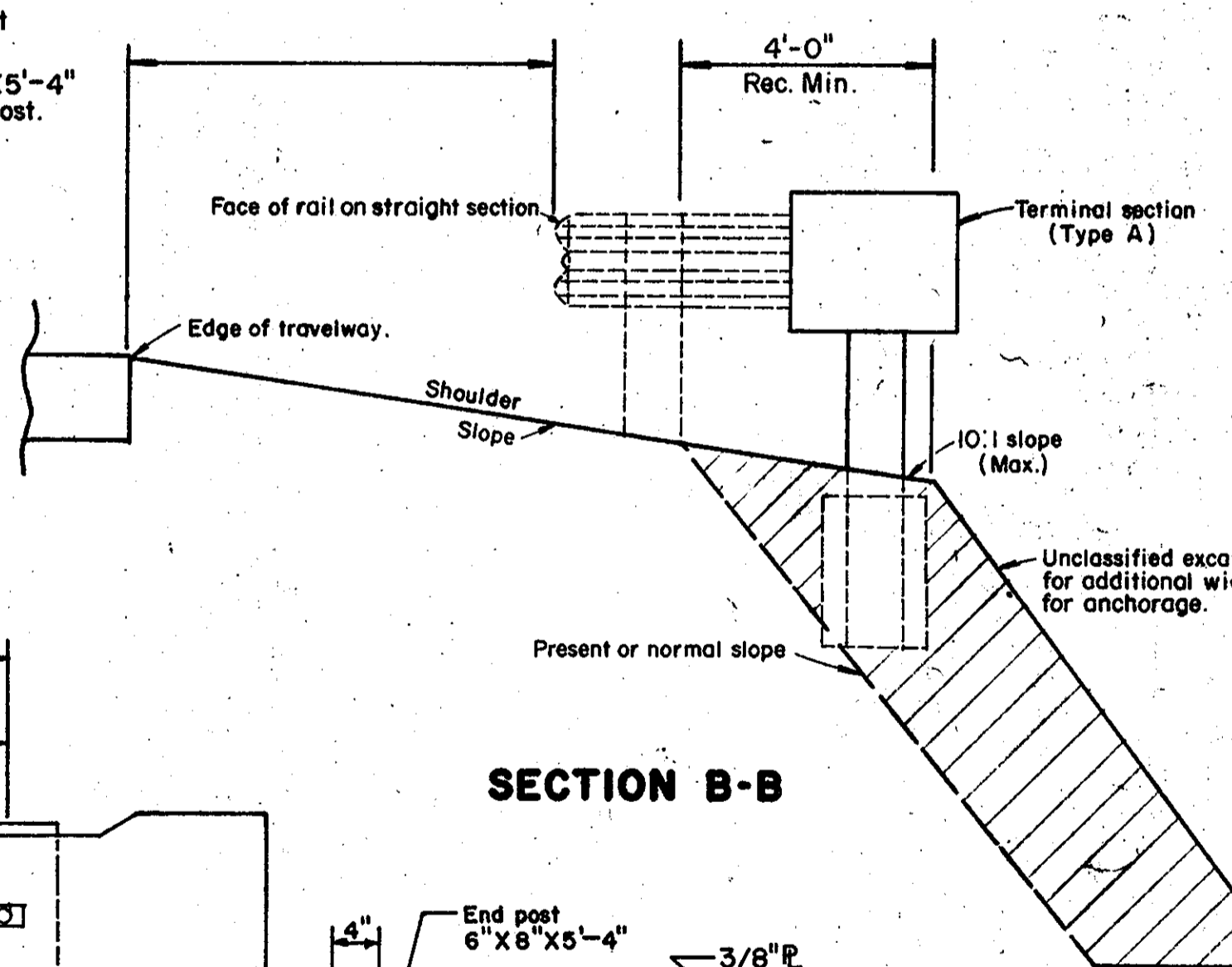
1. ALL MATERIAL SHALL BE GALVANIZED IN COMPLIANCE WITH A. S. T. M. DESIGNATION A 123-65.
2. ALL FABRICATION SHALL BE COMPLETE AND READY FOR ASSEMBLY BEFORE GALVANIZING.
3. CONSTRUCTION ENGINEER SHALL FURNISH THE CONTRACTOR THE NUMBER OF POST NEEDED AND THE LENGTH OF EACH POST.
4. PAYMENT FOR METAL POST WITH ALL ACCESSORIES SHALL BE INCLUDED IN BID PRICE FOR STEEL BEAM GUARD RAIL.

DETAIL OF METAL GUARD RAIL POST AND BASE PLATE TO BE ANCHORED TO CULVERTS AT LOCATIONS WHERE DEPTH IS INSUFFICIENT TO DRIVE WOOD POST GR.-1A

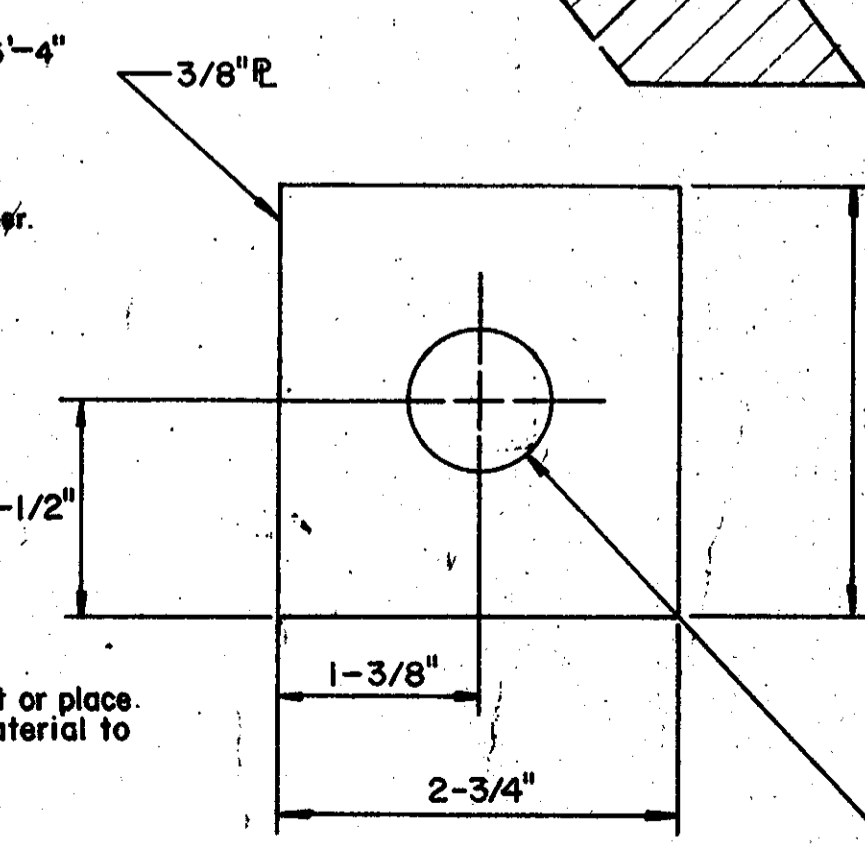
| FED. ROAD DIV. NO. | STATE | COUNTY | FILE NO. | PROJ. NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|----------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11,530-1 | 12-85-3(66) | 1-85 | 58 | 60 |



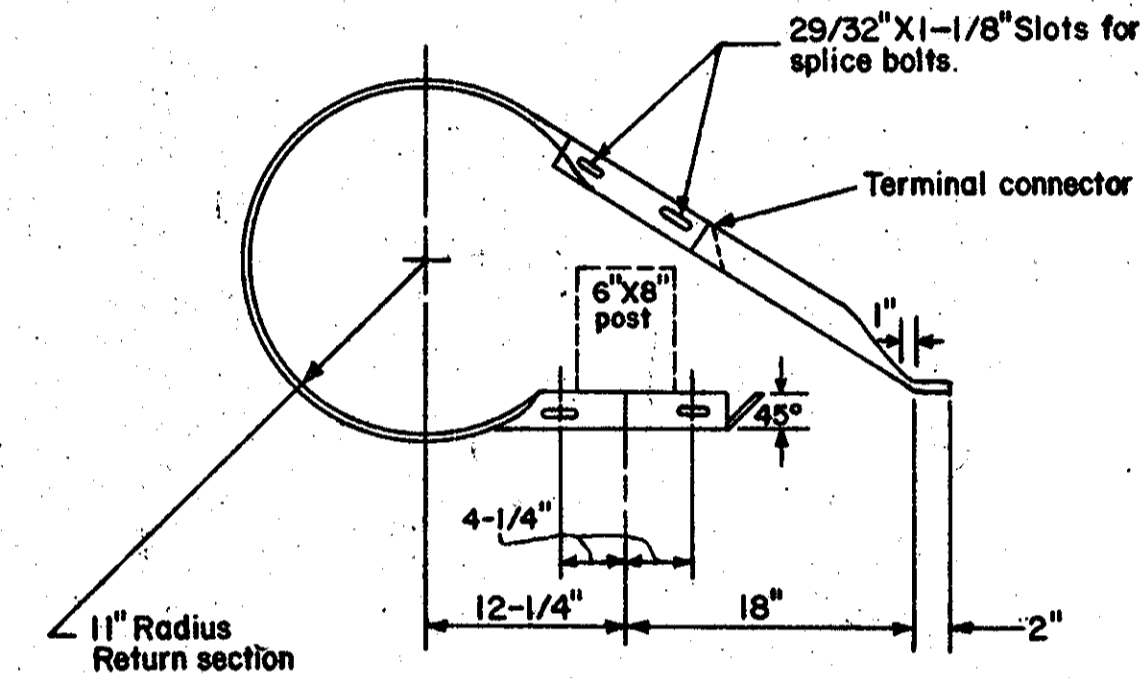
TYPE A TERMINAL



SECTION B-B

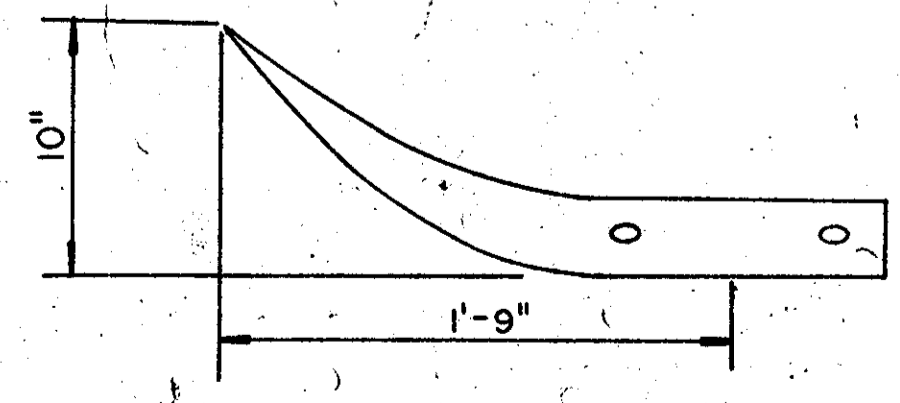


END PLATE

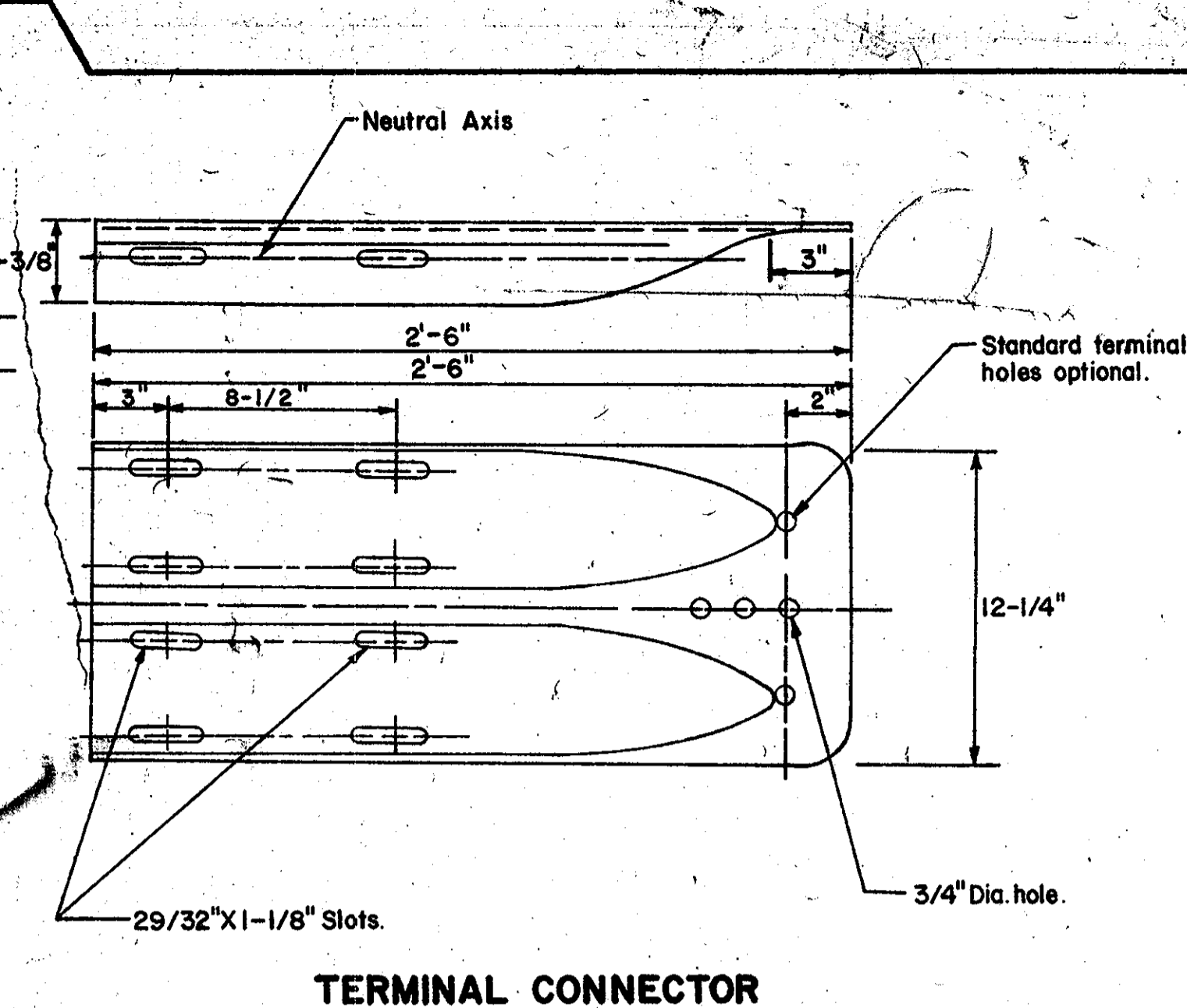


TERMINAL SECTION (TO BE USED WITH TYPE A)

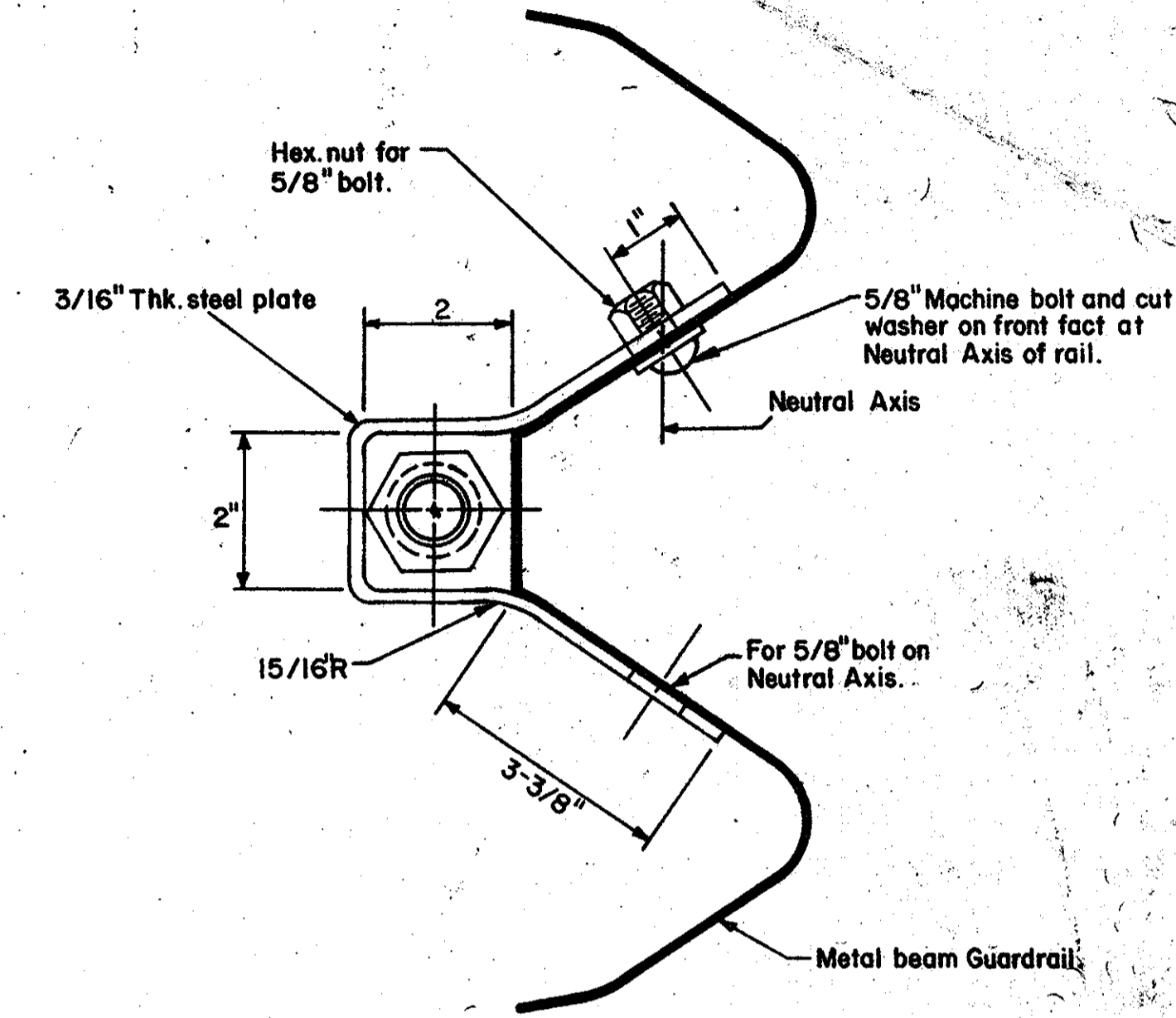
- NOTES**
- This terminal is used with wood post guardrail systems G4(1W), G4(2W), G4(1S) and G4(2S) as shown in AASHTO publication, "Guide for Selecting, Locating, and Designing Traffic Barriers, 1977.
 - For Trailing end of guardrail adjacent to one-way roadway omit terminal section, & use Standard Terminal section. Next to last post to be a line post.
 - Post to be centered in concrete footing.
 - Refer to latest edition of AASHTO-AGC-ARTBA publication, "A guide to Standardized Highway Barrier Rail Hardware," for standardized parts.
 - The Bid Item: End Anchor (TYPE A) each & End Anchor (TYPE B) each. End Anchor (TYPE A) each shall include the Terminal Section, Terminal Connector, Concrete Anchors for post, 3/4" cable with swaged fitting and stud, Anchor Plate and necessary hardware and labor to complete End Anchor. End Anchor (TYPE B) each shall include the Standard Terminal Section, Concrete Anchor for post, 3/4" cable with Swaged fitting and stud, Anchor Plate and necessary hardware and labor to complete End Anchor.



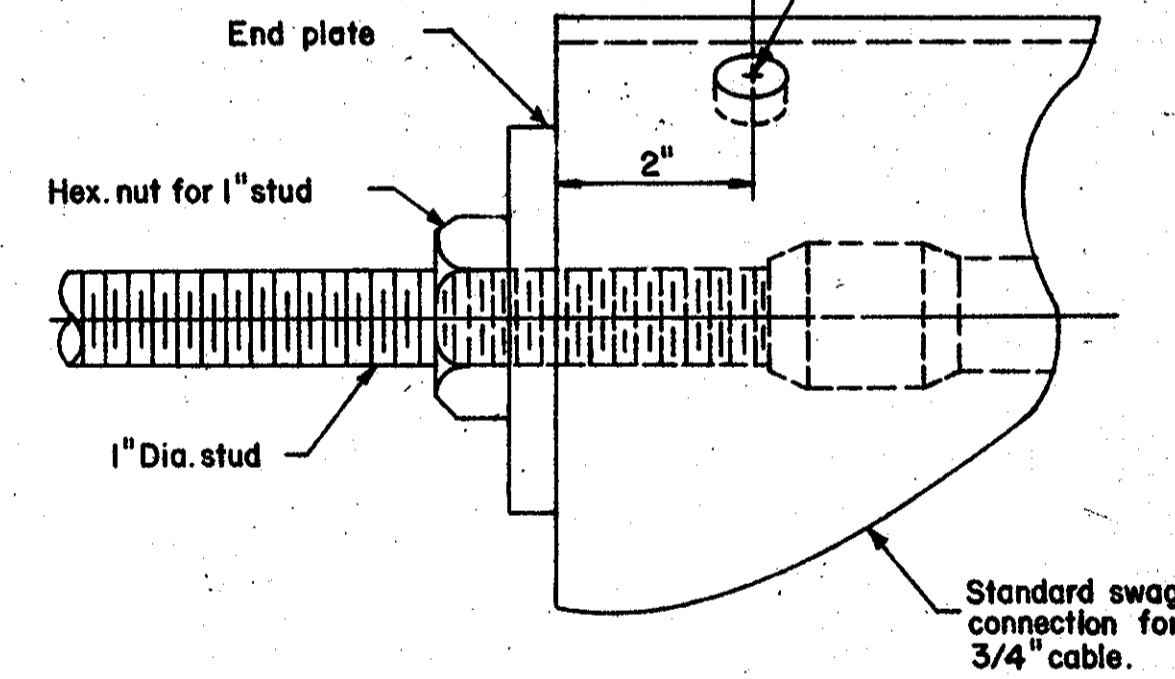
STANDARD TERMINAL SECTION (TO BE USED WITH TYPE B)



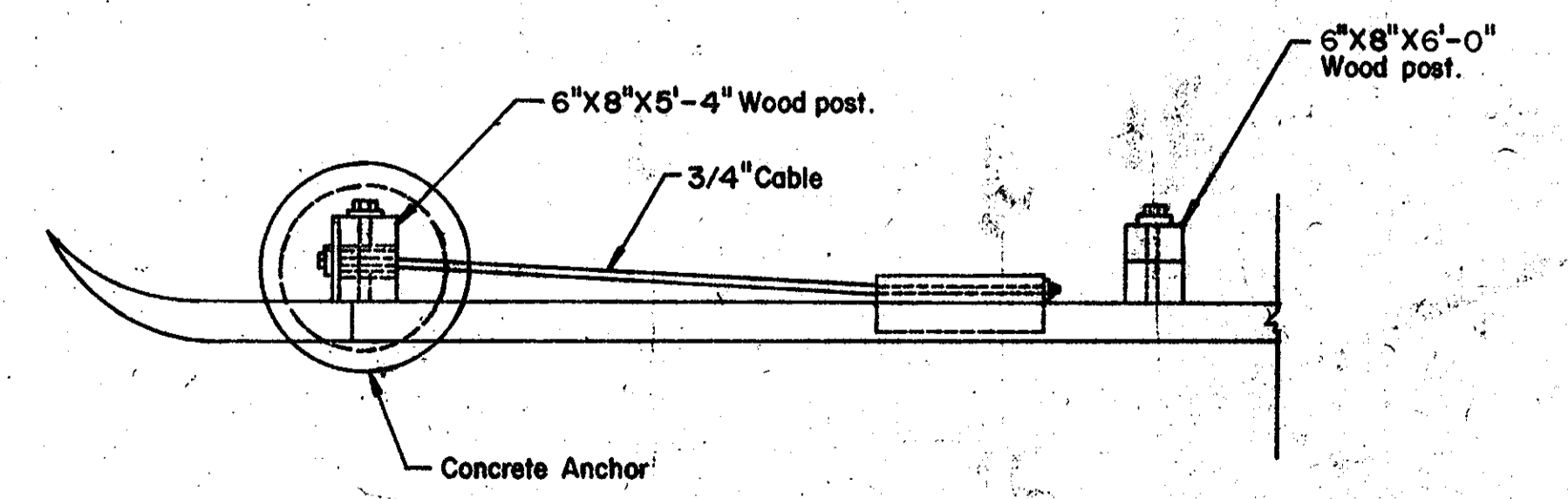
TERMINAL CONNECTOR



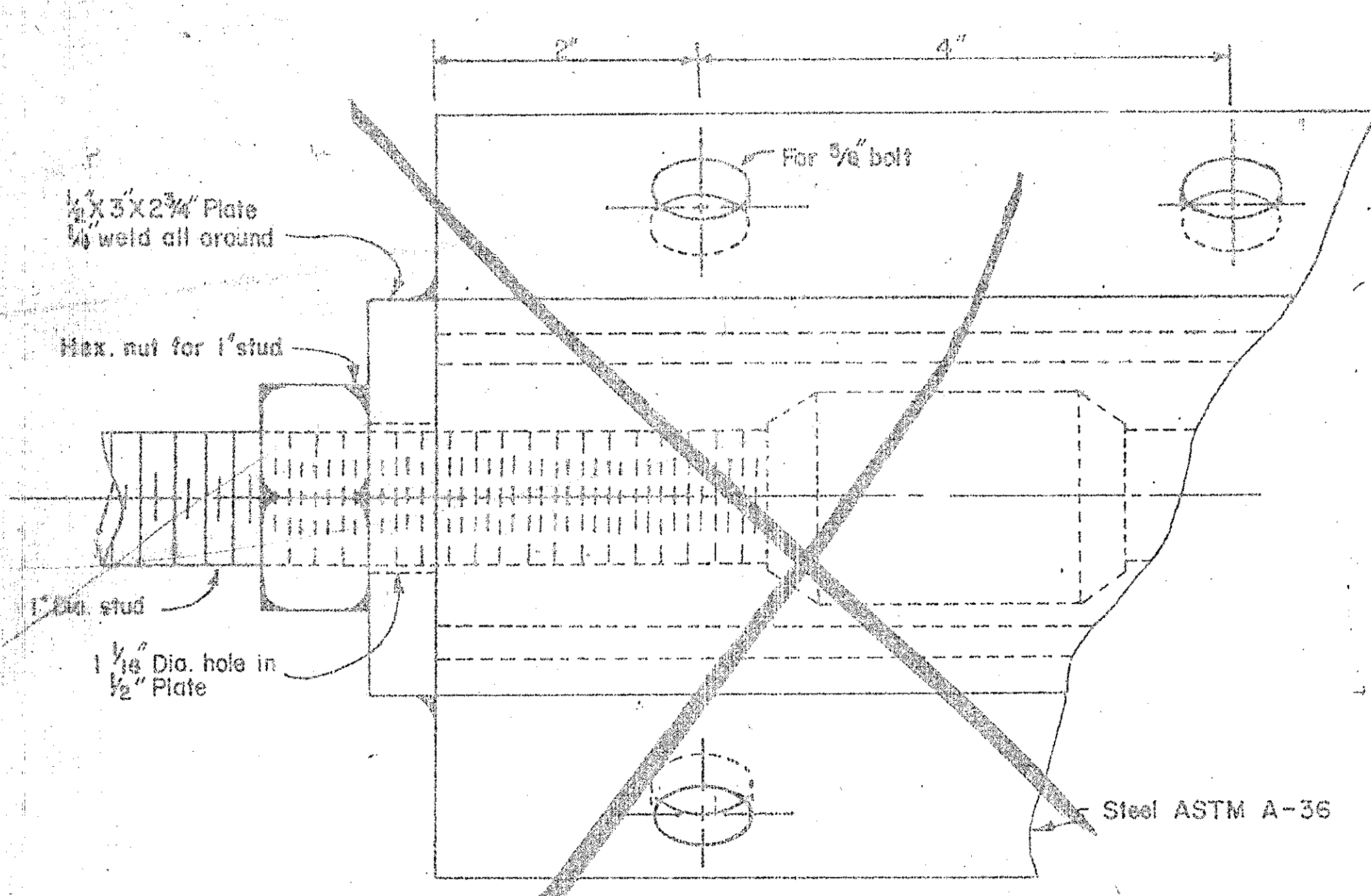
SECTION A-A (END PLATE REMOVED)



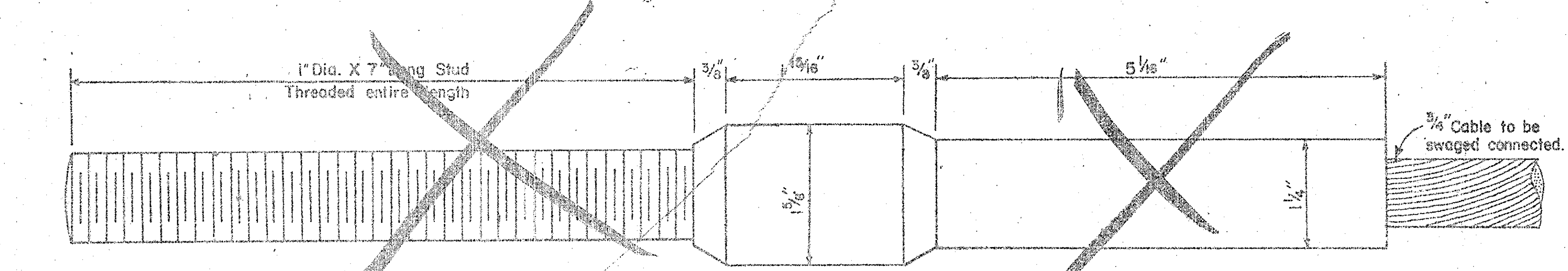
ANCHOR PLATE DETAILS



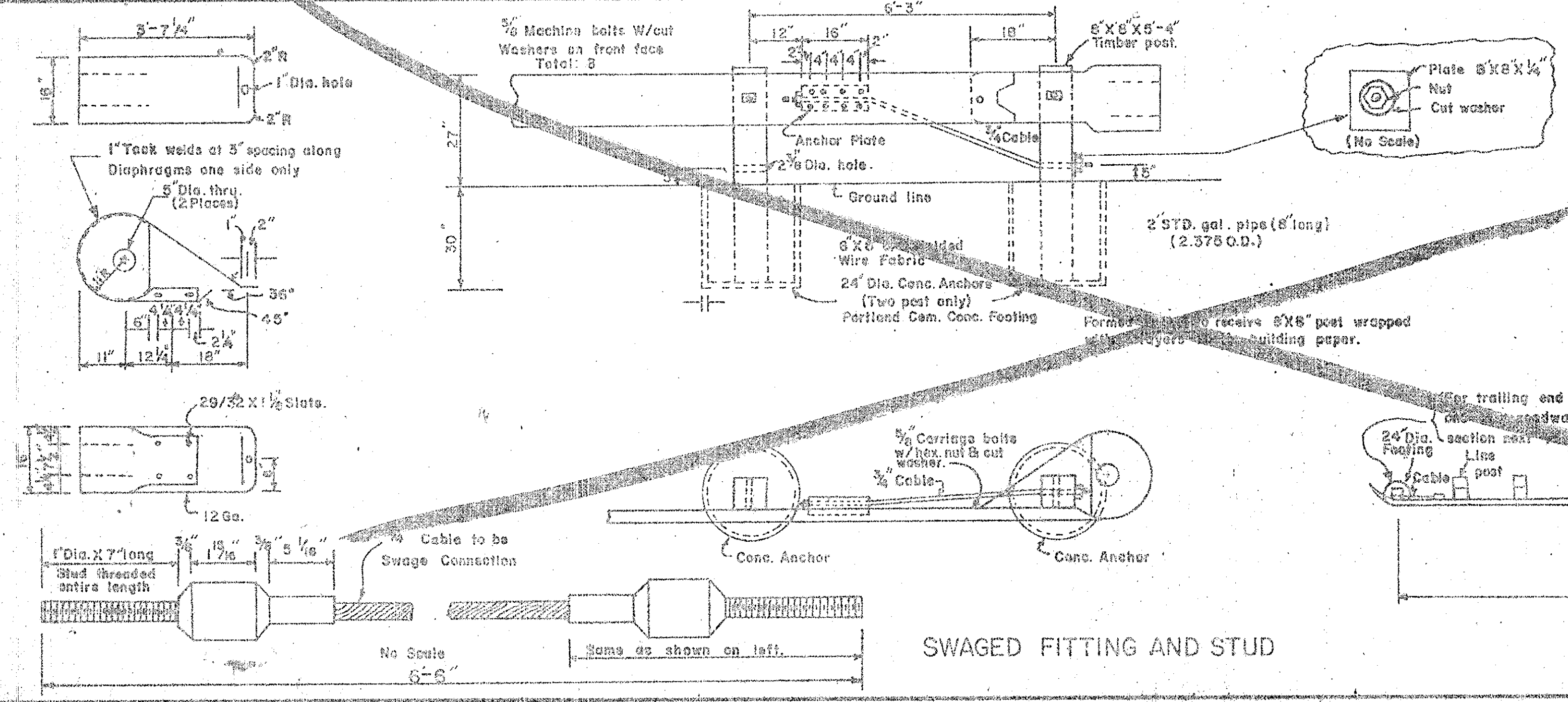
TYPE B ANCHOR



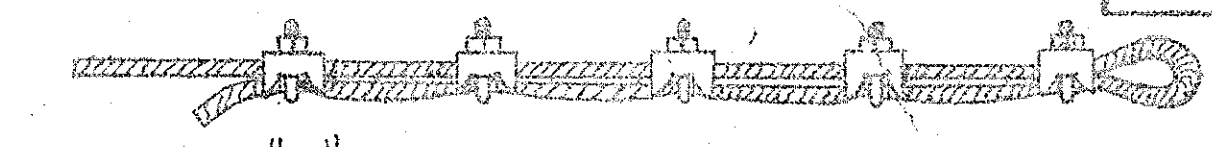
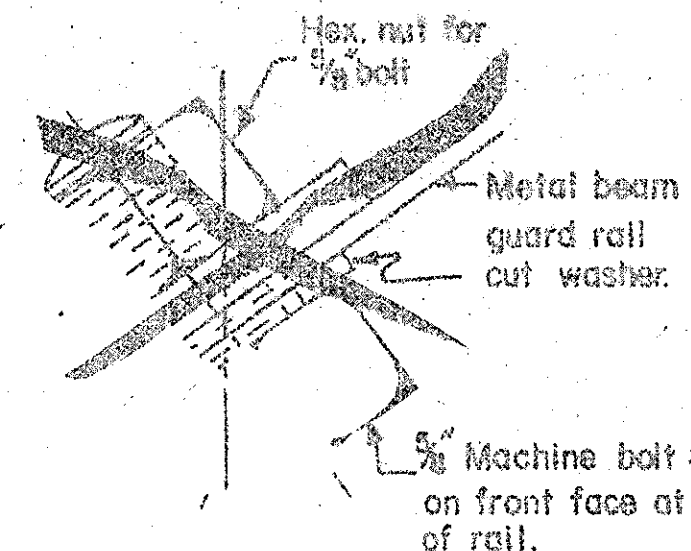
ANCHOR PLATE DETAILS



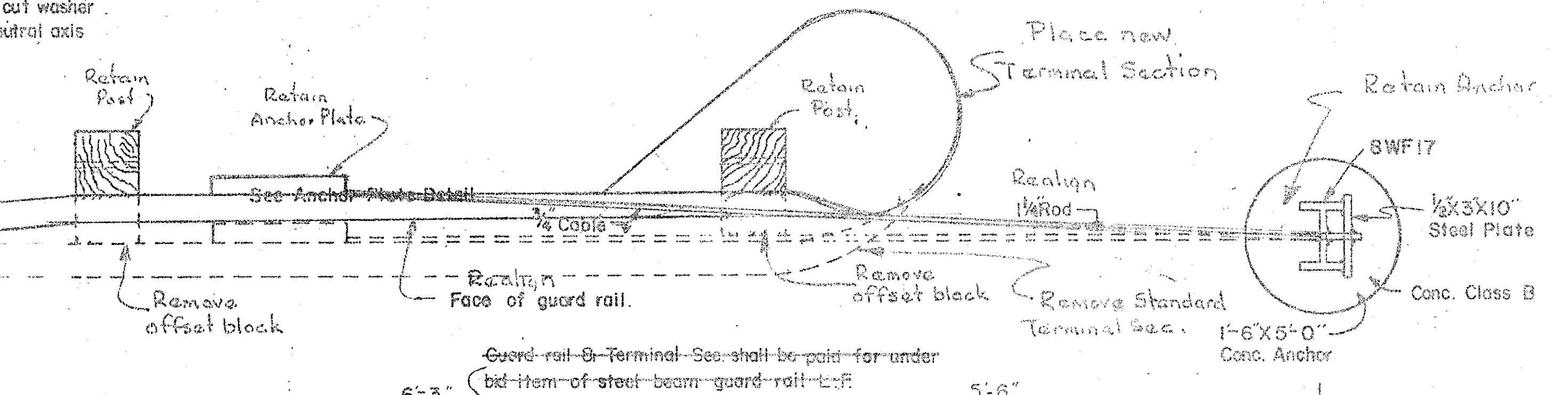
STANDARD SWAGED FITTING & STUD



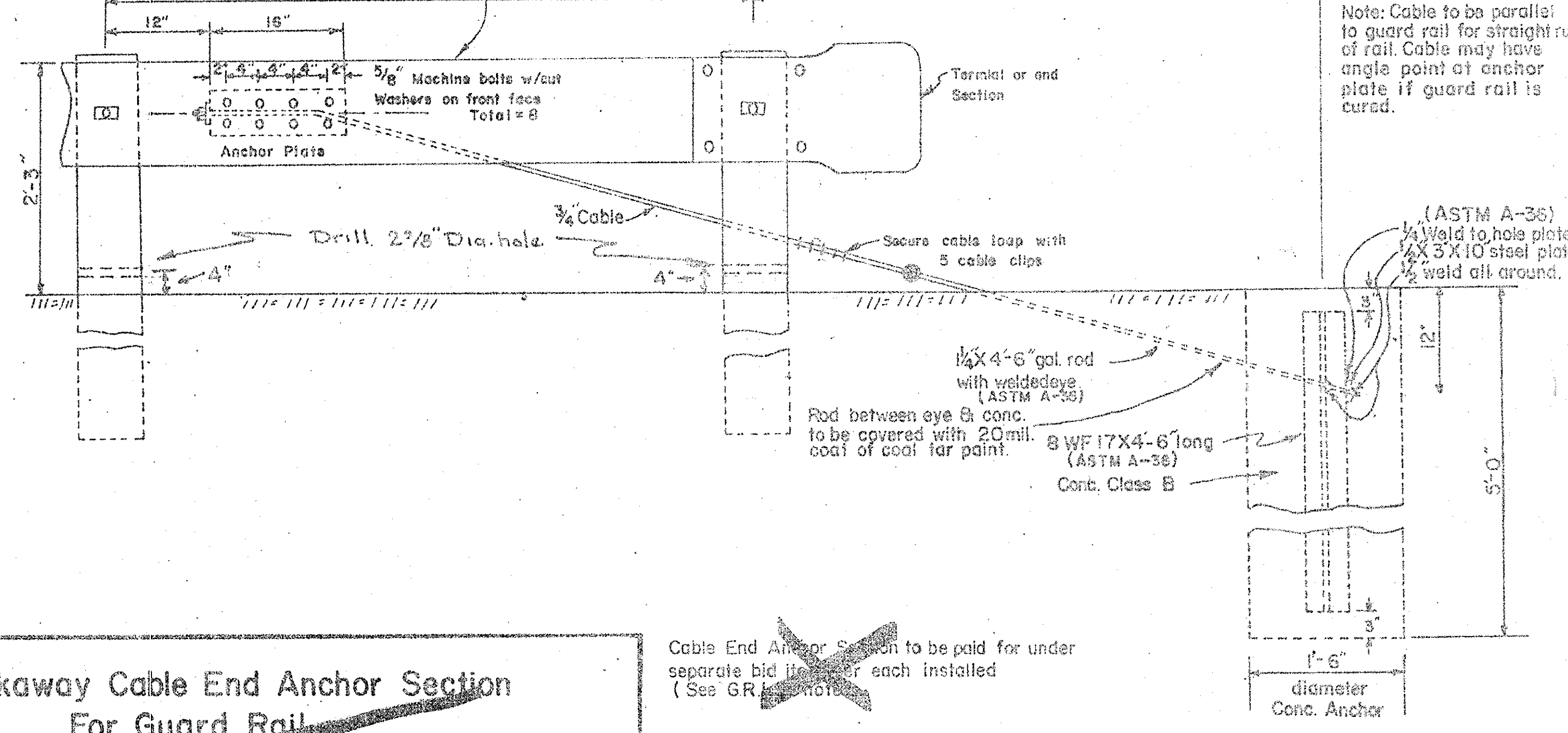
SWAGED FITTING AND STUD



ADJUST-CABLE CLIP INSTALLATION
ASTM-A-164 Entire Hardware for Clamps & Washer.



Note: Cable to be parallel to guard rail for straight run of rail. Cable may have angle point at anchor plate if guard rail is curved.



Breakaway Cable End Anchor Section For Guard Rail

Cable End Anchor Section to be paid for under separate bid item for each installed (See GR-2)

Note: All fitting, plates, bolts, nuts, clamps, washers, studs to be galvanized in accordance with standard specification.

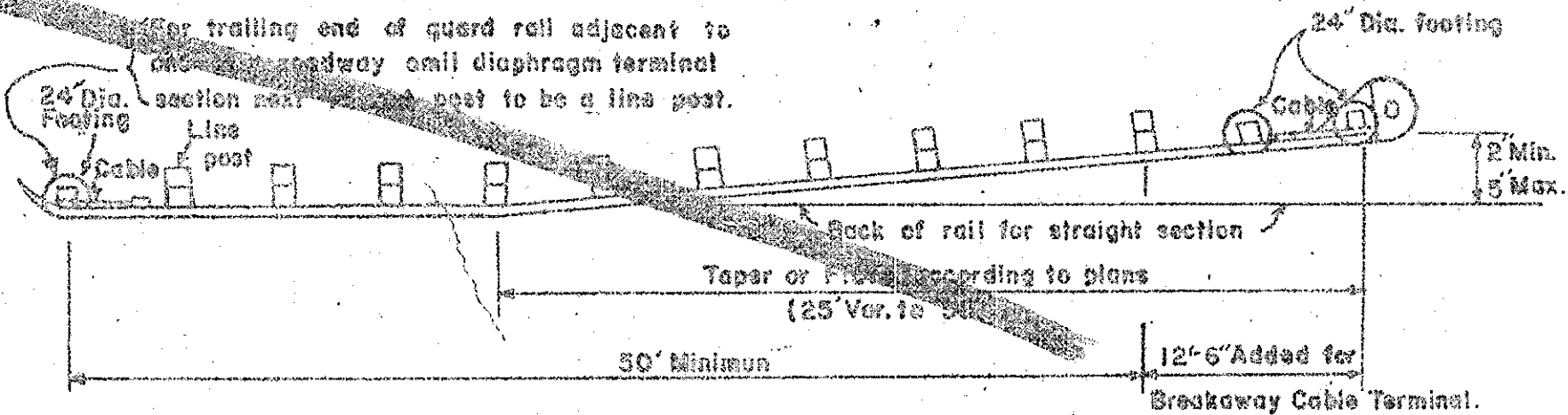
Galvanized Cable ASTM A-803-70

All bolts nuts & studs ASTM A-307 steel

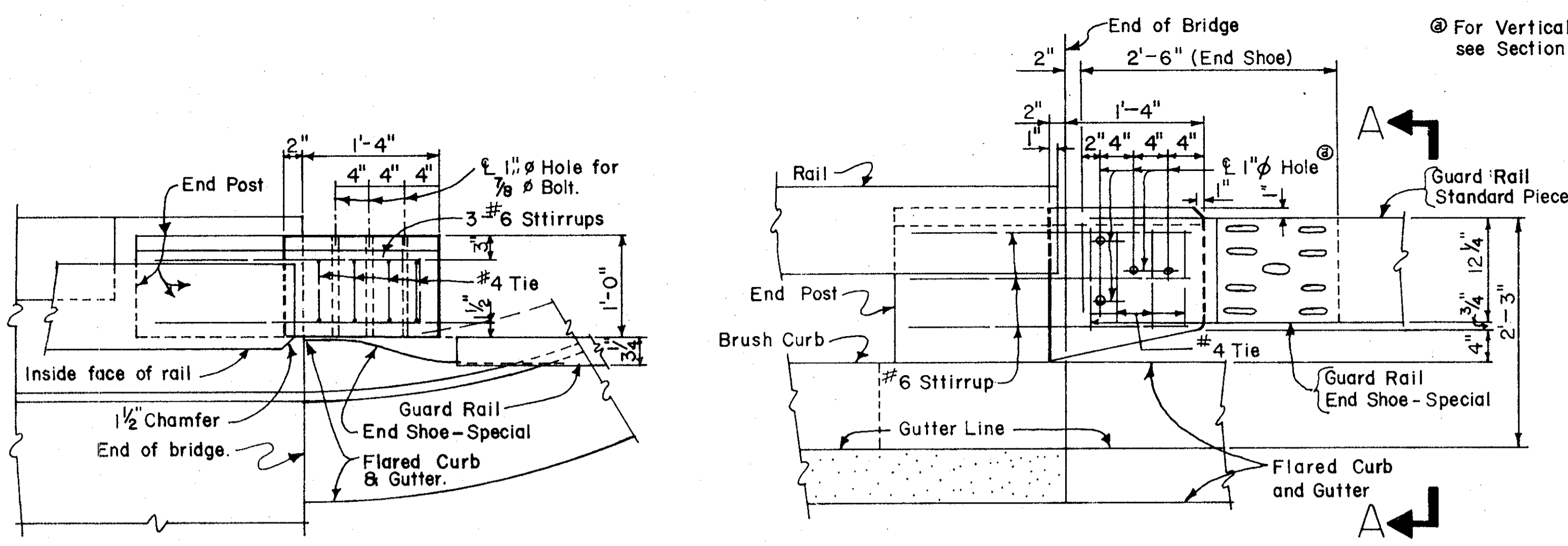
All steel to be ASTM-A-36

End Anchor (Type A) (Cable and converted)

CABLE END ANCHOR SECTION FOR GUARD RAIL

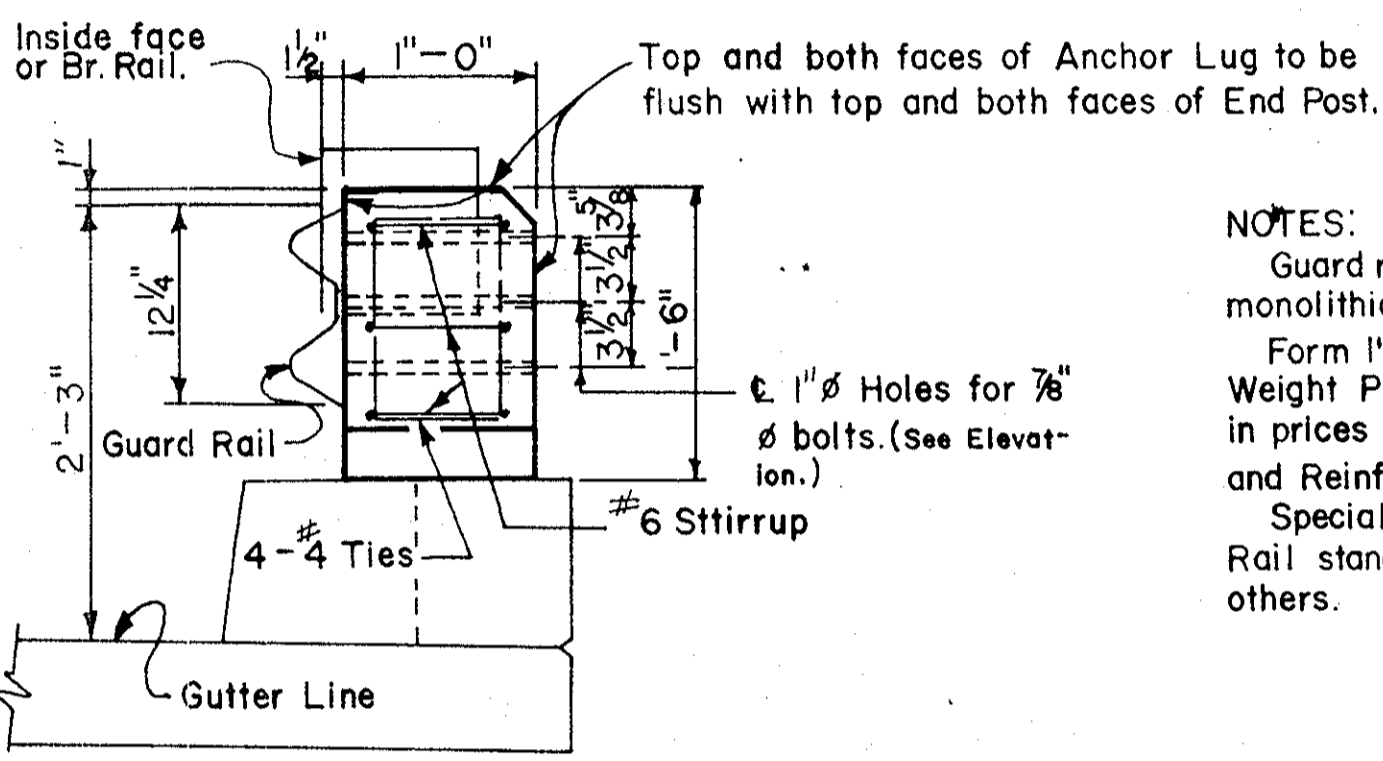


PLAN No Scale



PLAN

ELEVATION



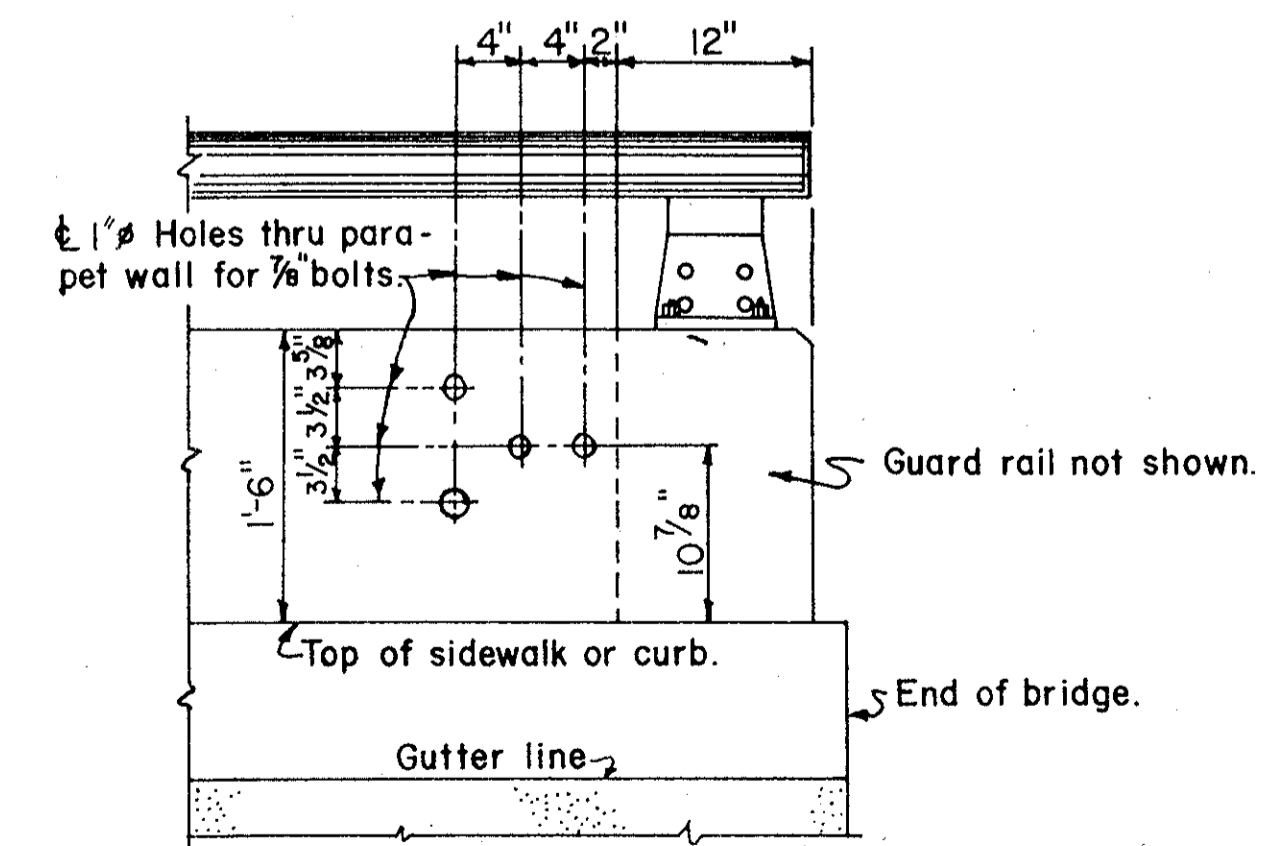
SECTION A-A

NOTES:
 Guard rail Anchor lug to be cast monolithic with end post.
 Form 1" holes with 1" Standard Weight Pipe. Cost of pipe to be included in prices bid for Class 'A' Concrete and Reinforcing steel.
 Special End Shoe and Guard Rail standard piece furnished by others.

| QUANTITIES ONE LUG | |
|--------------------|----------|
| Class "A" Concrete | .07 C.Y. |
| Reinforcing Steel | 37 Lbs. |

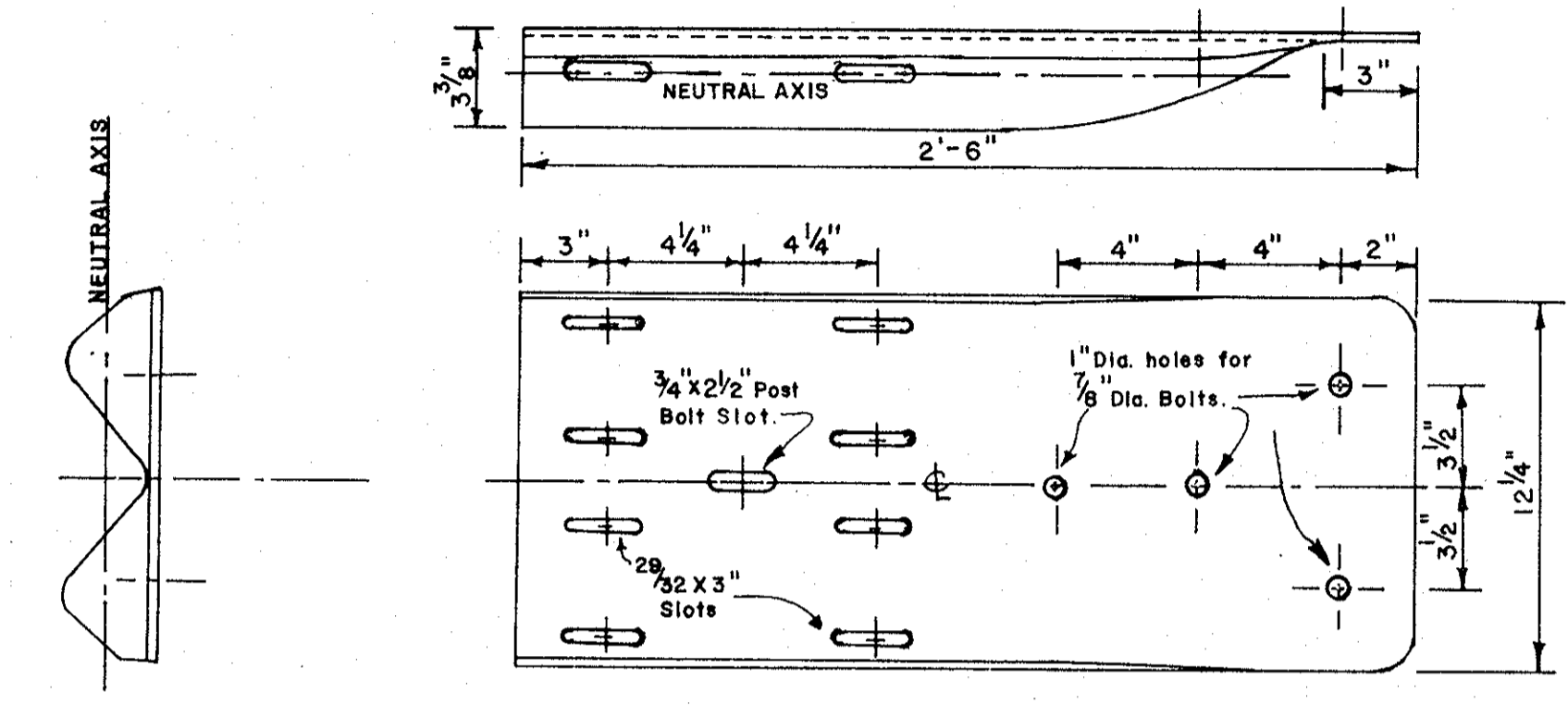
GUARDRAIL ANCHOR LUG DETAILS
 SCALE: 1"=1'-0"

Guard rail to be constructed on each end of bridge, this connection will be constructed in bridge contract. Guard rail contract to furnish four (4) bolts and four (4) rectangular washers for connecting guard rail to bridge at each connection.
 If guard rail and roadway contract has been complete before bridge contract has been complete, contractor will furnish necessary hardware for S.C. Hwy. Dept. Maintenance Force to connect guard rail to bridge.



DETAIL AT ENDS OF BRIDGE WITH 1'-6" PARAPET WALLS SHOWING PROVISION FOR ATTACHMENT OF GUARD RAIL.
 Scale: 1"=1'-0"

Note: Form 1" holes with 1" standard weight pipe. Cost of pipe to be included in the price bid for reinforcing steel. Pipe to remain in place.



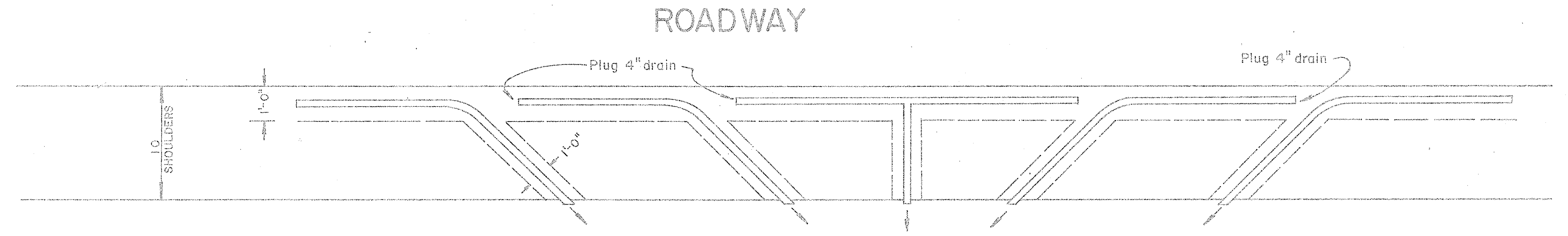
SPECIAL END SHOE

Guard rail contractor to furnish special end shoe to connect guard rail to concrete anchor lug on bridge ends.

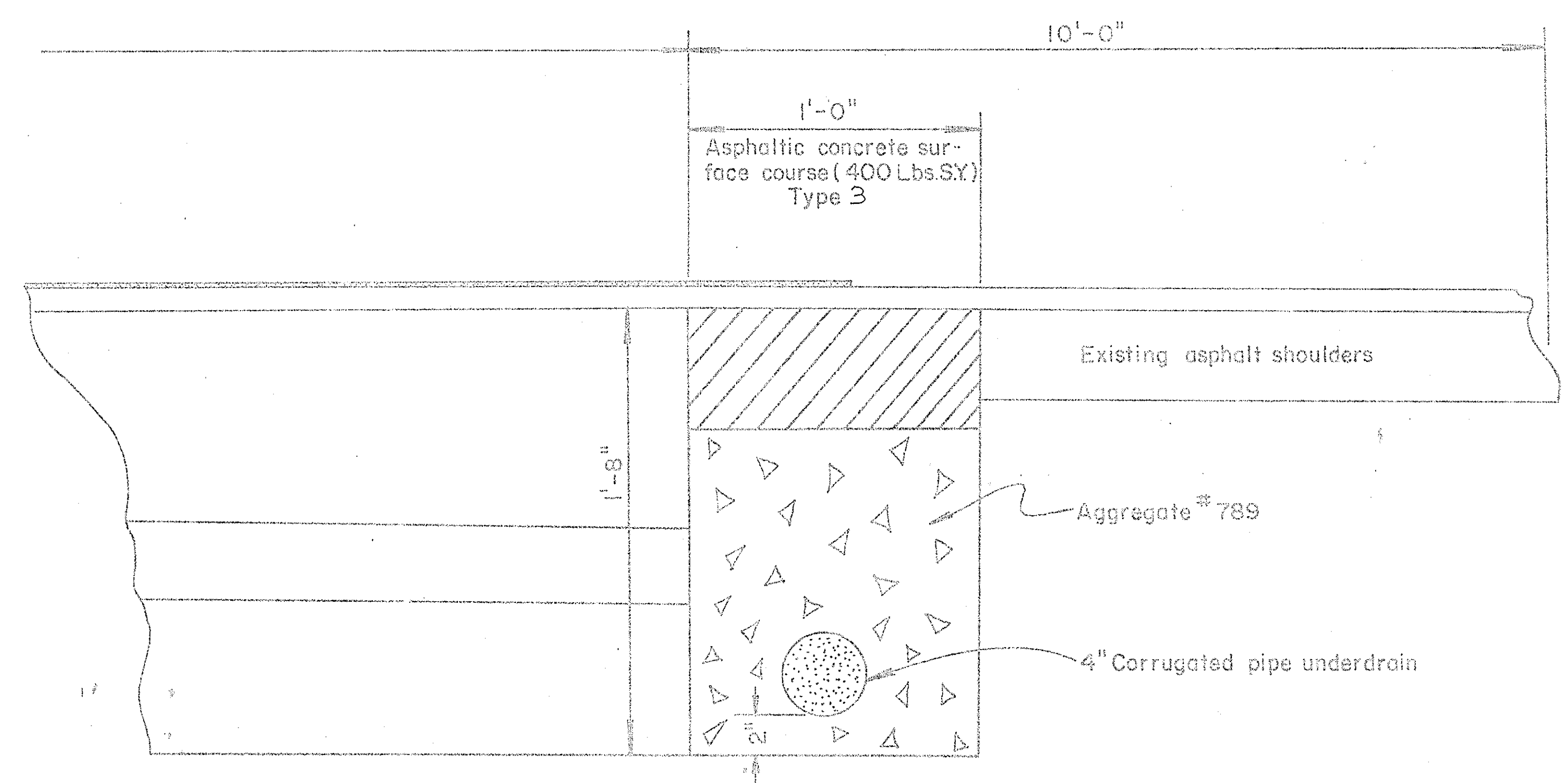
Special end shoe shall be of iron or steel of not less than 10 gauge. It shall be a minimum of twelve (12") inches wide and when tested by clamping a section of any section in testing machine shall show a minimum breaking strength of 60,000 pounds at the joints.

| | | |
|----------|------|---|
| REV. | | S.C. DEPARTMENT OF HIGHWAY & PUBLIC TRANSPORTATION |
| REV. | | COLUMBIA S.C. |
| REV. | | STANDARD DETAILS |
| REV. | | STANDARD CONNECTION OF GUARD RAIL TO END POST & SPECIAL END SHOE. |
| REVIEWED | | IN CHARGE |
| TR. | | |
| DES. | | |
| BY | CHKD | DATE |

| Fed. Road Div. No. | State | County | File No. | Proj. No. | Sheet No. | Total Sheets |
|--------------------|-------|----------|----------|-----------|-----------|--------------|
| 3 | S.C. | CHEROKEE | 11,530.1 | 285-3(45) | 185 | 660 |

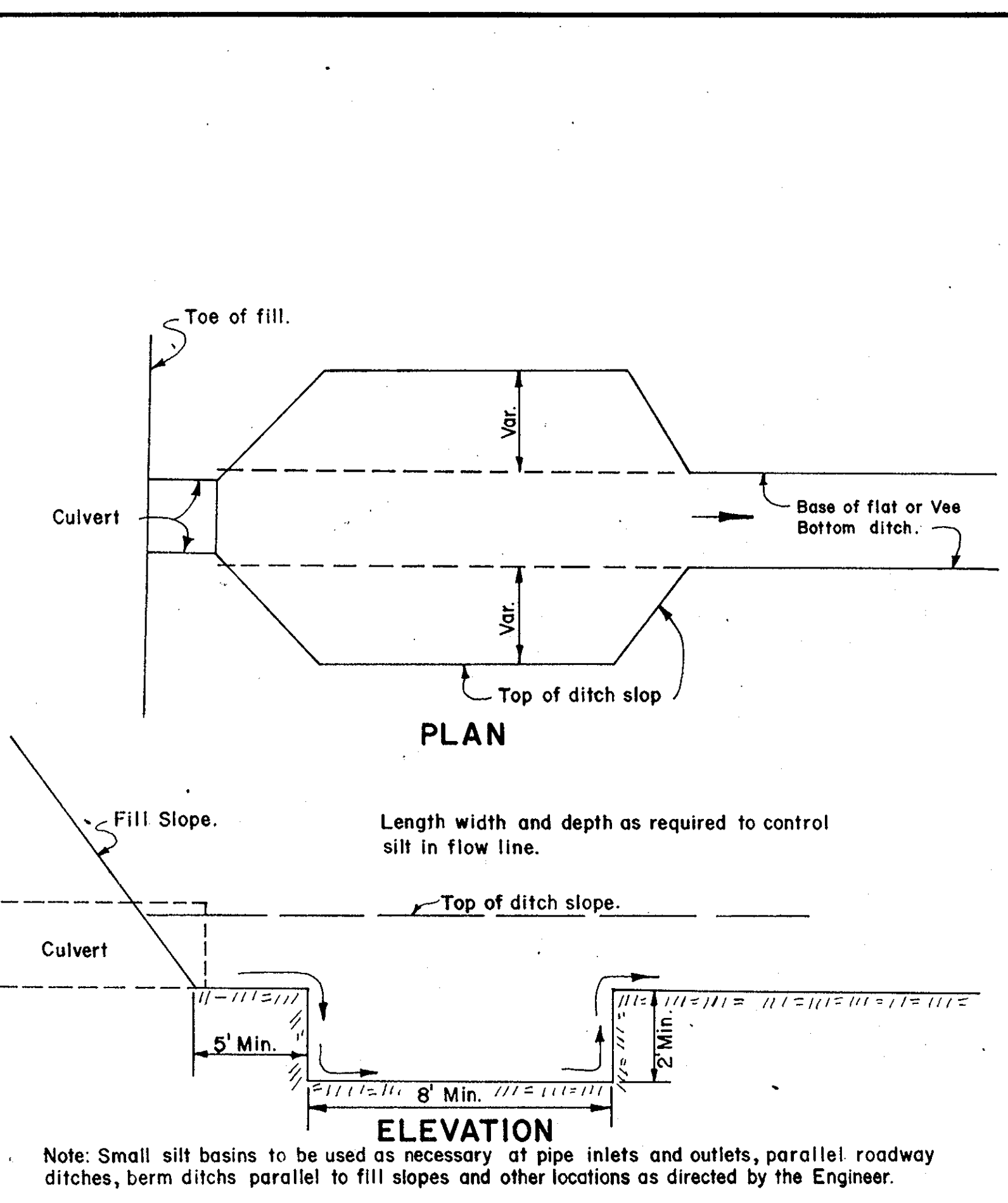


Plug drains as directed by Engineer.

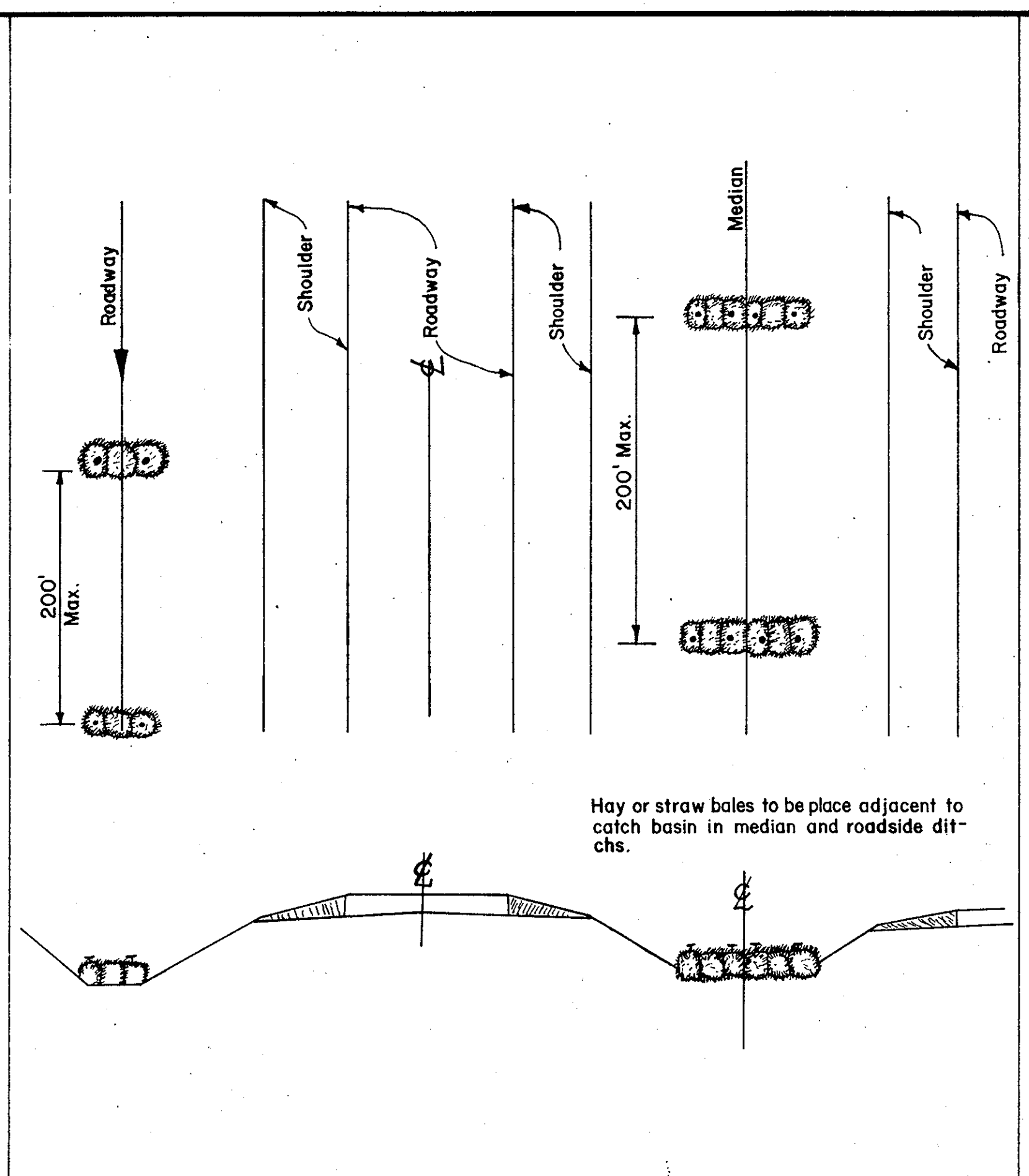


Note: The cost of aggregate #789 & asphaltic concrete surface course shall be included in the bid price of 4" corrugated pipe underdrain.

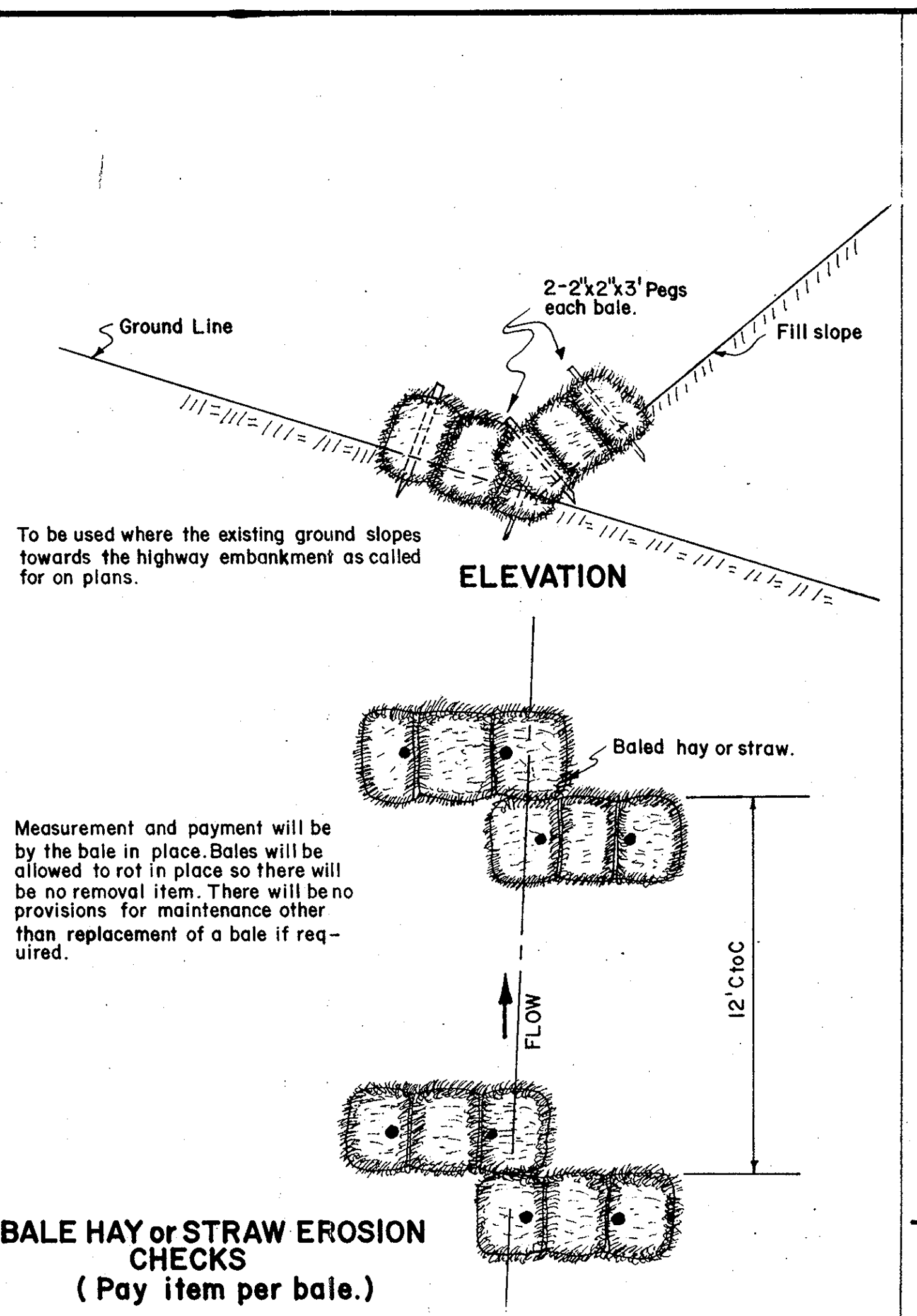
SEE CONSTRUCTION NOTE



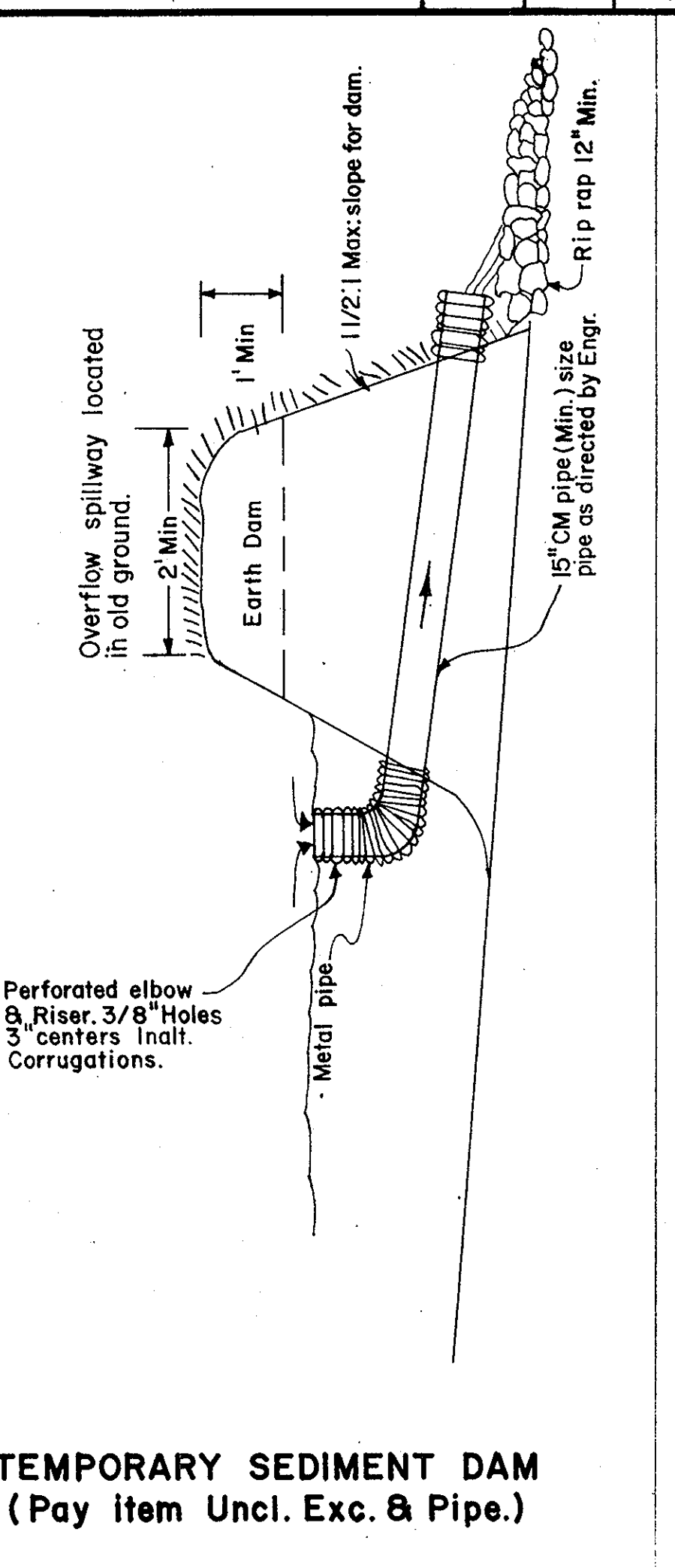
DETAIL OF SILT BASIN (Pay Item Uncl. Exc.)



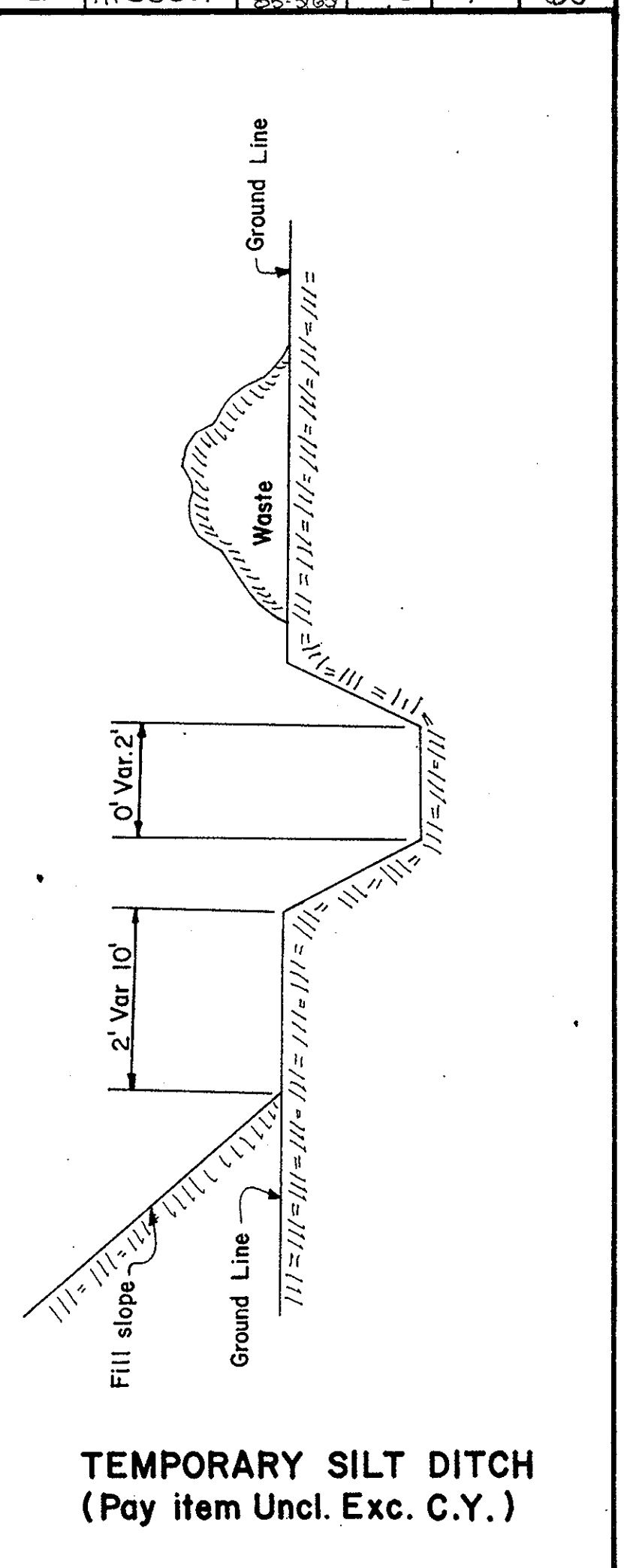
HAY BALE DAM, USED IN DITCHES (Pay Item per bales.)



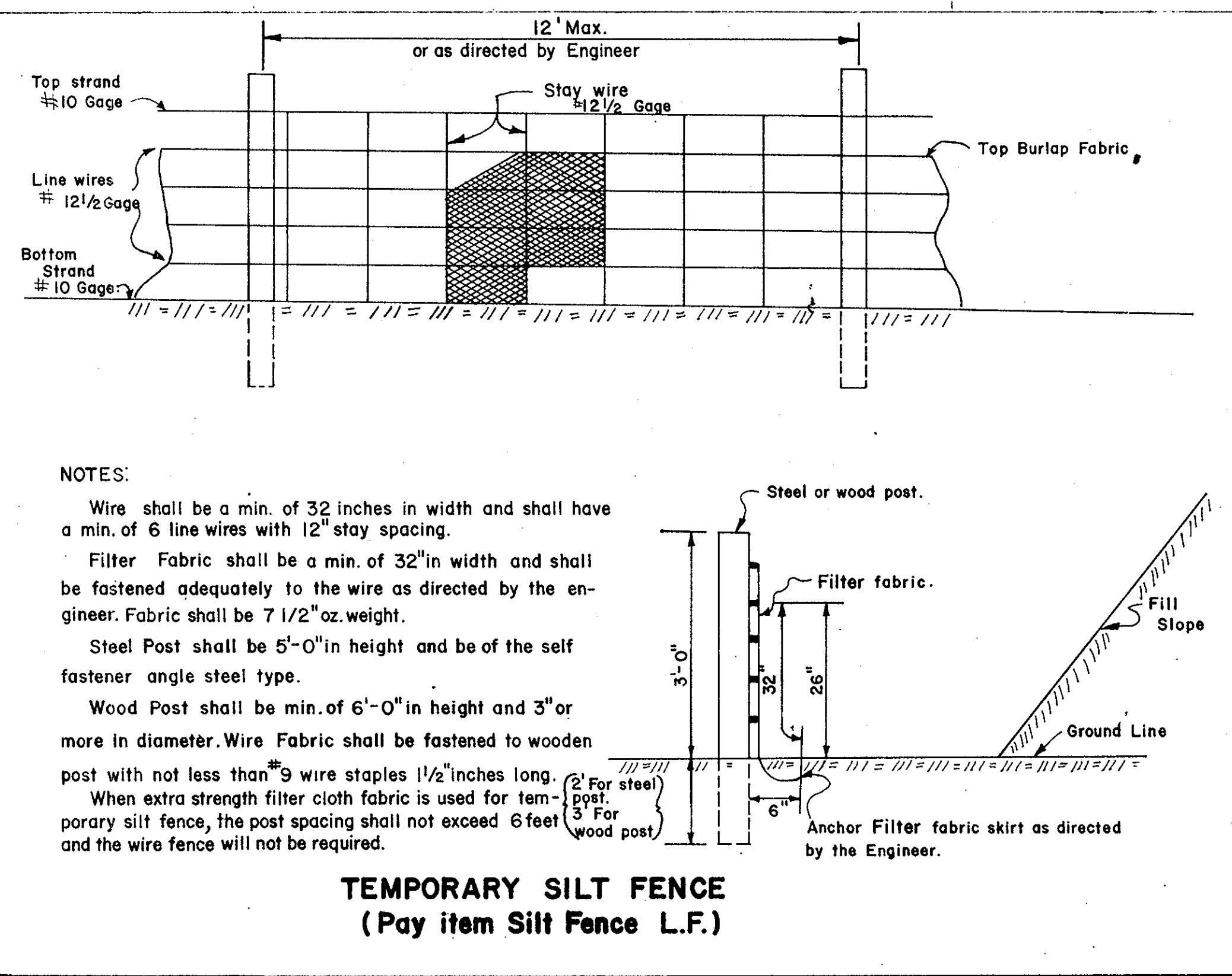
BALE HAY or STRAW EROSION CHECKS (Pay item per bale.)



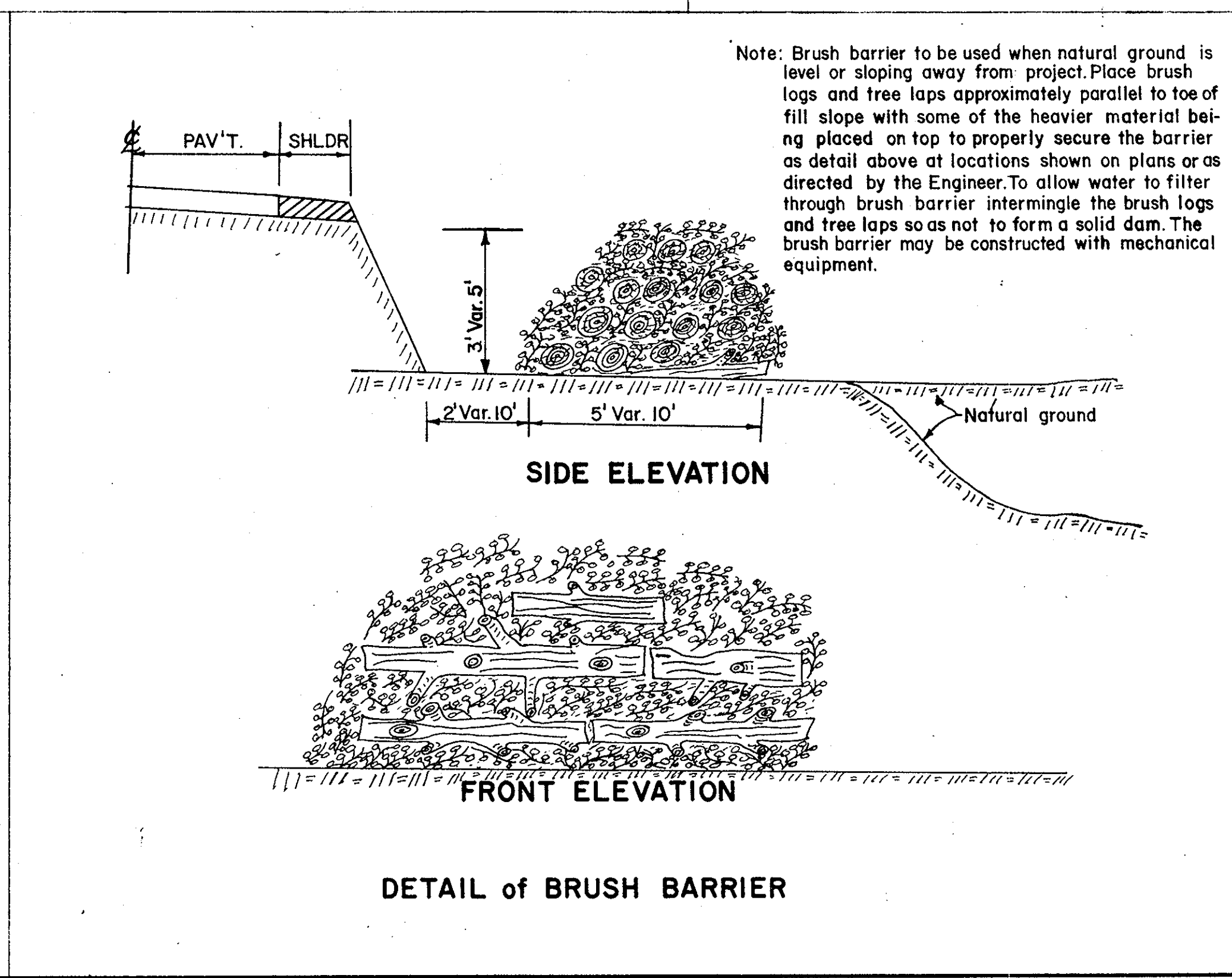
TEMPORARY SEDIMENT DAM (Pay item Uncl. Exc. & Pipe.)



TEMPORARY SILT DITCH (Pay item Uncl. Exc. C.Y.)



TEMPORARY SILT FENCE (Pay item Silt Fence L.F.)



DETAIL of BRUSH BARRIER

TEMPORARY EROSION AND SEDIMENTATION CONTROL DETAILS
S.C. DEPT. HWY. & PUBLIC TRANS.
DATE 9-11-78

DEMEINON FOR PRECAST CONCRETE CATCH BASINS

| PIPE SIZE | TYPE A | | |
|-----------|--------|-------|--------|
| | W | W I | MIN H |
| 15" | 3'-0" | 3'-0" | 2'-7" |
| 18" | 3'-0" | 3'-0" | 2'-10" |
| 24" | 3'-0" | 3'-0" | 3'-8" |
| 30" | 3'-0" | 3'-0" | 4'-2" |

PLAN TYPE B

NOTE: REFER TO THE STANDARD DESIGN OF CATCH BASINS TYPE 12 FOR ALL DETAILS NOT SHOWN ON THIS DRAWING.

NOTE: THE TYPE B CAP SHALL HAVE THE SAME REINFORCING THROUGHOUT AS THE STANDARD STRUCTURE.

NOTE: REINFORCING STEEL SHALL BE 2/2, 6X6 WELDED WIRE FABRIC TO ALLOW A MINIMUM OF 2" CLEARANCE.

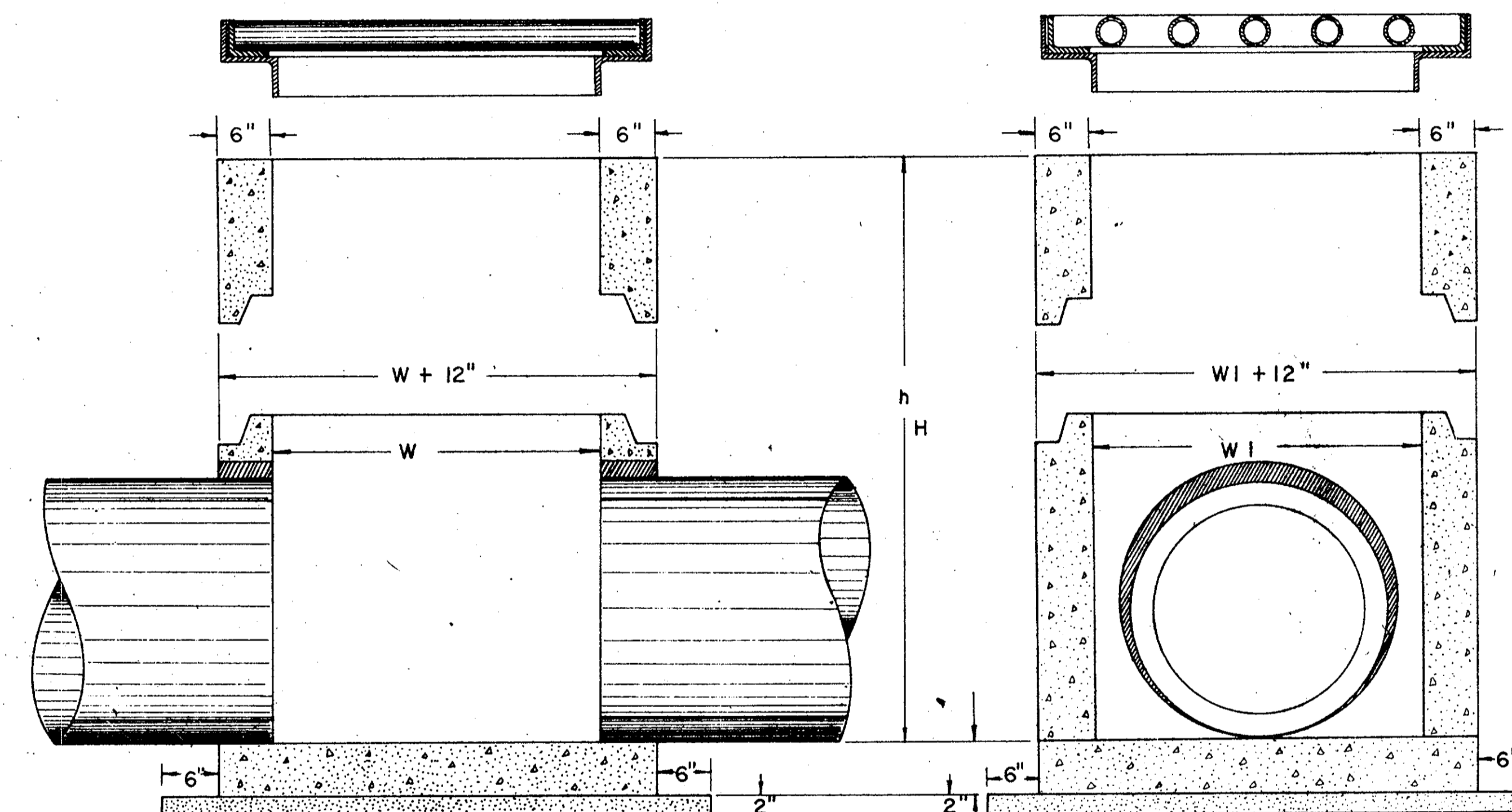
REINFORCING RODS OF 1/2" DIAMETER PLACED ON 12" CENTERS BOTH VERTICAL AND HORIZONTAL MAY BE USED AS AN ALTERNATE TO THE ABOVE.

REINFORCING STEEL SHALL BE CONTINUOUS WITHIN EACH SECTION OF THE STRUCTURE INCLUDING THE BASE, WALLS AND TOP, AND SHALL TIE THE WALLS TO THE BASE TO INSURE A FIRM BOND.

NOTE: IF BOX DEPTH EXCEEDS 4'-6", METAL STEPS (MIN. OF 3/4" BAR) ARE TO BE PLACED 15" CTR. TO CTR. ON WALL.

NOTE: THE CONTRACT UNIT PRICE FOR CATCH BASINS TYPE 12 SHALL INCLUDE THE COST OF FURNISHING ALL MATERIALS AND WORK INCIDENTAL TO THE CONSTRUCTION OF THE STRUCTURE COMPLETE IN PLACE AS SHOWN, INCLUDING POROUS BED OF STONE OR SAND IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.

NOTE: THE BASE OF THE BOX SHALL BE 6" THICK AND SHALL BE TIED TO THE CAP WITH A MINIMUM OF 12" TO 15" DIAMETER PIPE EXTENDING THROUGH THE CAP AND BASE AND SHALL BE FIRMLY SECURED TO EACH WITH CEMENT MORTAR MEETING THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS.



ELEVATION TYPE A

ELEVATION TYPE A

A POROUS BED CONSISTING OF COURSE SAND OR CRUSHED STONE 2" IN DEPTH SHALL BE PLACED UNIFORMLY OVER THE SUBGRADE PRIOR TO PLACING THE CATCH BASIN. THE POROUS BED SHALL EXTEND SIX (6") INCHES OUTSIDE THE NEAT LINES OF THE CATCH BASIN.

See drawing no. 720-12 for details of frame, grate and paved collar.

APPROVED BY _____ DATE _____

STATE HIGHWAY ENGINEER

S.C. STATE HIGHWAY DEPT.

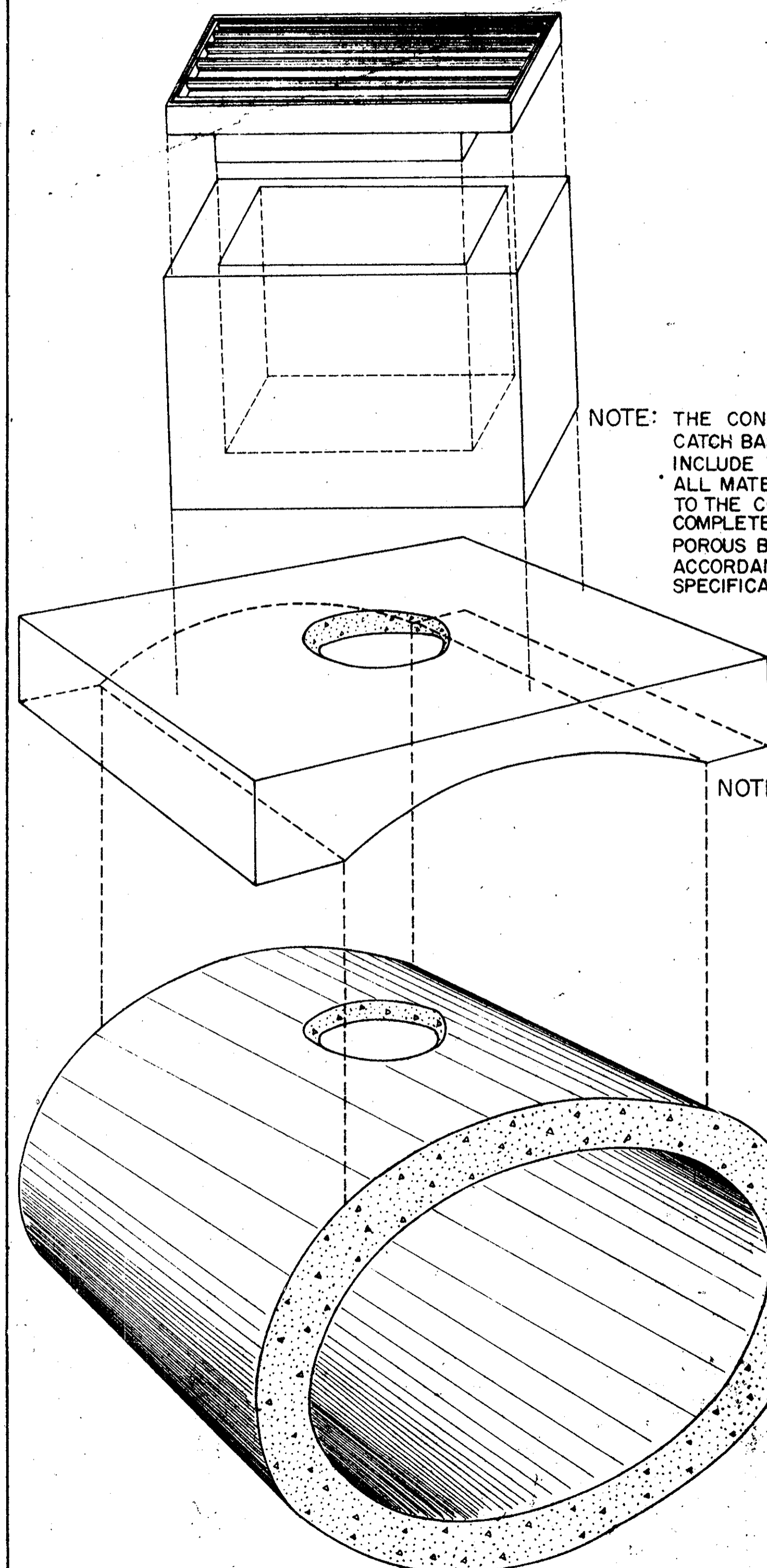
STANDARD CONSTRUCTION DETAIL
PRECAST CONCRETE MEDIAN
CATCH BASINS TYPE 12

SCALE
1" = 1'-0"

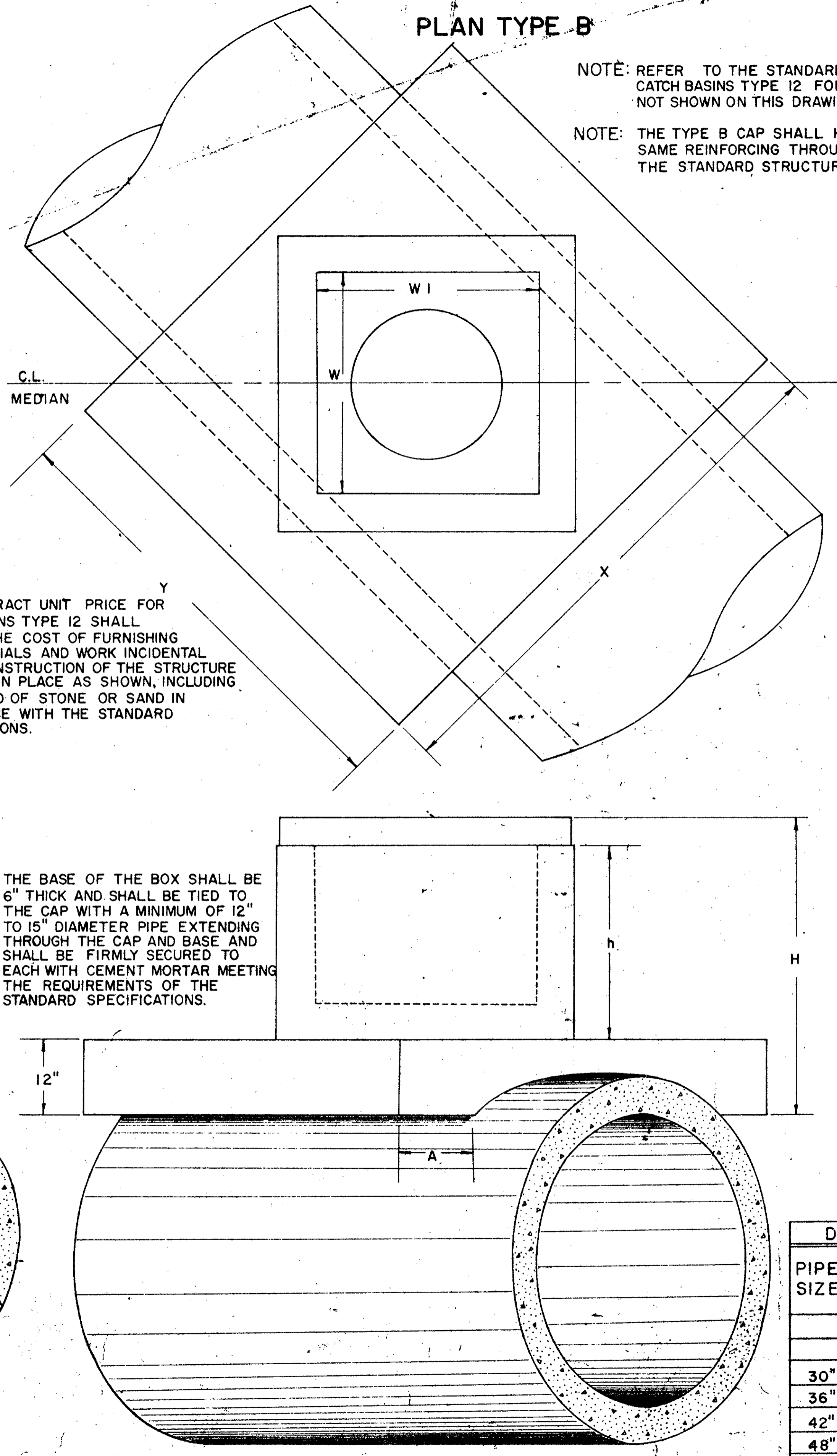
DRAWING NO. 720.12.1

DIMENSIONS FOR PRECAST CONC. CATCH BASINS

| PIPE SIZE | TYPE B | | | | | |
|-----------|--------|-------|-------|-------|-----|-------|
| | W I | W | X | Y | A | MIN h |
| 30" | 3'-0" | 3'-0" | 5'-0" | 5'-0" | 16" | 2'-0" |
| 36" | 3'-0" | 3'-0" | 5'-0" | 5'-0" | 15" | 2'-0" |
| 42" | 3'-0" | 3'-0" | 5'-0" | 5'-0" | 14" | 2'-0" |
| 48" | 3'-0" | 3'-0" | 5'-0" | 5'-0" | 12" | 2'-0" |
| 54" | 3'-0" | 3'-0" | 6'-0" | 5'-0" | 17" | 2'-0" |
| 60" | 3'-0" | 3'-0" | 6'-0" | 5'-0" | 16" | 2'-0" |



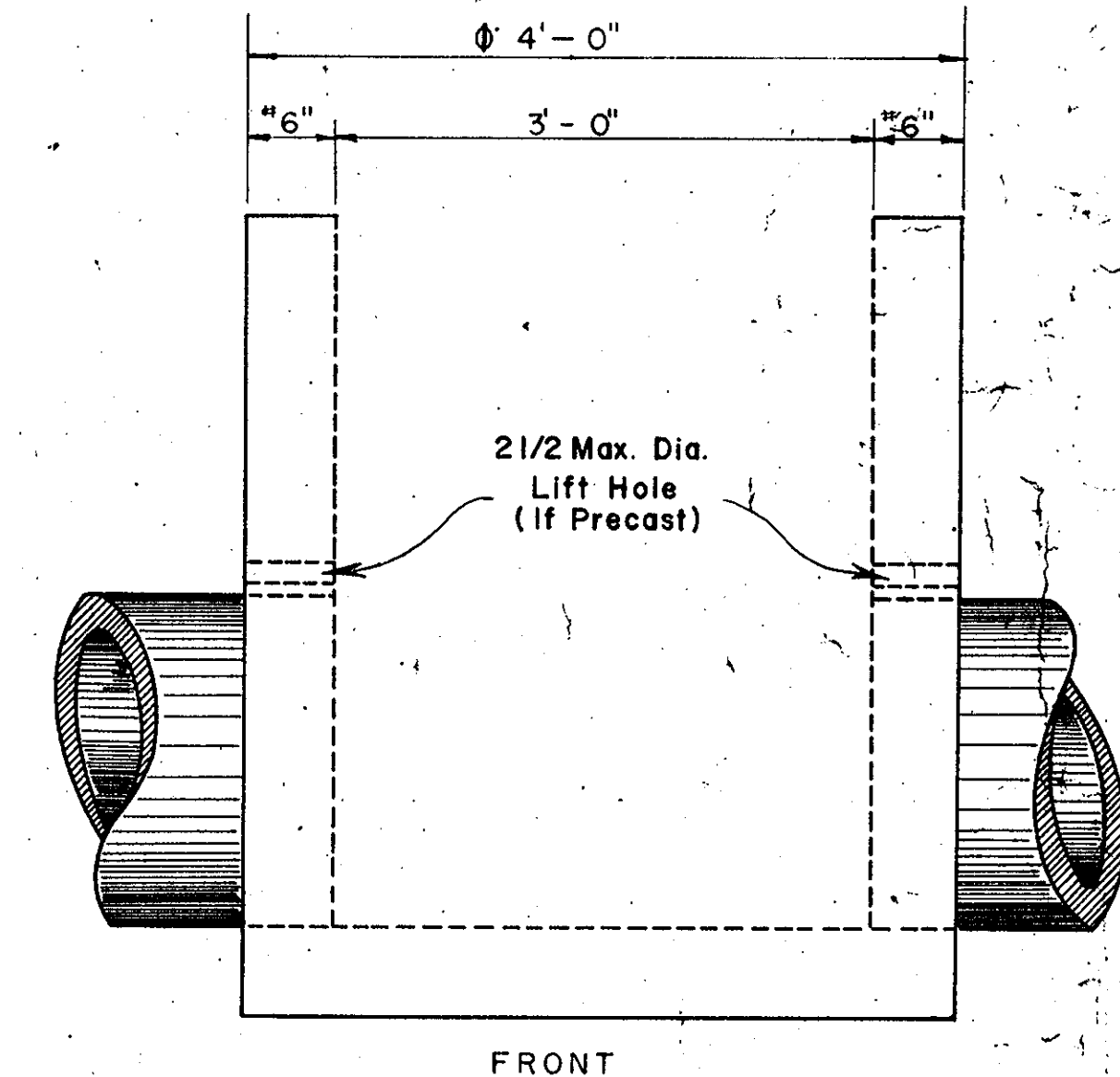
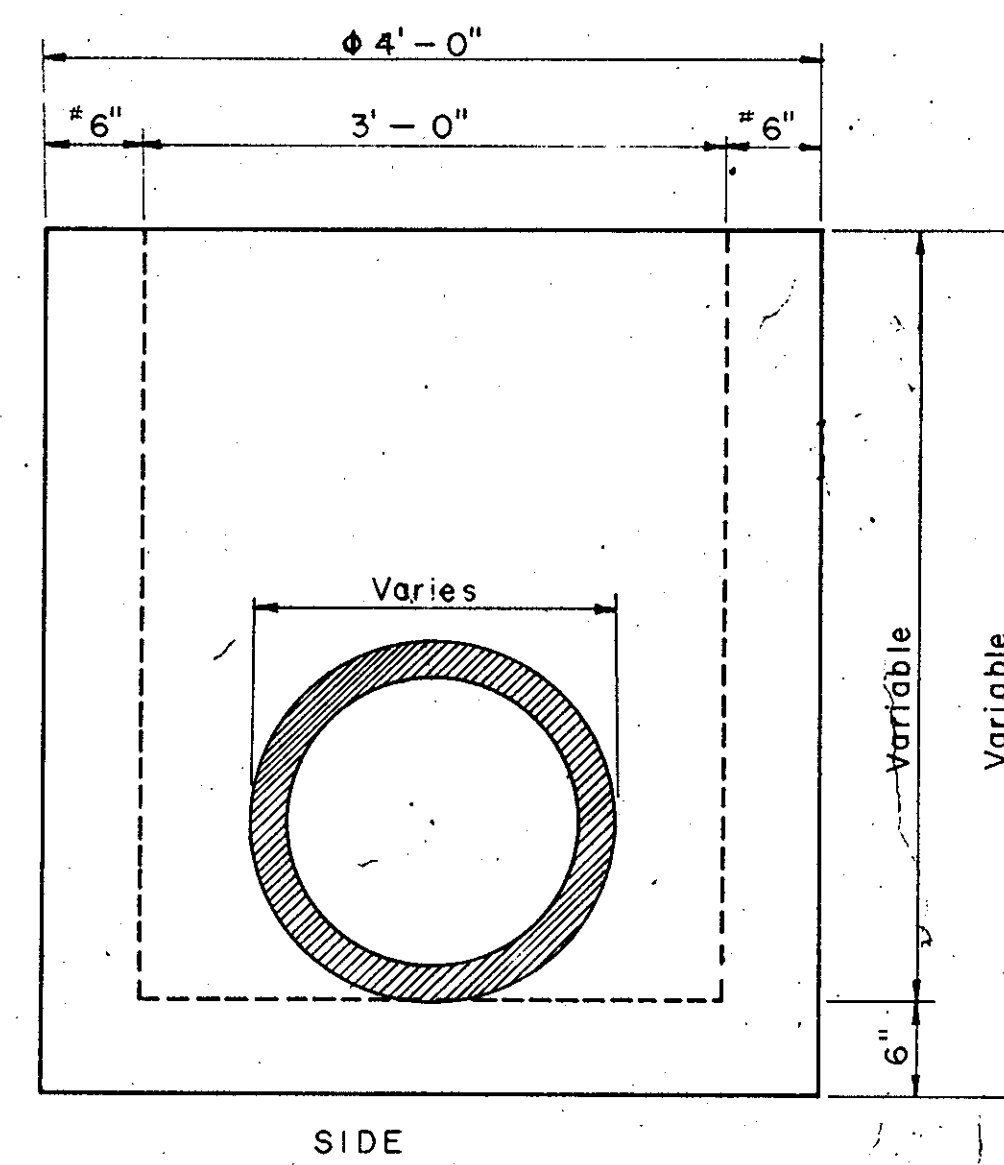
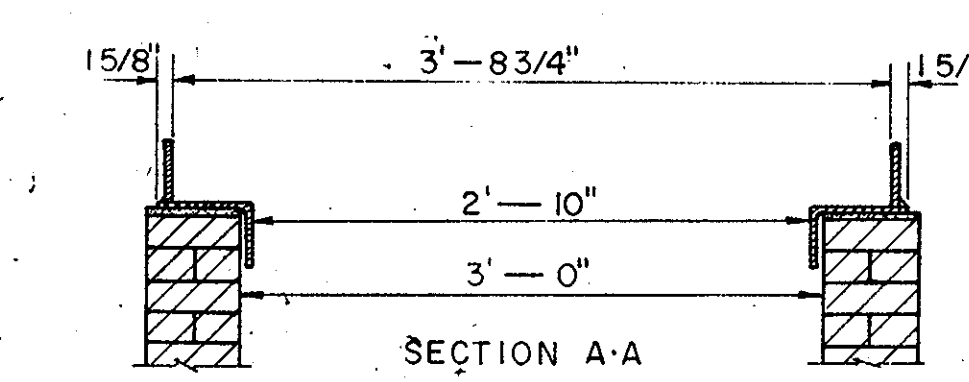
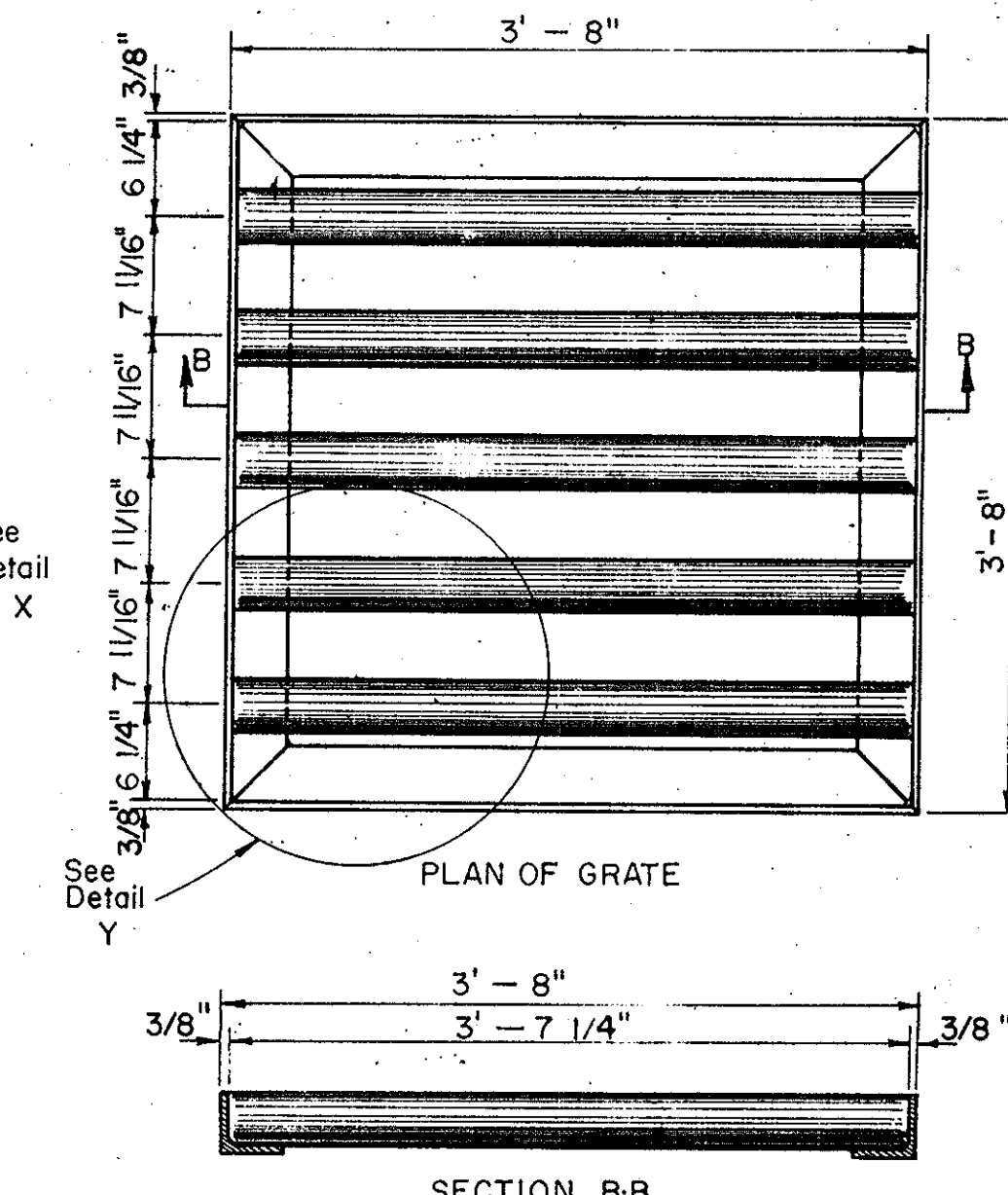
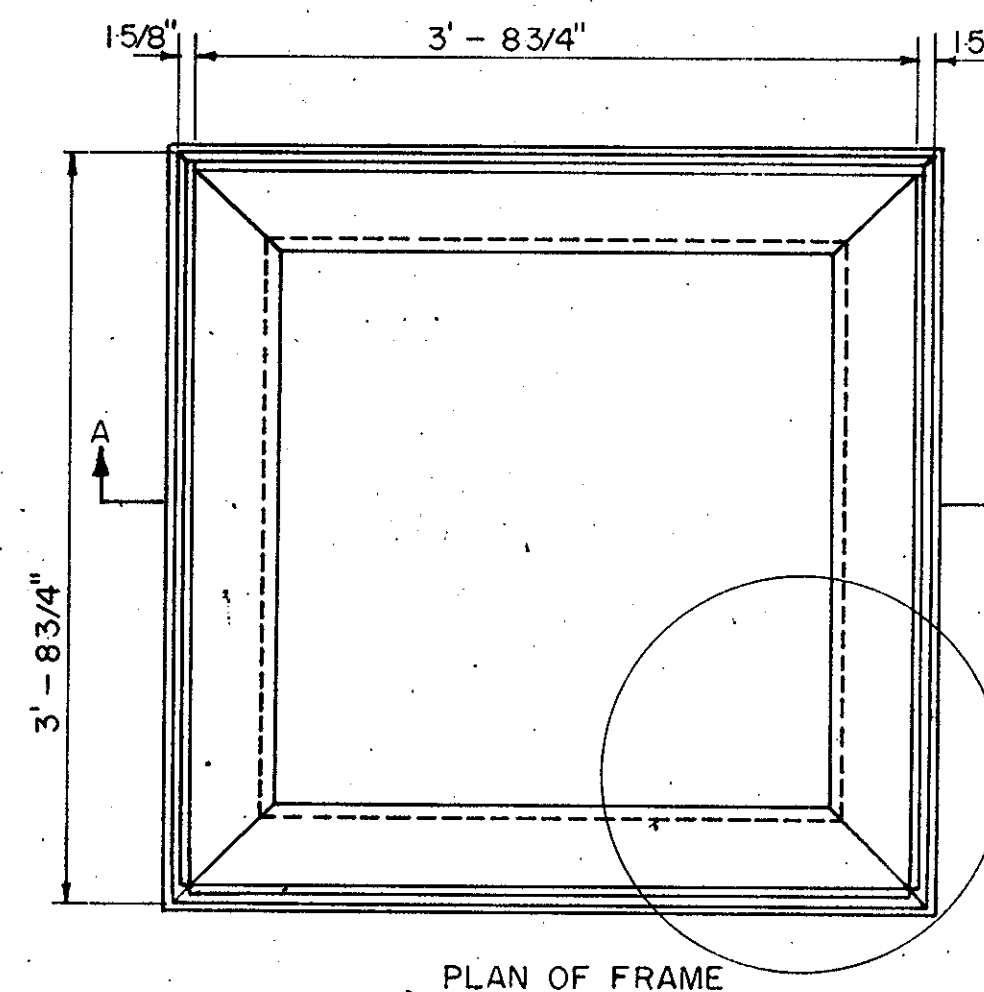
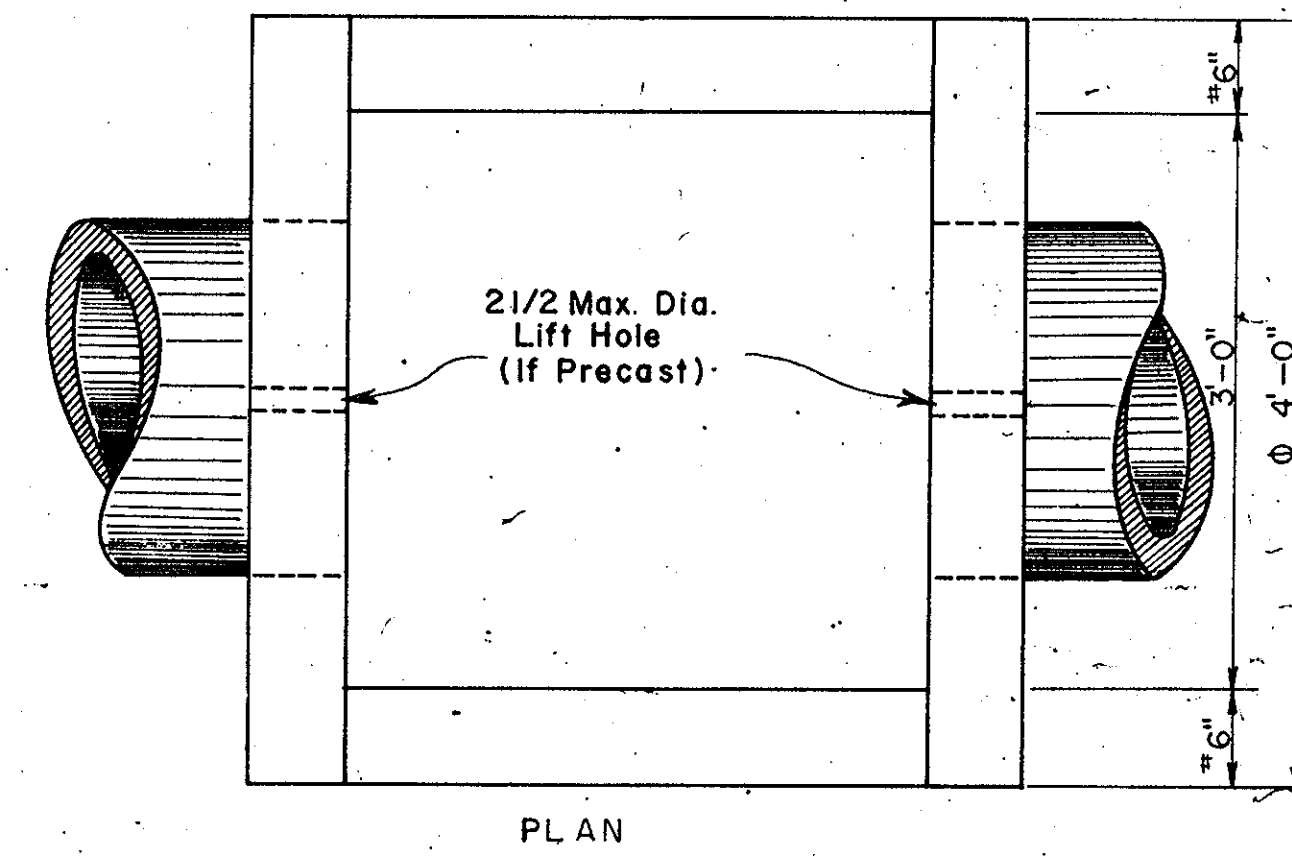
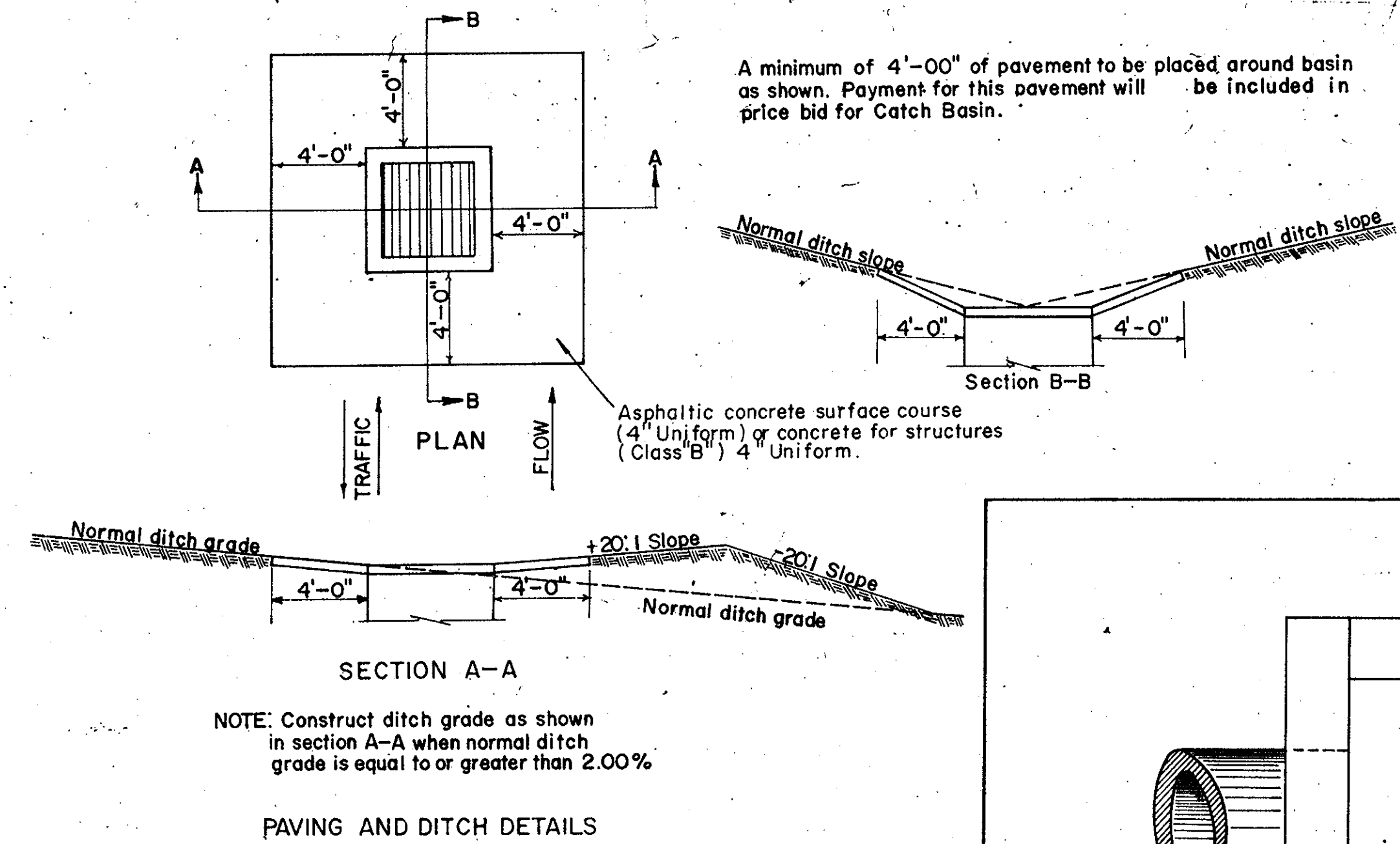
PERSPECTIVE TYPE B



ELEVATION TYPE B

| FED. ROAD DIV. NO. | STATE | COUNTY | FILE NO. | PROJ. NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|-----------|------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11-538(1) | 12-85-3(2) | 1-85 | 9-A | 60 |

S.C. STATE HIGHWAY DEPT. COLUMBIA S.C.



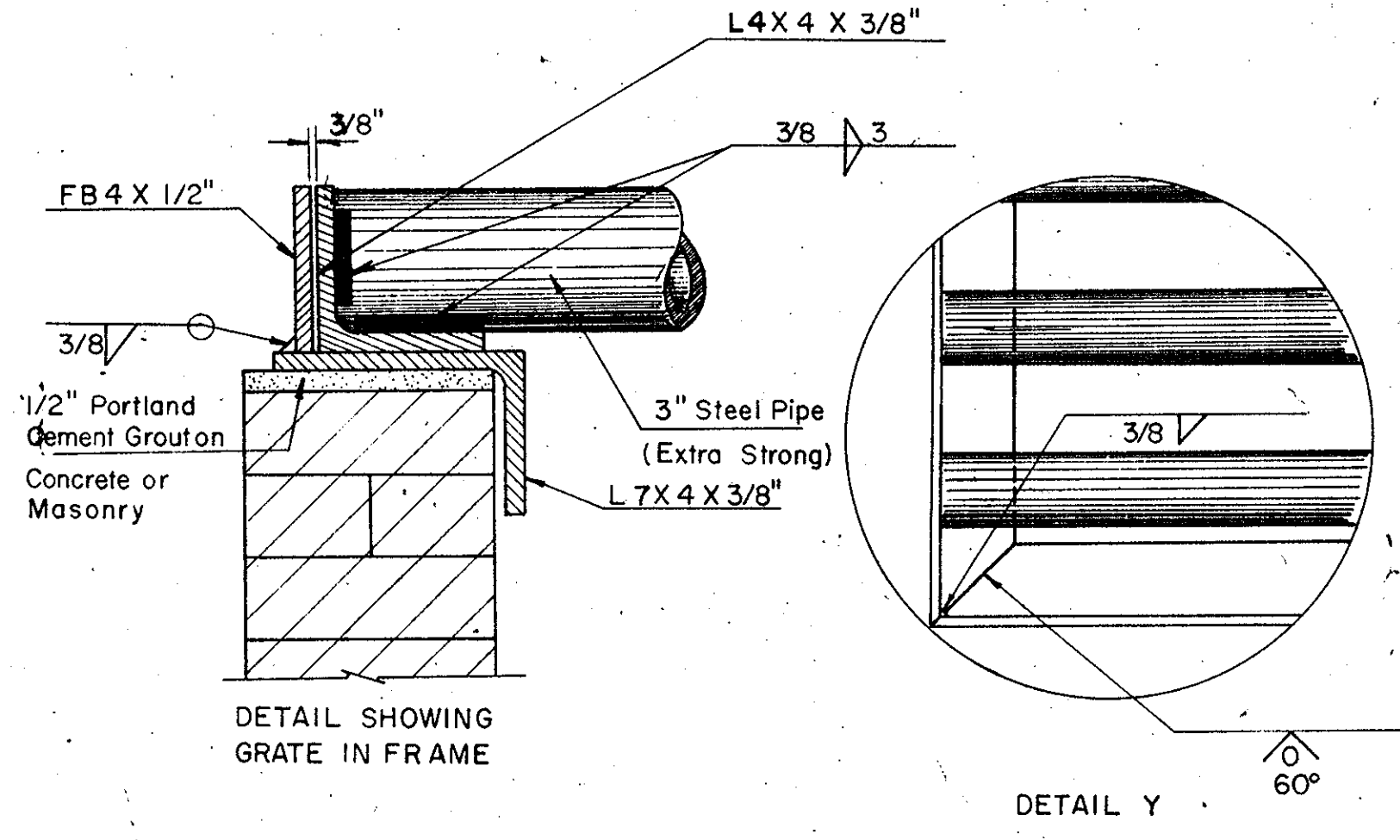
- # 6" for Concrete
- 8" for Brick
- 4'-0" for Concrete
- 4'-4" for Brick

NOTE: For construction of Catch Basin, either brick masonry or Class "A" Concrete may be used. In case of concrete, the walls and floor are to be 6" thick as shown (See sect. 702 of the standard specifications.) For brick, the walls are to be 8" thick and the floor 6" thick. (See section 709 of the standard specifications.)

If desired, these items may be precast prior to installation in lieu of them being cast in place. The use of precast units will not relieve the contractor of the responsibility of obtaining satisfactory installations.

NOTE: All steel to be shop painted with two coats of red lead, or two coats of basic lead/silica chromate, ready-mixed primer; or two coats of zinc yellow-iron oxide base, ready mixed primer; and one coat of black alkylid gloss enamel exterior metal paint.

Note: If box depth exceeds 4'-6", metal steps (Min. of 3/4" bar) are to be placed 15" ctr. to ctr. on wall.



NOTE: Extra strong steel pipe to conform to ASTM specifications A 53, Type E or S, Grade B, open hearth or basic oxygen steel, sulfur not to exceed 0.05 %. Wall thickness 0.300 inches.

Structural steel for angles and bars to conform to ASTM A 36.

REVISED 3-60-74 TO REDUCE SIZE OF 4x4 & 7x4 ANGLES FROM 7/16" TO 3/8" THICKNESS AND TO UPDATE REFERENCE TO STANDARD SPECIFICATIONS

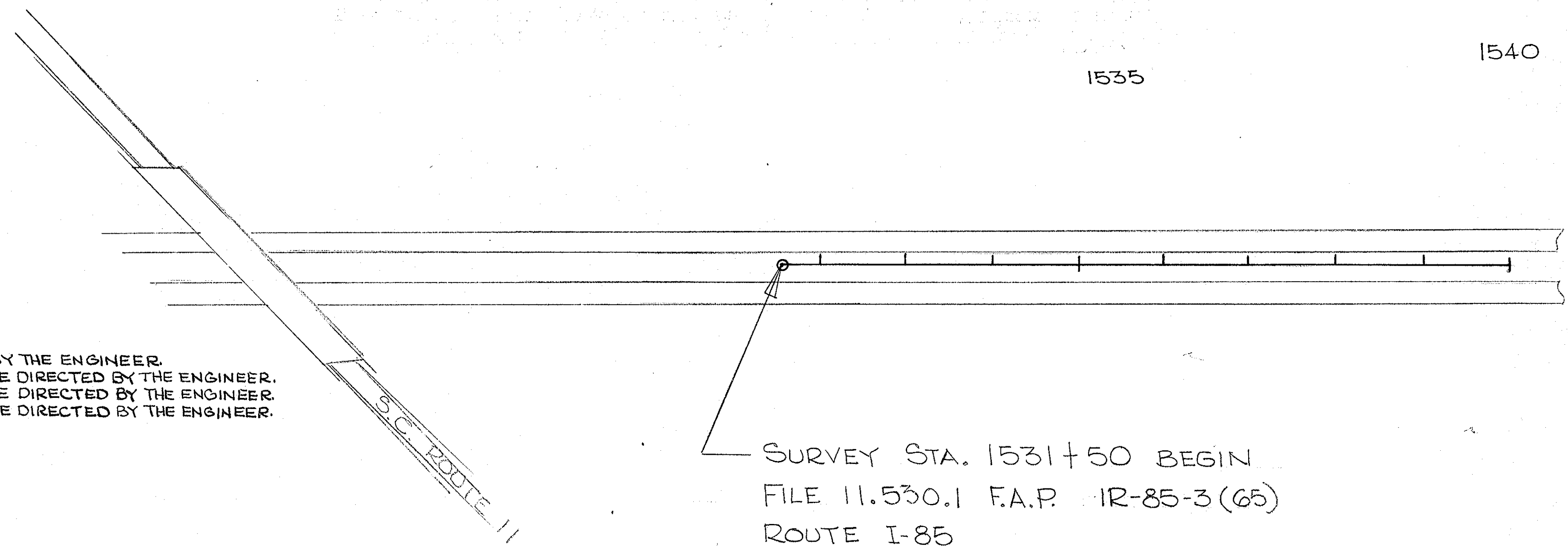
APPROVED _____ DATE _____
STATE HIGHWAY ENGINEER

STANDARD DESIGN
CATCH BASIN
TYPE 12
DRAWING NO. 720-12

CHANGES INVOLVING INCREASED COST OF PROJECT OR CHANGES IN ALIGNMENT MUST BE SPECIFICALLY AUTHORIZED BY THE STATE HIGHWAY ENGINEER. DISTRICT ENGINEER MAY AUTHORIZE MINOR ALTERATIONS NOT IN CONFLICT WITH THE STANDARD PRACTICES OF THE DEPARTMENT AND NOT INVOLVING INCREASES IN COST. FORWARD INFORMATION ON ANY CHANGE IN ALIGNMENT TO THE COLUMBIA OFFICE AS SOON AS THE REVISION IS COMPLETED.

THE FOLLOWING QUANTITIES ARE NOT SHOWN IN DETAIL ON THE PLANS BUT ARE INCLUDED IN THE SUMMARY OF ESTIMATED QUANTITIES AND MAY BE VARIED DURING CONSTRUCTION AS DIRECTED BY THE ENGINEER:

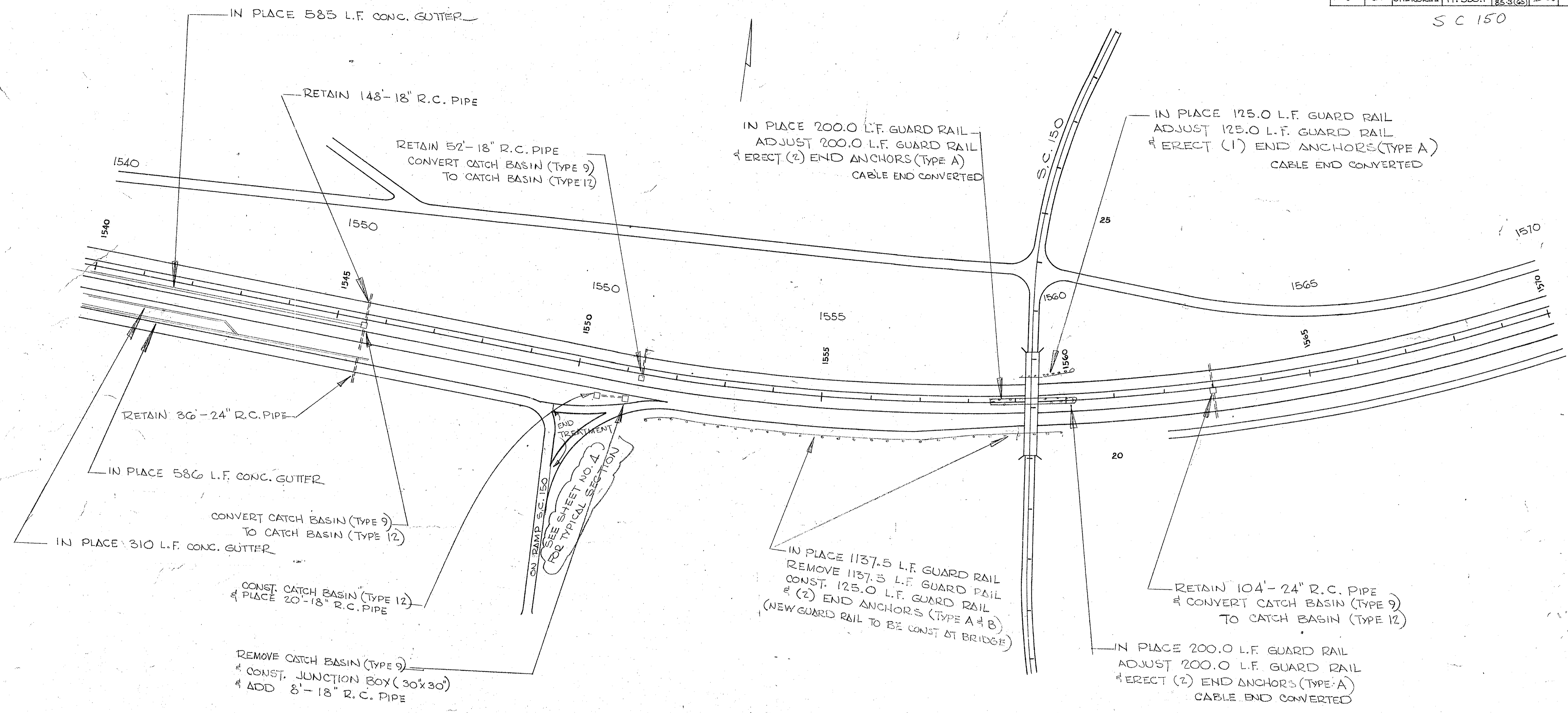
| | | |
|--|---------|--|
| BREAKING AND RETAINING OF EXISTING CONCRETE GUTTER | 7,100 | SY. WHERE DIRECTED BY THE ENGINEER. |
| REMOVAL OF EXISTING CONCRETE GUTTER | 7,100 | SY. WHERE DIRECTED BY THE ENGINEER. |
| UNCLASSIFIED EXCAVATION | 4,000 | C.Y. FOR CATH BASINS WHERE DIRECTED BY THE ENGINEER. |
| ASPHALT CEMENT IN PAVING MIXTURE | 511 | TON FOR LEVELING EXISTING PAVEMENT WHERE DIRECTED BY THE ENGINEER. |
| ASPHALTIC CONCRETE BINDER COURSE (TYPE 2) | 2,010 | TON FOR LEVELING EXISTING PAVEMENT WHERE DIRECTED BY THE ENGINEER. |
| ASPHALTIC CONCRETE SURFACE COURSE (TYPE 3) | 6,835 | TON FOR LEVELING EXISTING PAVEMENT WHERE DIRECTED BY THE ENGINEER. |
| ASPHALTIC CONCRETE SURFACE COURSE FOR DITCH PAVING (TYPE 3) | 1,125 | TON WHERE DIRECTED BY THE ENGINEER. |
| NYLON FILAMENT MATTING (FOR DITCH PAVING - 8' WIDE) | 3,150 | SY. WHERE DIRECTED BY THE ENGINEER. |
| FIBER GLASS ROVING (FOR DITCH PAVING - 8' WIDE) | 3,150 | SY. WHERE DIRECTED BY THE ENGINEER. |
| PART DEPTH ASPHALT PATCHING | 27,500 | S.F. WHERE DIRECTED BY THE ENGINEER. |
| FULL DEPTH ASPHALT PAVEMENT PATCHING | 6,000 | S.Y. WHERE DIRECTED BY THE ENGINEER. |
| CRUSHER RUN STONE (CCR-14) | 1,800 | TON WHERE DIRECTED BY THE ENGINEER. |
| EMULSIFIED ASPHALT FOR CRACK SEALING | 10,500 | GAL WHERE DIRECTED BY THE ENGINEER. |
| 15" R.C. PIPE CULVERT (CLASS III) | 200 | L.F. WHERE DIRECTED BY THE ENGINEER. |
| 18" R.C. PIPE CULVERT (CLASS III) | 200 | L.F. WHERE DIRECTED BY THE ENGINEER. |
| 24" R.C. PIPE CULVERT (CLASS III) | 100 | L.F. WHERE DIRECTED BY THE ENGINEER. |
| 30" R.C. PIPE CULVERT (CLASS III) | 100 | L.F. WHERE DIRECTED BY THE ENGINEER. |
| CATCH BASIN TYPE NO. 12 | 10 | EA. WHERE DIRECTED BY THE ENGINEER. |
| STEEL BEAM GUARD RAIL | 3,350.0 | L.F. WHERE DIRECTED BY THE ENGINEER. |
| REMOVAL OF EXISTING GUARD RAIL | 1,550.0 | L.F. WHERE DIRECTED BY THE ENGINEER. |
| 4" CORRUGATED PIPE UNDERDRAIN | 34,400 | L.F. WHERE DIRECTED BY THE ENGINEER. |
| HAY BALES | 1100 | EA. WHERE DIRECTED BY THE ENGINEER. |
| SEEDING (MULCHED) | 352,856 | MSY. FOR SHOULDERS AND SLOPES WHERE DIRECTED BY THE ENGINEER. |
| FERTILIZER (4-12-12) | 36.42 | TON FOR SHOULDERS AND SLOPES WHERE DIRECTED BY THE ENGINEER. |
| LIME | 72.84 | TON FOR SHOULDERS AND SLOPES WHERE DIRECTED BY THE ENGINEER. |
| NITROGEN | 3,496 | LBS. FOR SHOULDERS AND SLOPES WHERE DIRECTED BY THE ENGINEER. |
| GUARD RAIL POST (ADDITION LENGTHS) | 1,000 | L.F. WHERE DIRECTED BY THE ENGINEER. |
| STOCK PILE CATCH BASIN (TYPE 9) TOPS AT LOCATION DESIGNATED BY THE ENGINEER. | | |



SURVEY STA. 1531+50 BEGIN
 FILE 11.530.1 F.A.P. IR-85-3(65)
 ROUTE I-85

| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | 12.853(65) | I-85 | 11 | 60 |

SC 150



IN PLACE 585 L.F. CONC. GUTTER

RETAIN 148'-18" R.C. PIPE

RETAIN 52'-18" R.C. PIPE
 CONVERT CATCH BASIN (TYPE 9)
 TO CATCH BASIN (TYPE 12)

IN PLACE 200.0 L.F. GUARD RAIL
 ADJUST 200.0 L.F. GUARD RAIL
 & ERECT (2) END ANCHORS (TYPE A)
 CABLE END CONVERTED

IN PLACE 125.0 L.F. GUARD RAIL
 ADJUST 125.0 L.F. GUARD RAIL
 & ERECT (1) END ANCHORS (TYPE A)
 CABLE END CONVERTED

RETAIN 36'-24" R.C. PIPE

IN PLACE 586 L.F. CONC. GUTTER

CONVERT CATCH BASIN (TYPE 9)
 TO CATCH BASIN (TYPE 12)

IN PLACE 310 L.F. CONC. GUTTER

CONST. CATCH BASIN (TYPE 12)
 & PLACE 20'-18" R.C. PIPE

REMOVE CATCH BASIN (TYPE 9)
 & CONST. JUNCTION BOX (30'x30')
 & ADD 8'-18" R.C. PIPE

ON RAMP S.C. 150
 SEE SHEET NO. 4
 FOR TYPICAL SECTION

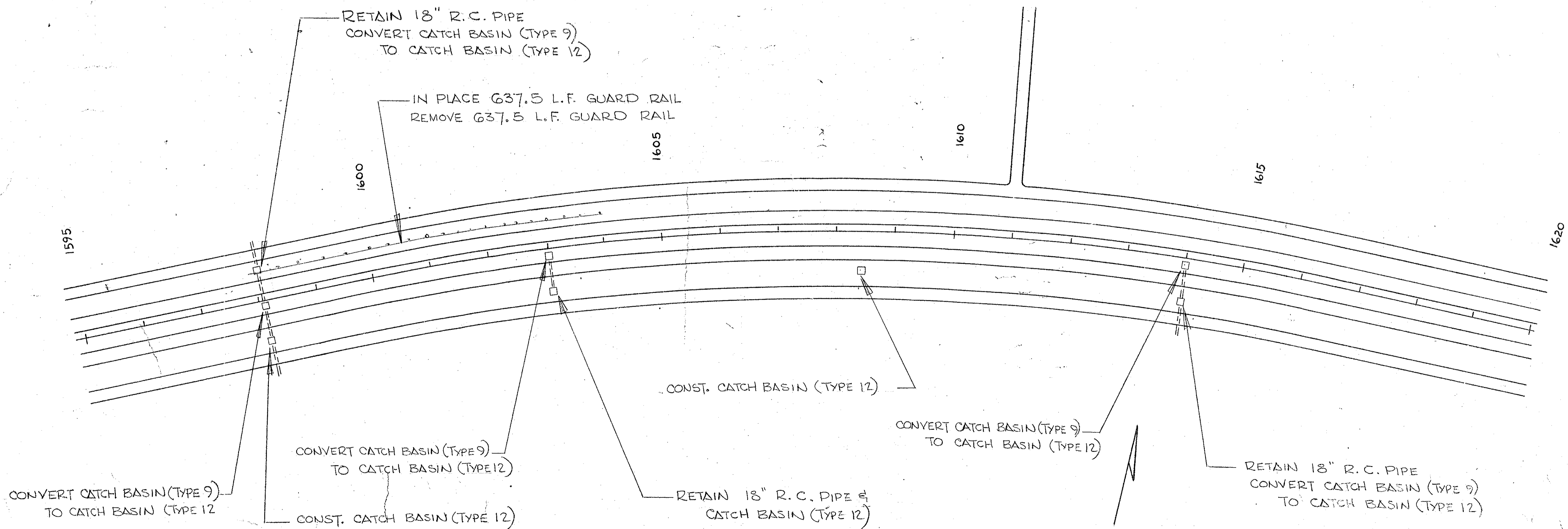
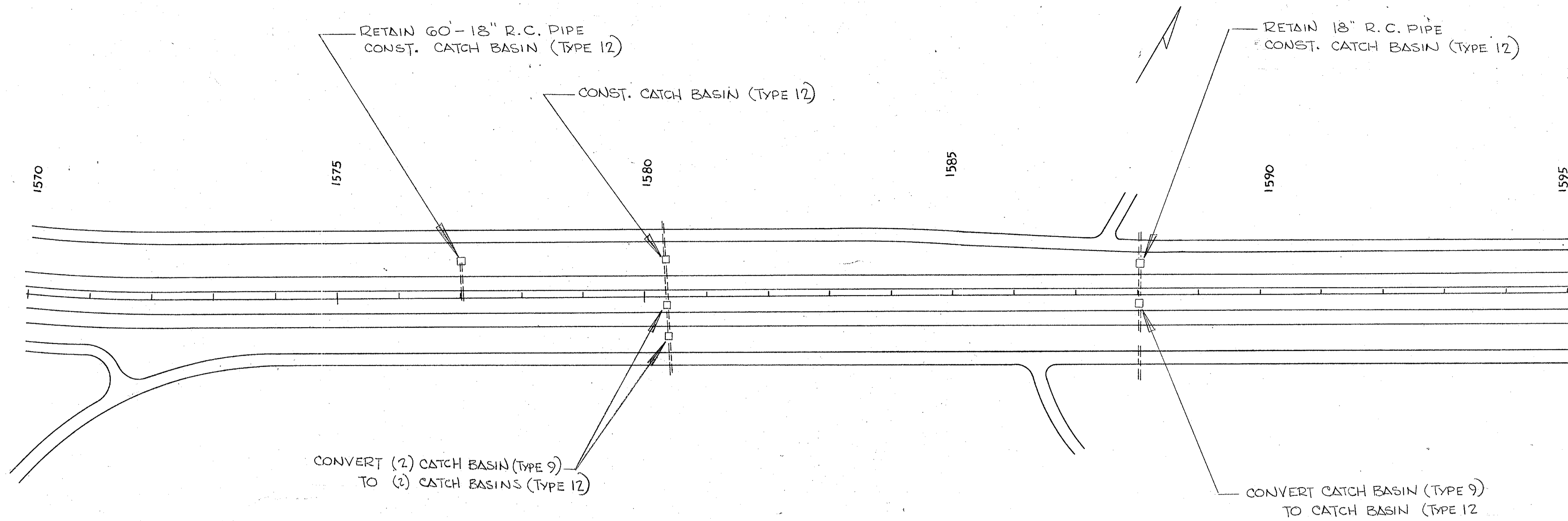
IN PLACE 1137.5 L.F. GUARD RAIL
 REMOVE 1137.3 L.F. GUARD RAIL
 CONST. 125.0 L.F. GUARD RAIL
 & (2) END ANCHORS (TYPE A & B)
 (NEW GUARD RAIL TO BE CONST. AT BRIDGE)

RETAIN 104'-24" R.C. PIPE
 & CONVERT CATCH BASIN (TYPE 9)
 TO CATCH BASIN (TYPE 12)

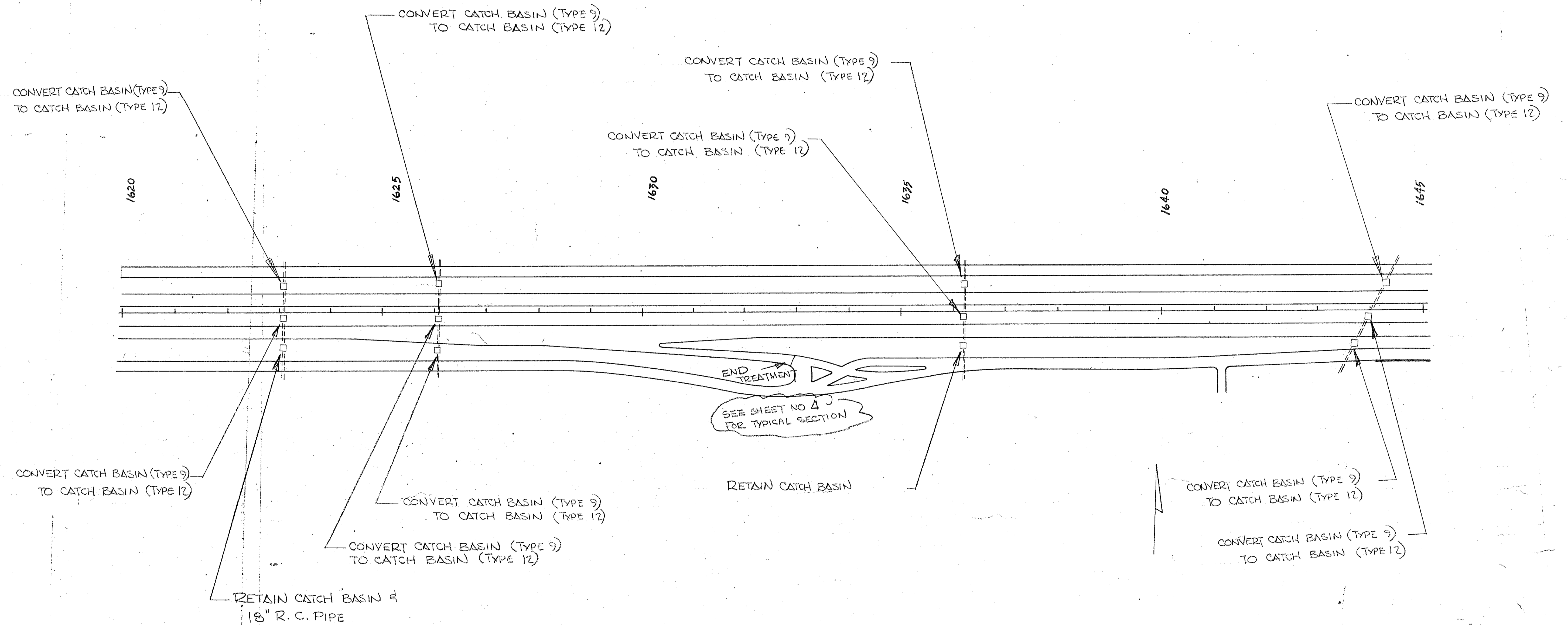
IN PLACE 200.0 L.F. GUARD RAIL
 ADJUST 200.0 L.F. GUARD RAIL
 & ERECT (2) END ANCHORS (TYPE A)
 CABLE END CONVERTED

LEG ON ROUTE 150

| FED. ROAD DIV. NO. | STATE | COUNTY | OBJECT NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | 12-85-3 (6) | 1-85 | 12 | 60 |

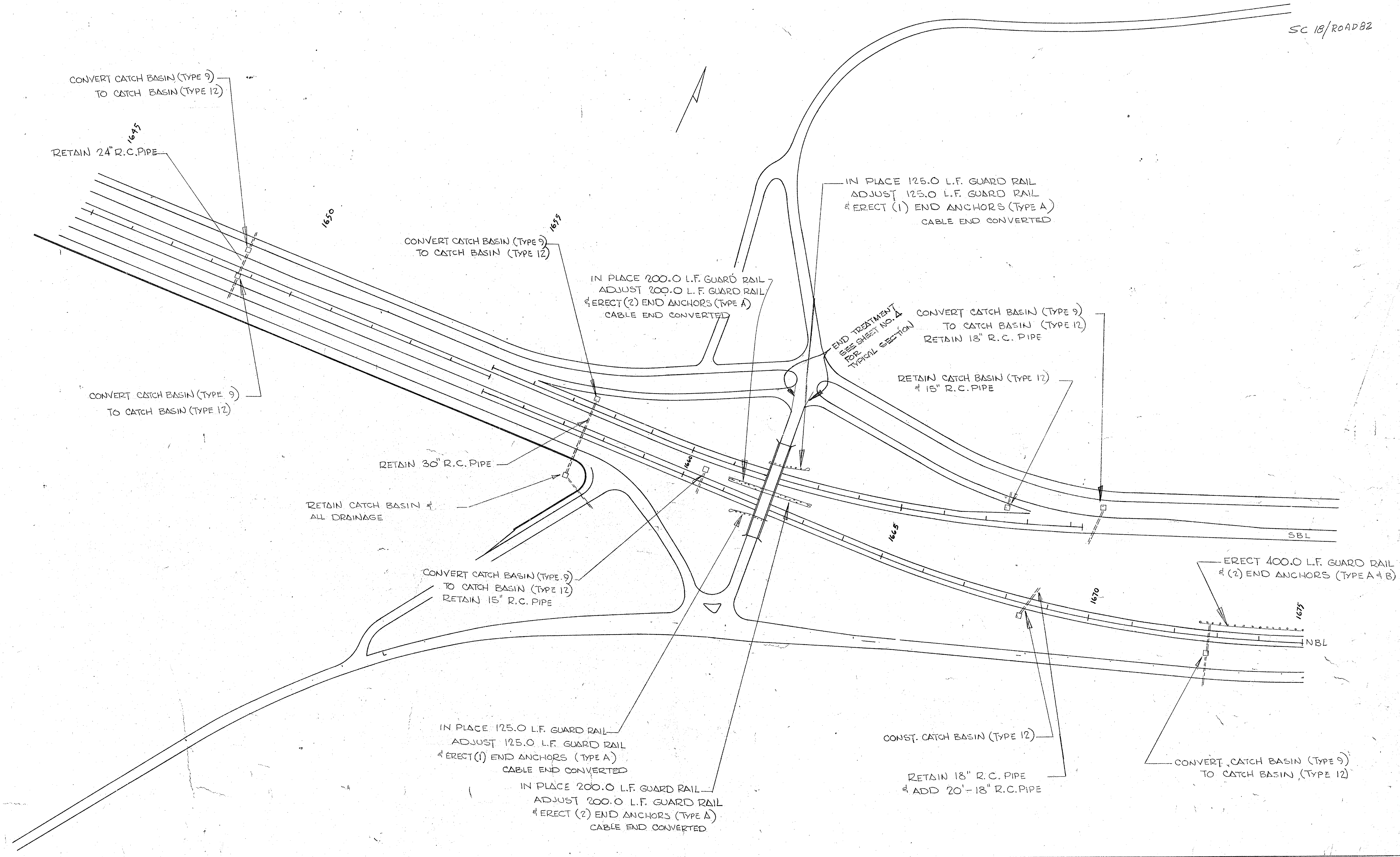


| FED. ROAD DIV. NO. | STATE | COUNTY | DECKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11,530.1 | 12 85-3(68) | 1-85 | 13 | 60 |



| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11,530.1 | 12-85-2(6) | I-85 | 14 | 60 |

SC 18/ROAD 82



CONVERT CATCH BASIN (TYPE 9)
TO CATCH BASIN (TYPE 12)

RETAIN 24" R.C. PIPE

1650

CONVERT CATCH BASIN (TYPE 9)
TO CATCH BASIN (TYPE 12)

IN PLACE 200.0 L.F. GUARD RAIL
ADJUST 200.0 L.F. GUARD RAIL
& ERECT (2) END ANCHORS (TYPE A)
CABLE END CONVERTED

IN PLACE 125.0 L.F. GUARD RAIL
ADJUST 125.0 L.F. GUARD RAIL
& ERECT (1) END ANCHORS (TYPE A)
CABLE END CONVERTED

END TREATMENT
SEE SHEET NO. 1
FOR
TYPICAL SECTION

CONVERT CATCH BASIN (TYPE 9)
TO CATCH BASIN (TYPE 12)
RETAIN 18" R.C. PIPE

CONVERT CATCH BASIN (TYPE 9)
TO CATCH BASIN (TYPE 12)

RETAIN 30" R.C. PIPE

RETAIN CATCH BASIN &
ALL DRAINAGE

RETAIN CATCH BASIN (TYPE 12)
& 15" R.C. PIPE

CONVERT CATCH BASIN (TYPE 9)
TO CATCH BASIN (TYPE 12)
RETAIN 15" R.C. PIPE

ERECT 400.0 L.F. GUARD RAIL
& (2) END ANCHORS (TYPE A & B)

IN PLACE 125.0 L.F. GUARD RAIL
ADJUST 125.0 L.F. GUARD RAIL
& ERECT (1) END ANCHORS (TYPE A)
CABLE END CONVERTED

IN PLACE 200.0 L.F. GUARD RAIL
ADJUST 200.0 L.F. GUARD RAIL
& ERECT (2) END ANCHORS (TYPE A)
CABLE END CONVERTED

CONST. CATCH BASIN (TYPE 12)

RETAIN 18" R.C. PIPE
& ADD 20'-18" R.C. PIPE

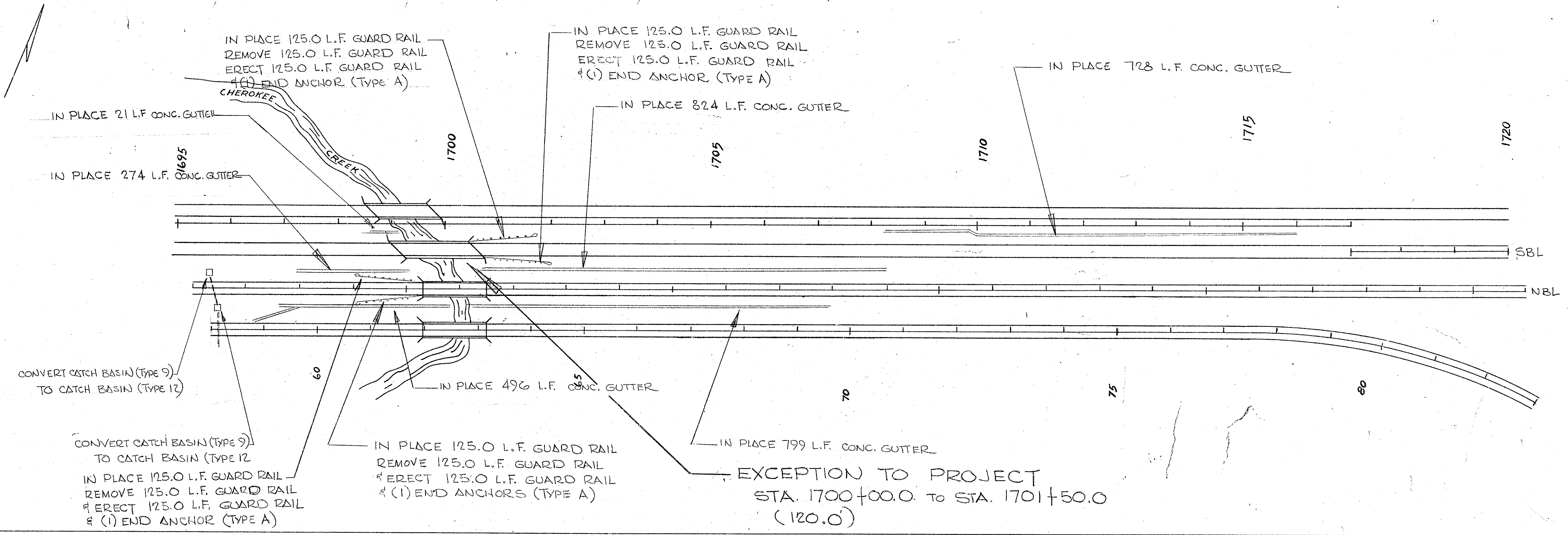
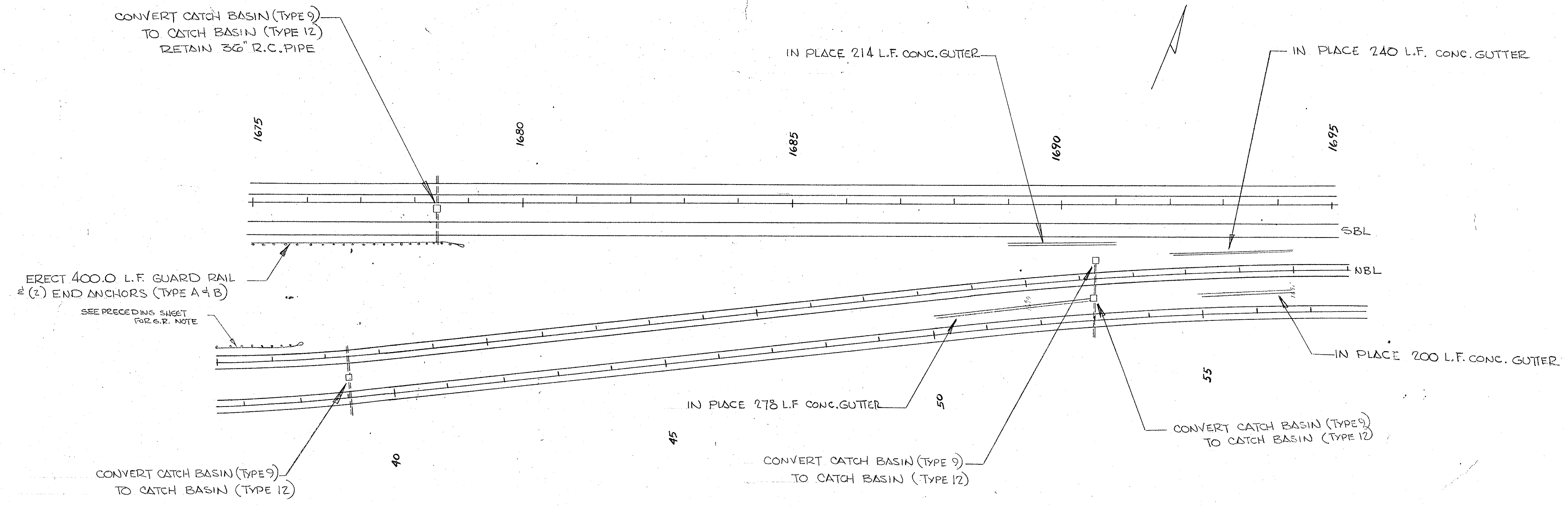
CONVERT CATCH BASIN (TYPE 9)
TO CATCH BASIN (TYPE 12)

SBL

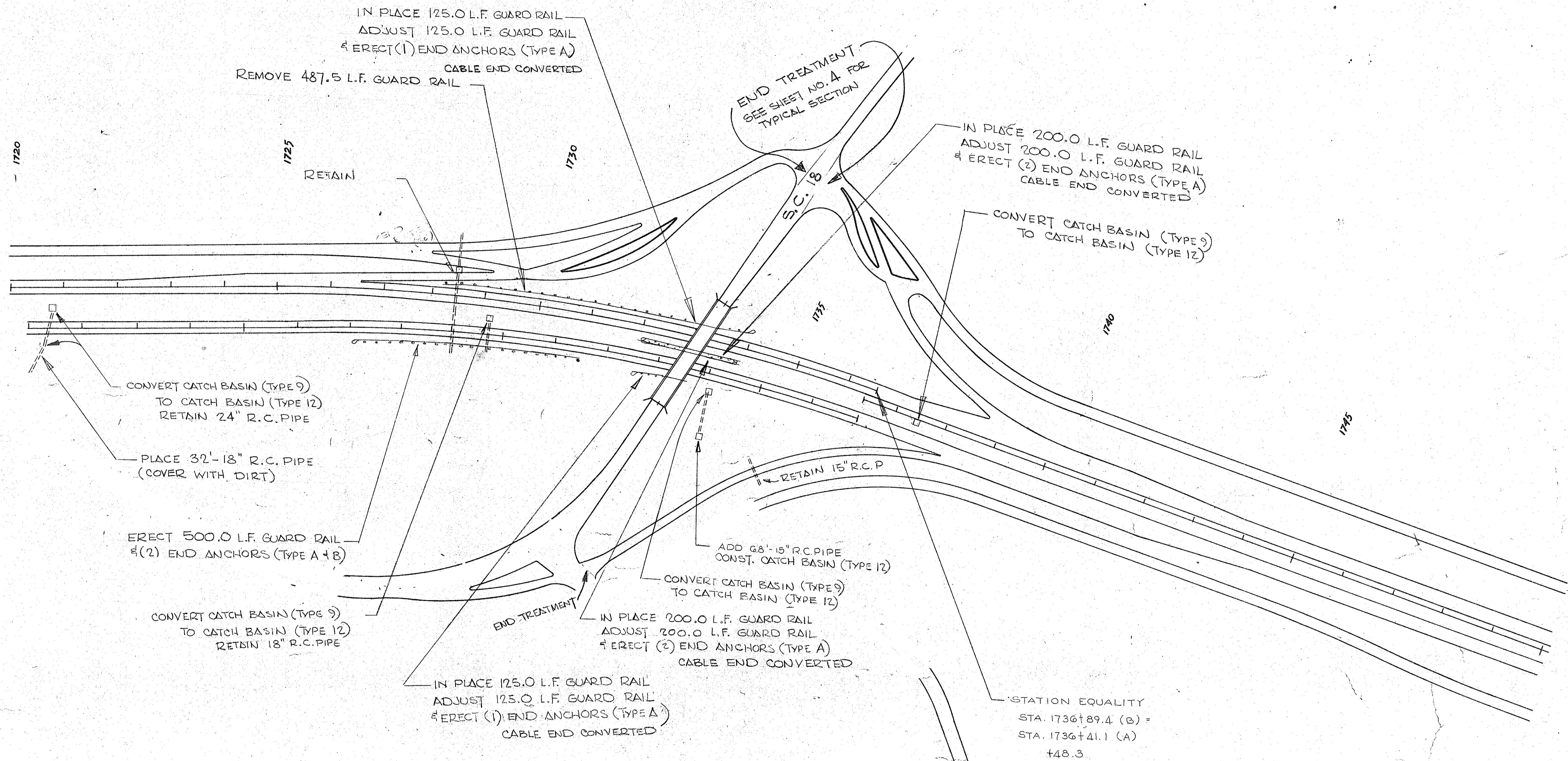
NBL



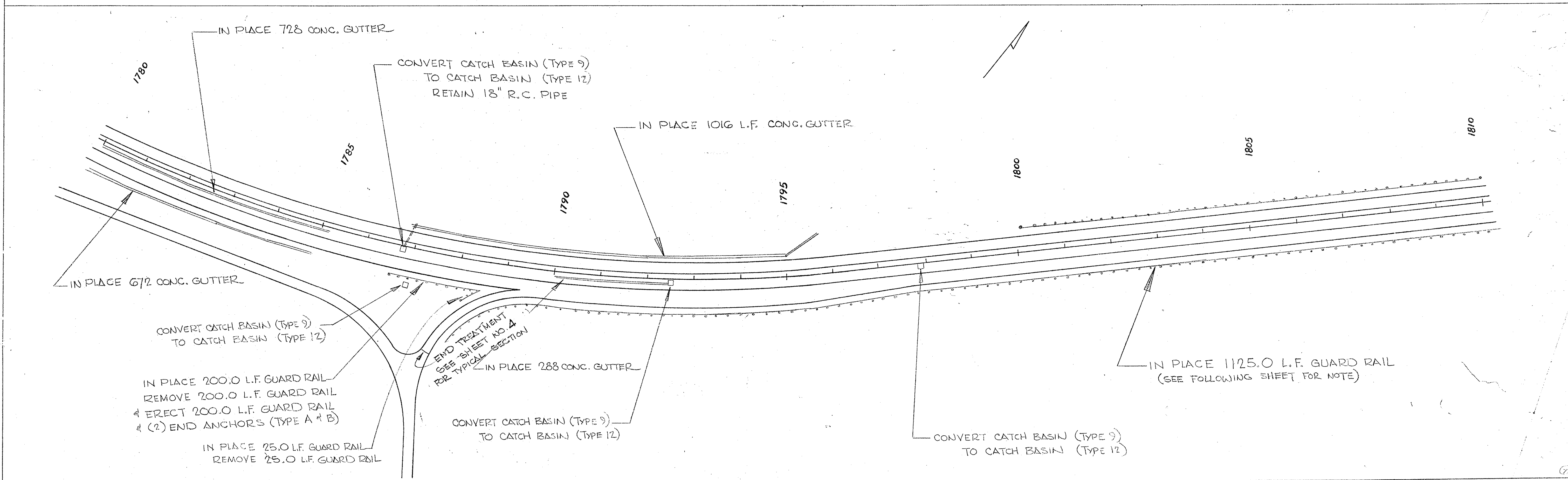
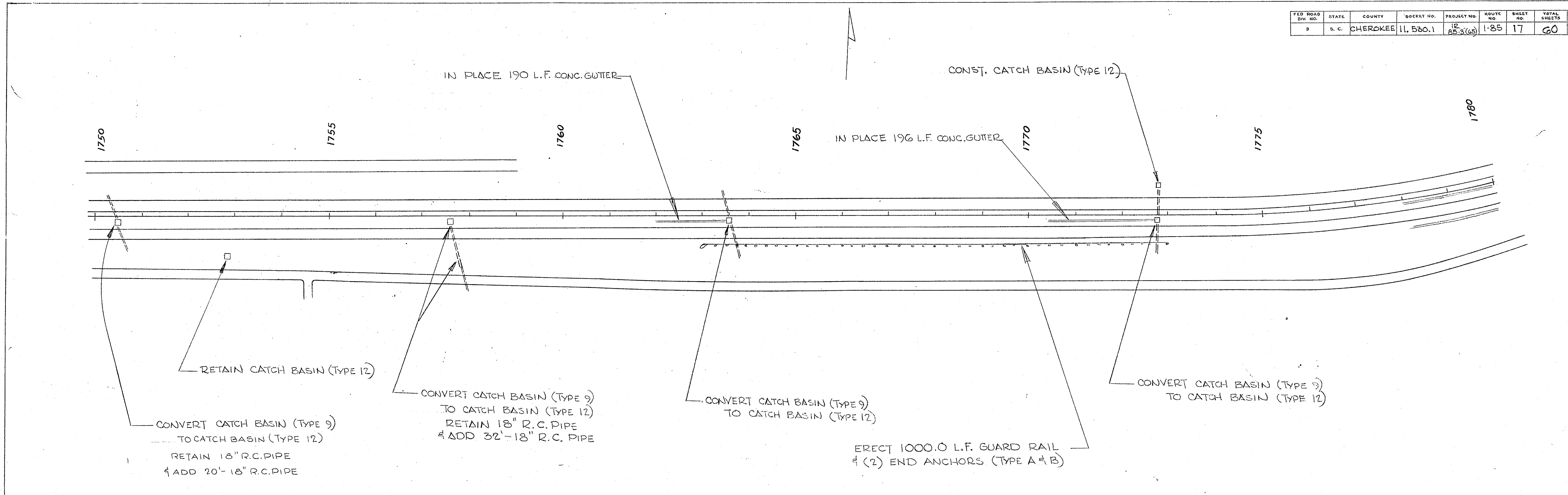
| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | 12-85-3(45) | 1-35 | 15 | 60 |



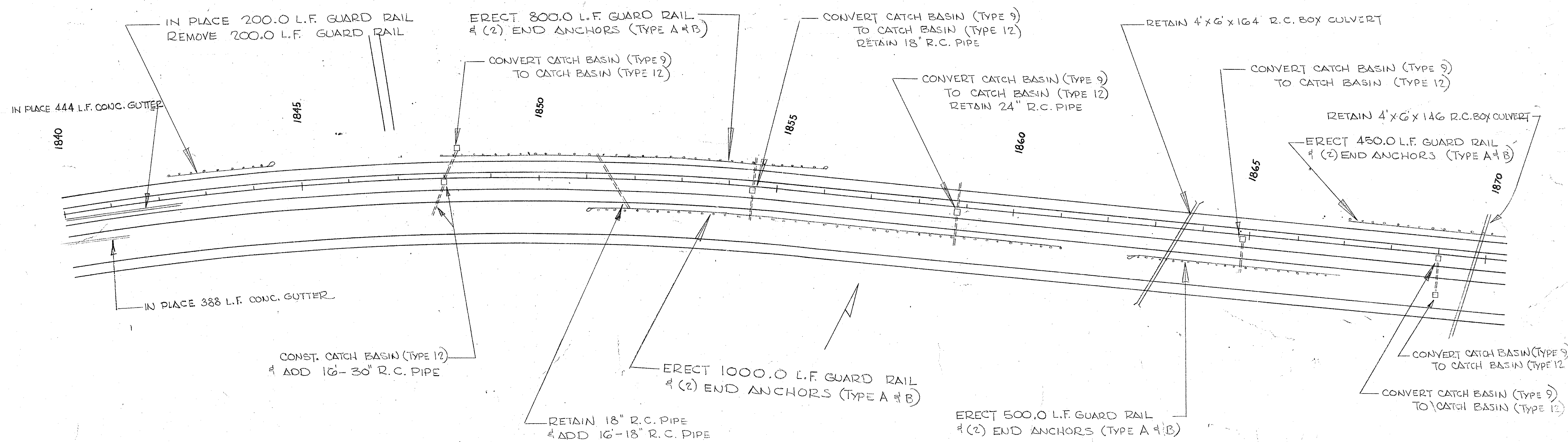
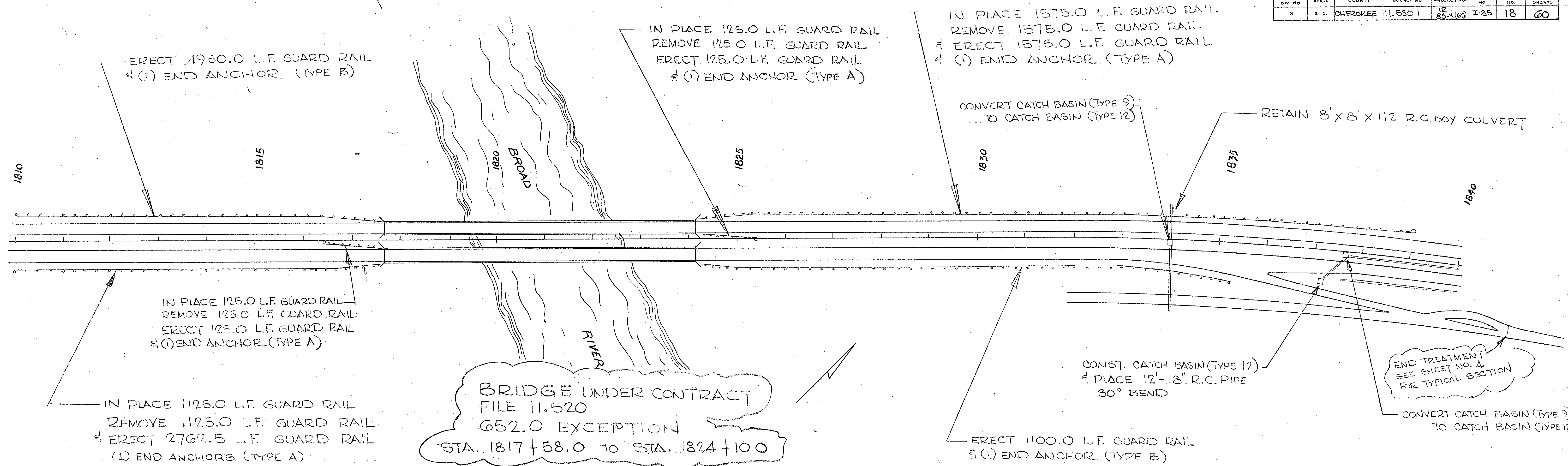
| FED. ROAD DIV. NO. | STATE | COUNTY | DISTRICT NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|--------------|--------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | 1E. 85.3(65) | 1-85 | 16 | 60 |



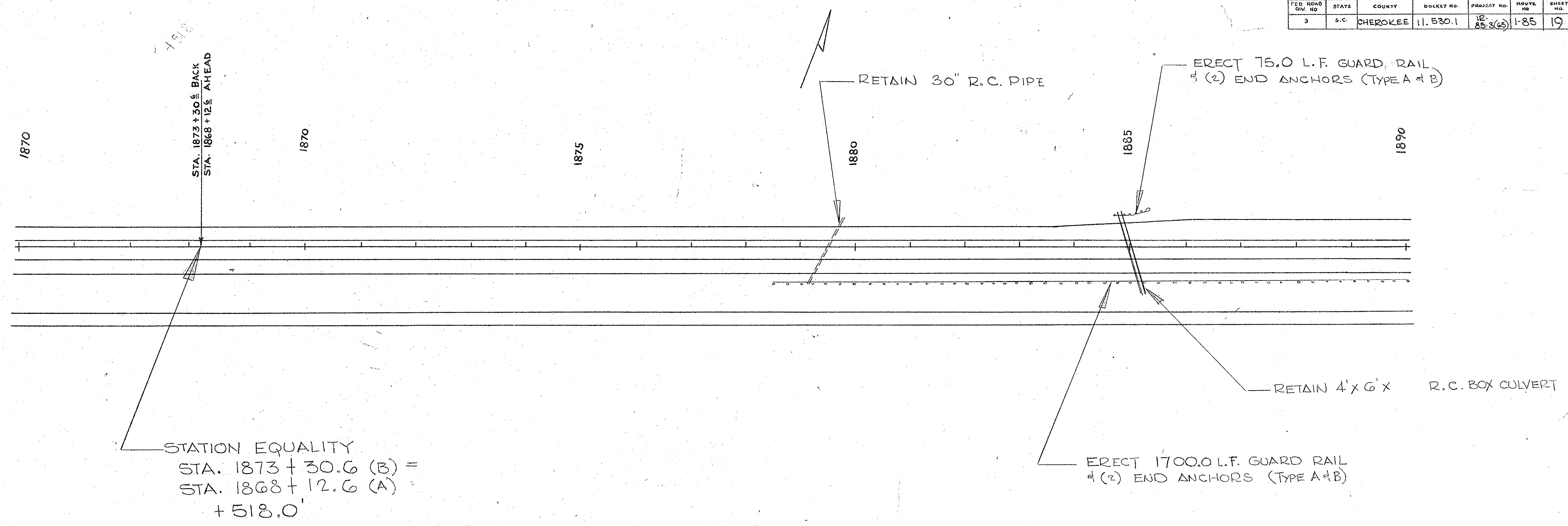
| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|---------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11, 530.1 | 12 853(45) | 1-85 | 17 | 60 |



| FED. ROAD DIST. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11,530.1 | 12 85-3(69) | I-85 | 18 | 60 |



| FED. ROAD DIV. NO. | STATE | COUNTY | DISTRICT NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|--------------|--------------|-----------|-----------|--------------|
| 3 | S.C. | CHEROKEE | 11, 530.1 | 12, 85.3(65) | 1-85 | 19 | 60 |



STATION EQUALITY
 STA. 1873 + 30.6 (B) =
 STA. 1868 + 12.6 (A) +
 + 518.0'

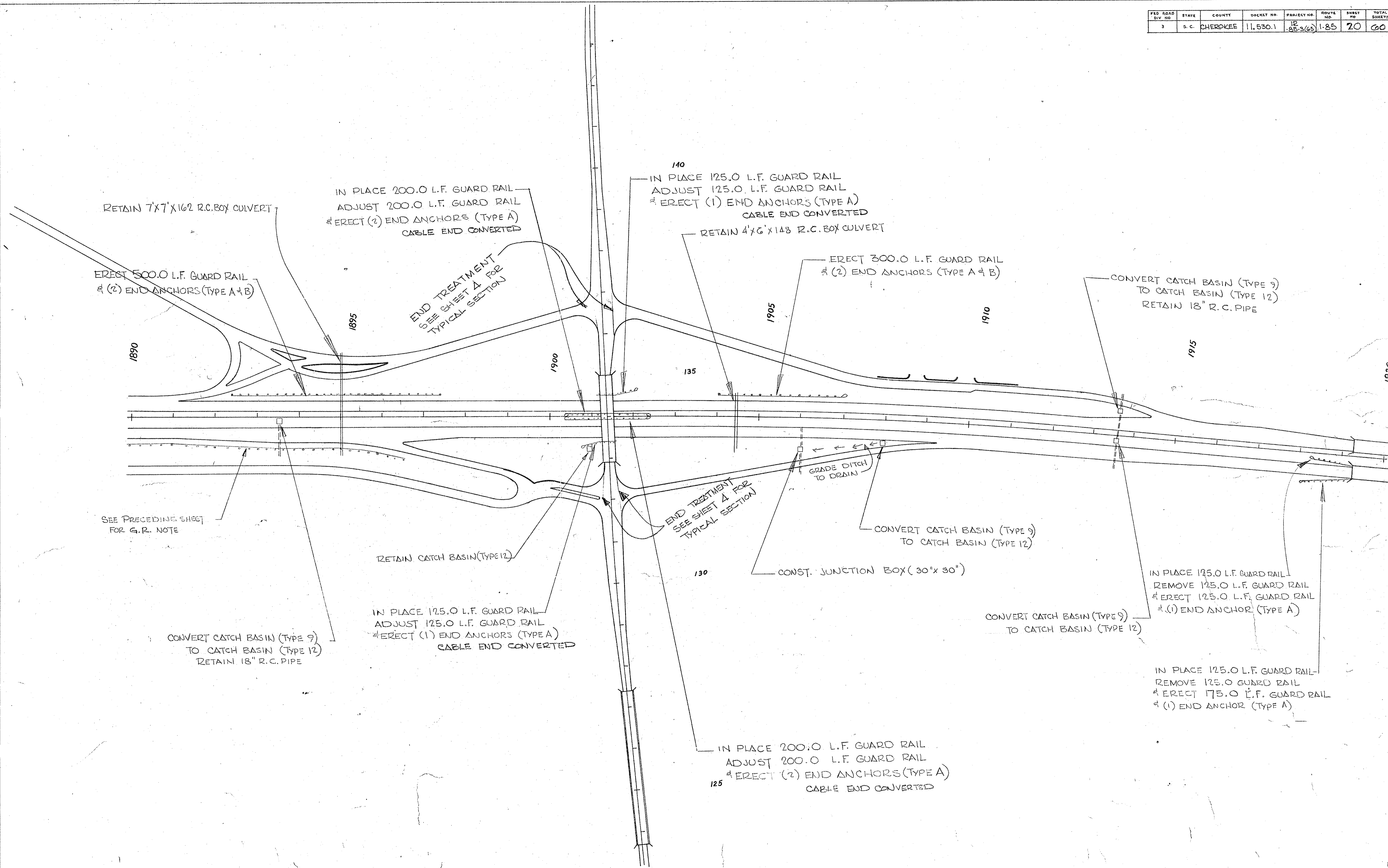
ERECT 1700.0 L.F. GUARD RAIL
 # (2) END ANCHORS (TYPE A+B)

RETAIN 4' x 6' x R.C. BOX CULVERT

RETAIN 30" R.C. PIPE

ERECT 75.0 L.F. GUARD RAIL
 # (2) END ANCHORS (TYPE A+B)

| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11,530.1 | 12-85-3(65) | 1-85 | 20 | 60 |



RETAIN 7'x7'x16'2 R.C. BOX CULVERT

IN PLACE 200.0 L.F. GUARD RAIL
ADJUST 200.0 L.F. GUARD RAIL
& ERECT (2) END ANCHORS (TYPE A)
CABLE END CONVERTED

140
IN PLACE 125.0 L.F. GUARD RAIL
ADJUST 125.0 L.F. GUARD RAIL
& ERECT (1) END ANCHORS (TYPE A)
CABLE END CONVERTED

RETAIN 4'x6'x14'8 R.C. BOX CULVERT

ERECT 500.0 L.F. GUARD RAIL
& (2) END ANCHORS (TYPE A & B)

END TREATMENT
SEE SHEET 4 FOR
TYPICAL SECTION

ERECT 300.0 L.F. GUARD RAIL
& (2) END ANCHORS (TYPE A & B)

CONVERT CATCH BASIN (TYPE 9)
TO CATCH BASIN (TYPE 12)
RETAIN 18" R.C. PIPE

SEE PRECEDING SHEET
FOR G.R. NOTE

END TREATMENT
SEE SHEET 4 FOR
TYPICAL SECTION

CONVERT CATCH BASIN (TYPE 9)
TO CATCH BASIN (TYPE 12)

CONST. JUNCTION BOX (30"x30")

CONVERT CATCH BASIN (TYPE 9)
TO CATCH BASIN (TYPE 12)
RETAIN 18" R.C. PIPE

IN PLACE 125.0 L.F. GUARD RAIL
ADJUST 125.0 L.F. GUARD RAIL
& ERECT (1) END ANCHORS (TYPE A)
CABLE END CONVERTED

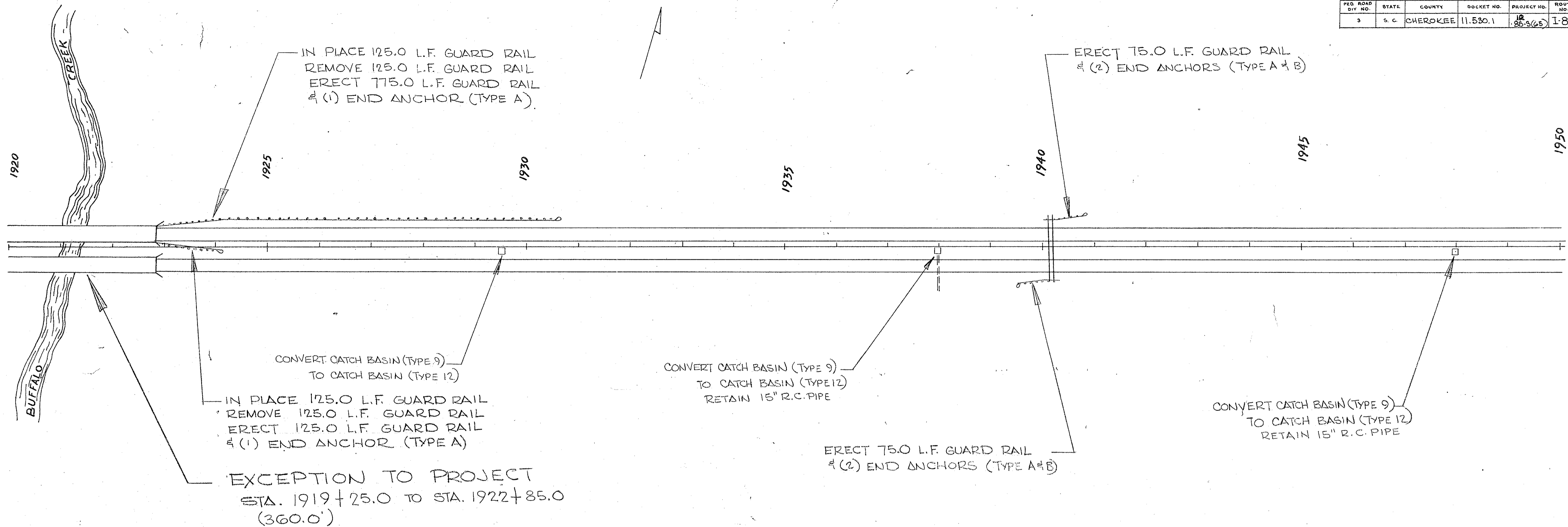
CONVERT CATCH BASIN (TYPE 9)
TO CATCH BASIN (TYPE 12)

IN PLACE 125.0 L.F. GUARD RAIL
REMOVE 125.0 L.F. GUARD RAIL
& ERECT 125.0 L.F. GUARD RAIL
& (1) END ANCHOR (TYPE A)

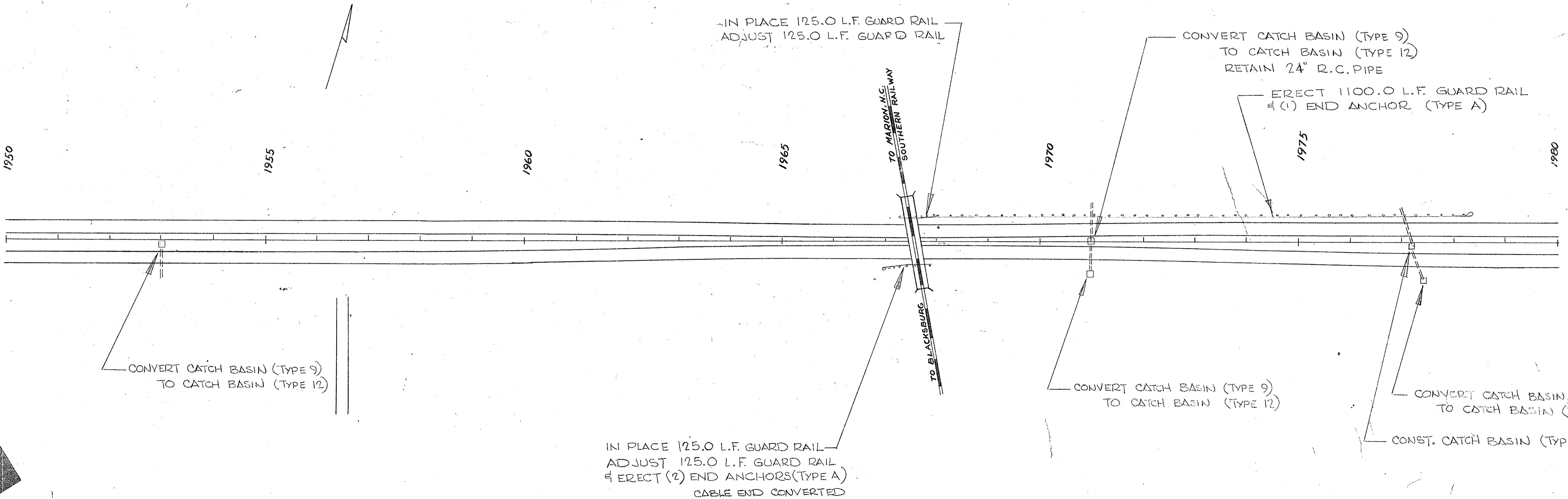
IN PLACE 125.0 L.F. GUARD RAIL
REMOVE 125.0 L.F. GUARD RAIL
& ERECT 175.0 L.F. GUARD RAIL
& (1) END ANCHOR (TYPE A)

125
IN PLACE 200.0 L.F. GUARD RAIL
ADJUST 200.0 L.F. GUARD RAIL
& ERECT (2) END ANCHORS (TYPE A)
CABLE END CONVERTED

| FED. ROAD DIST. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------|------------|----------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11,530.1 | 1A 85.3(65) | 1-85 | 21 | 60 |

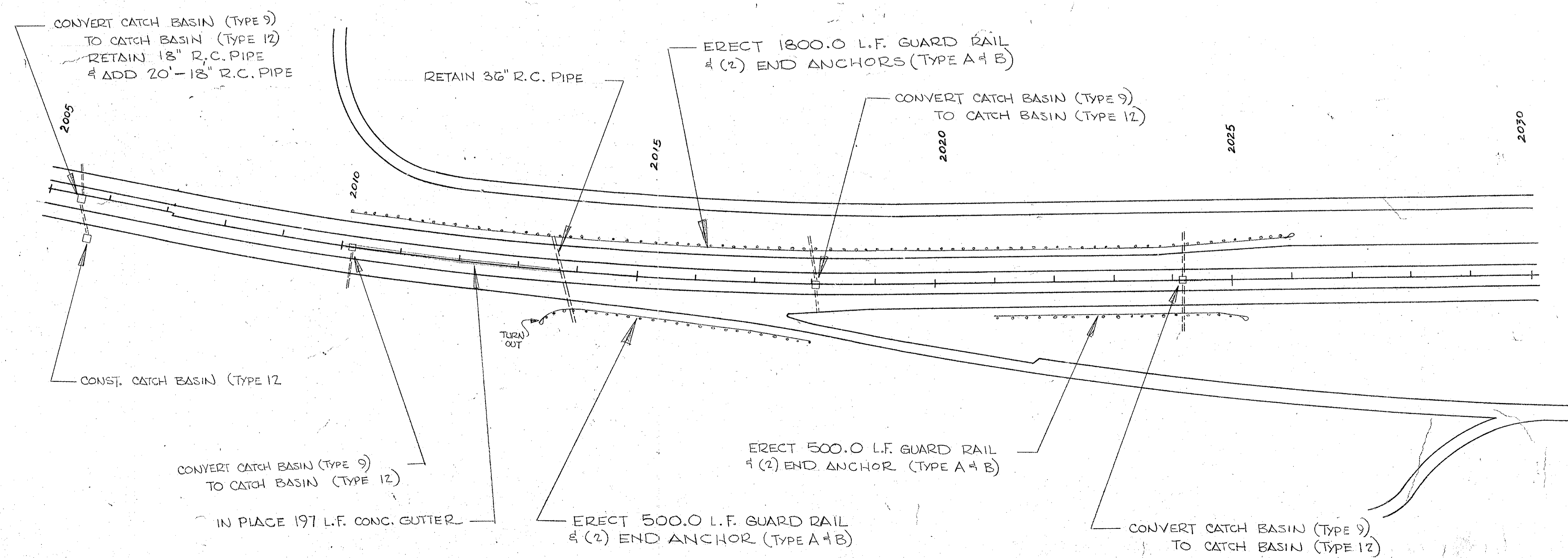
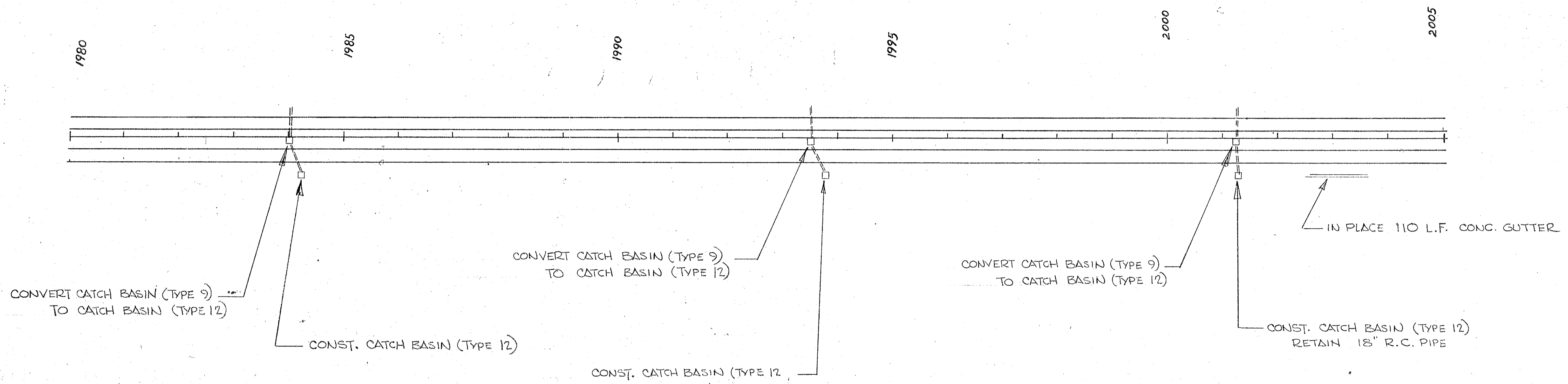


EXCEPTION TO PROJECT
 STA. 1919+25.0 TO STA. 1922+85.0
 (360.0')

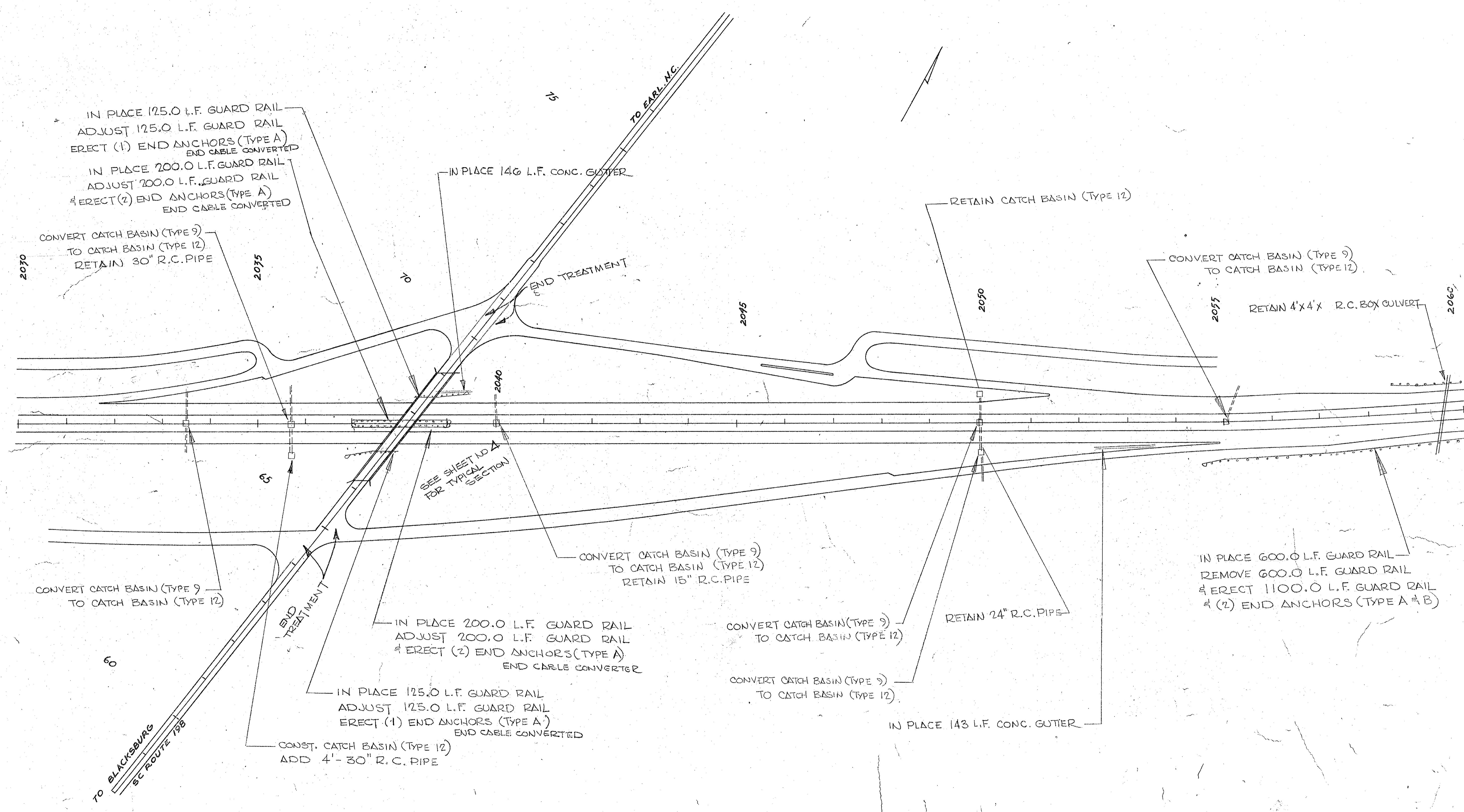


IN PLACE 125.0 L.F. GUARD RAIL
 ADJUST 125.0 L.F. GUARD RAIL
 & ERECT (2) END ANCHORS (TYPE A)
 CABLE END CONVERTED

| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11,530.1 | 12-85-3(65) | 1-85 | 22 | 60 |



| FED. ROAD DIST. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | 12.85.3(63) | 1-85 | 23 | 60 |



IN PLACE 125.0 L.F. GUARD RAIL
 ADJUST 125.0 L.F. GUARD RAIL
 ERECT (1) END ANCHORS (TYPE A)
 END CABLE CONVERTED
 IN PLACE 200.0 L.F. GUARD RAIL
 ADJUST 200.0 L.F. GUARD RAIL
 ERECT (2) END ANCHORS (TYPE A)
 END CABLE CONVERTED

2070
 CONVERT CATCH BASIN (TYPE 9)
 TO CATCH BASIN (TYPE 12).
 RETAIN 30" R.C. PIPE

IN PLACE 146 L.F. CONC. GUTTER

RETAIN CATCH BASIN (TYPE 12)

2055
 CONVERT CATCH BASIN (TYPE 9)
 TO CATCH BASIN (TYPE 12)

RETAIN 4'x4' R.C. BOX CULVERT

CONVERT CATCH BASIN (TYPE 9)
 TO CATCH BASIN (TYPE 12)

CONVERT CATCH BASIN (TYPE 9)
 TO CATCH BASIN (TYPE 12)
 RETAIN 15" R.C. PIPE

IN PLACE 200.0 L.F. GUARD RAIL
 ADJUST 200.0 L.F. GUARD RAIL
 ERECT (2) END ANCHORS (TYPE A)
 END CABLE CONVERTED

CONVERT CATCH BASIN (TYPE 9)
 TO CATCH BASIN (TYPE 12)

RETAIN 24" R.C. PIPE

IN PLACE 600.0 L.F. GUARD RAIL
 REMOVE 600.0 L.F. GUARD RAIL
 ERECT 1100.0 L.F. GUARD RAIL
 ERECT (2) END ANCHORS (TYPE A & B)

IN PLACE 125.0 L.F. GUARD RAIL
 ADJUST 125.0 L.F. GUARD RAIL
 ERECT (1) END ANCHORS (TYPE A)
 END CABLE CONVERTED

CONVERT CATCH BASIN (TYPE 9)
 TO CATCH BASIN (TYPE 12)

IN PLACE 143 L.F. CONC. GUTTER

CONST. CATCH BASIN (TYPE 12)
 ADD 4'-30" R.C. PIPE

TO BLACKSBURG
 SC ROUTE 198

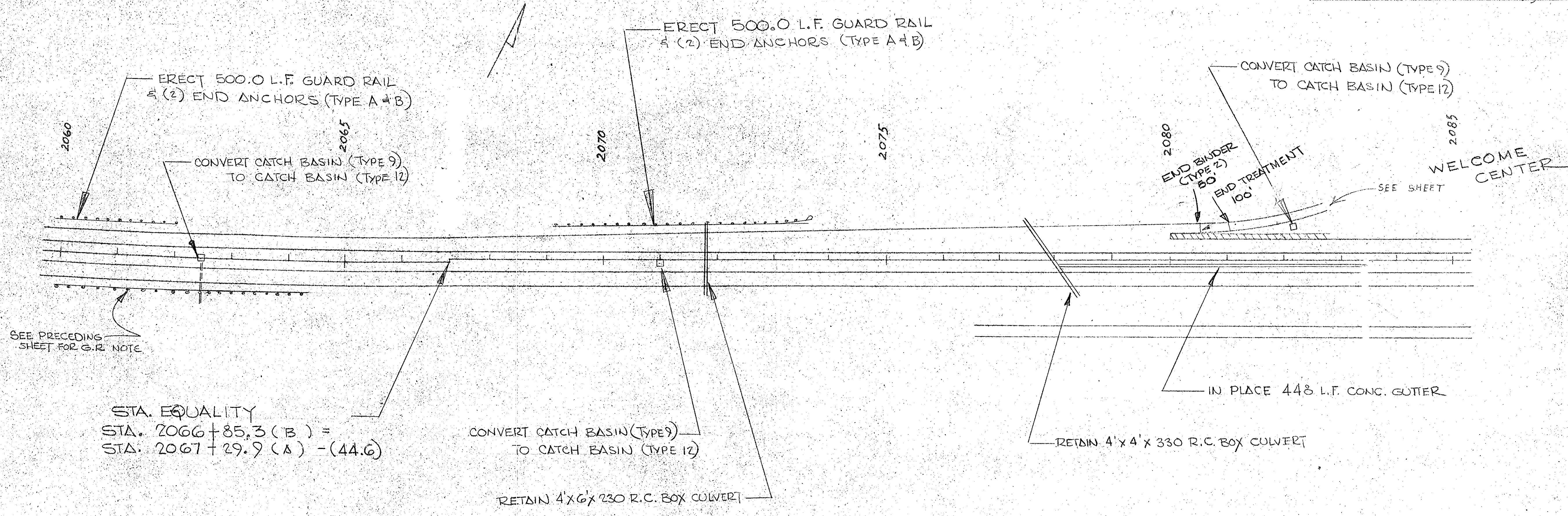
TO EARL, NC.

SEE SHEET NO. A
 FOR TYPICAL
 SECTION

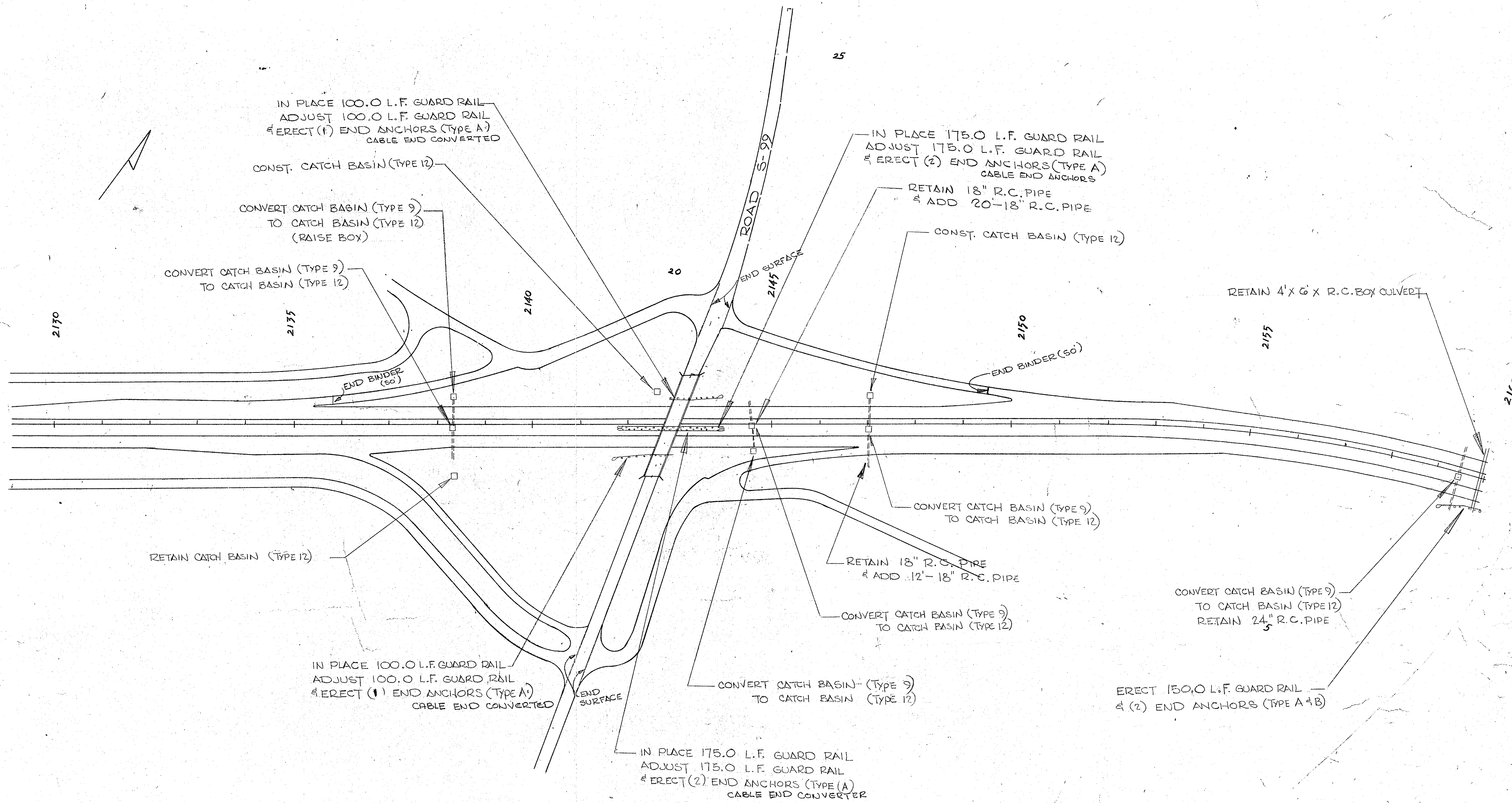
60

12

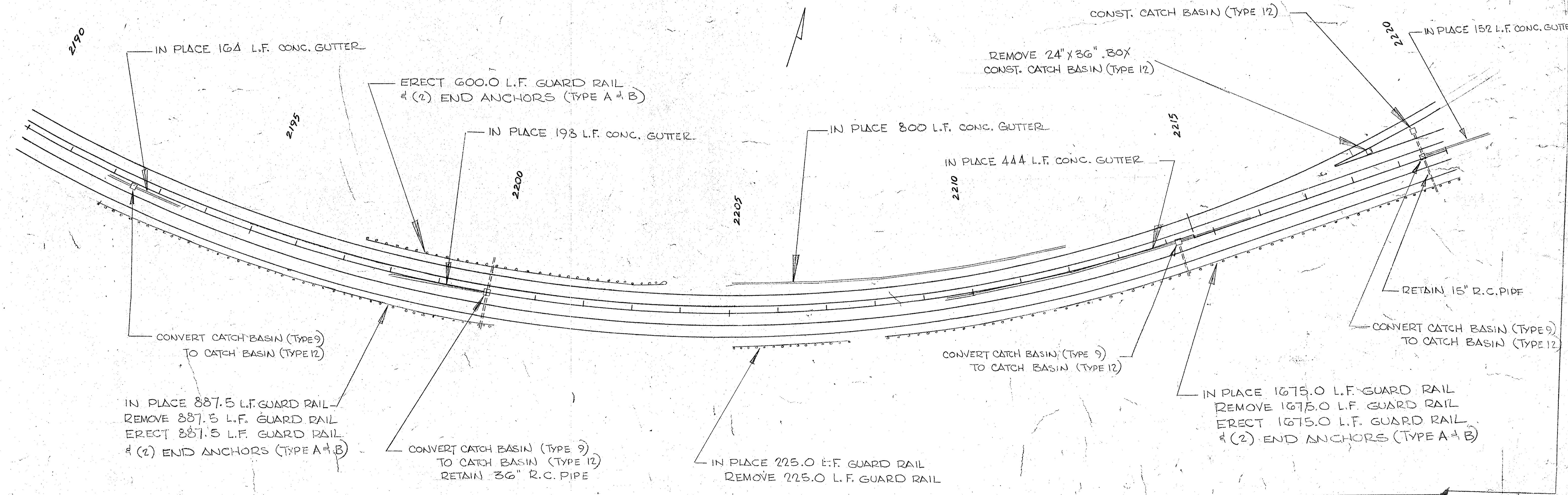
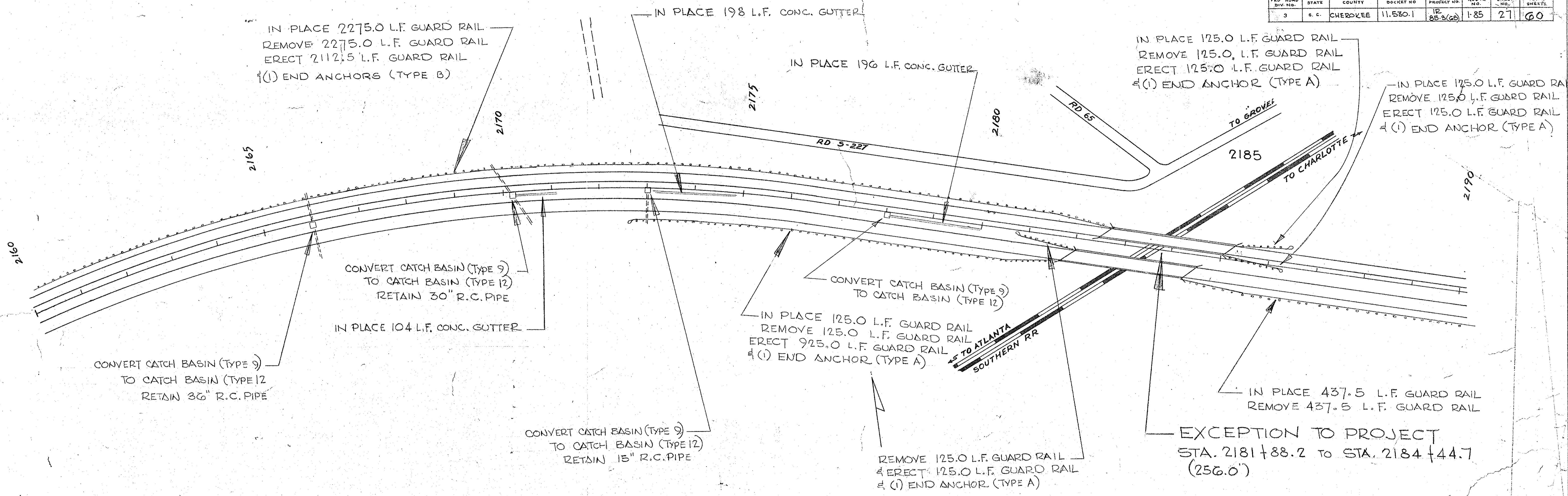
| FED. ROAD DIST. NO. | STATE | COUNTY | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | 12 | 85-2(68) | 1-85 |
| | | | | | 24 | 60 |



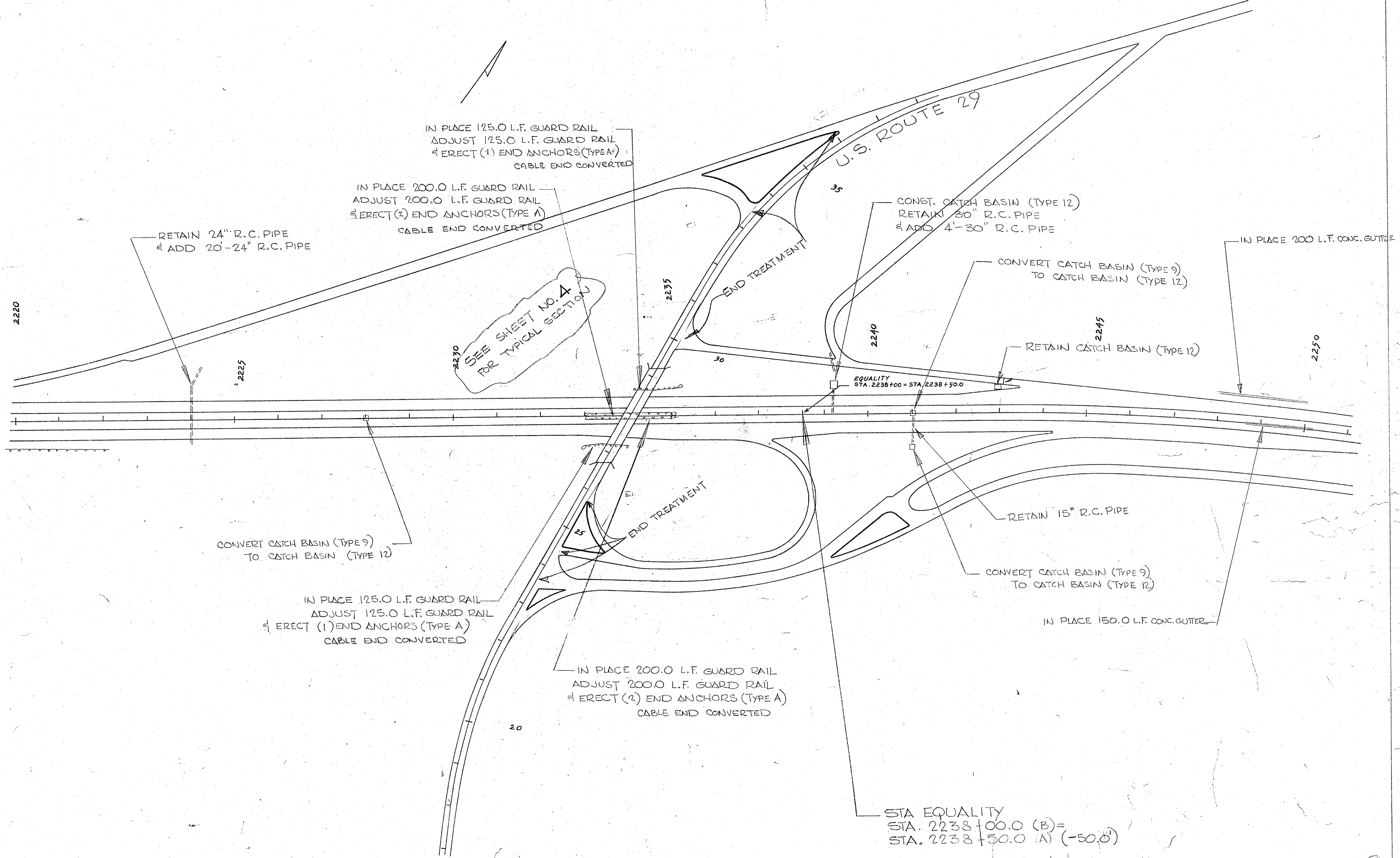
| P&S ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|-------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S.C. | CHEROKEE | 11.530.1 | 12.85.3(45) | 1-85 | 26 | 60 |



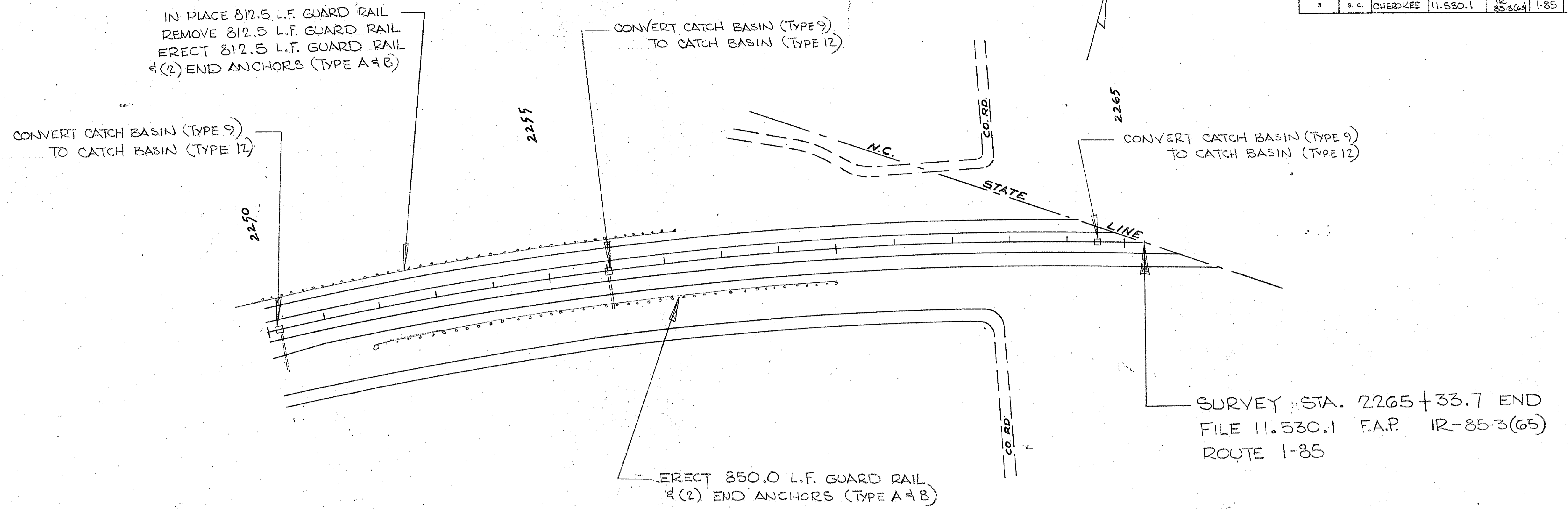
| FED. ROAD DIV. NO. | STATE | COUNTY | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|-------------|-------------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11,530.1 | 12 85.3(GS) | 185 | 27 |
| | | | | | | 60 |



| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|----------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | 12 85-3(65) | 1-85 | 78 | 60 |



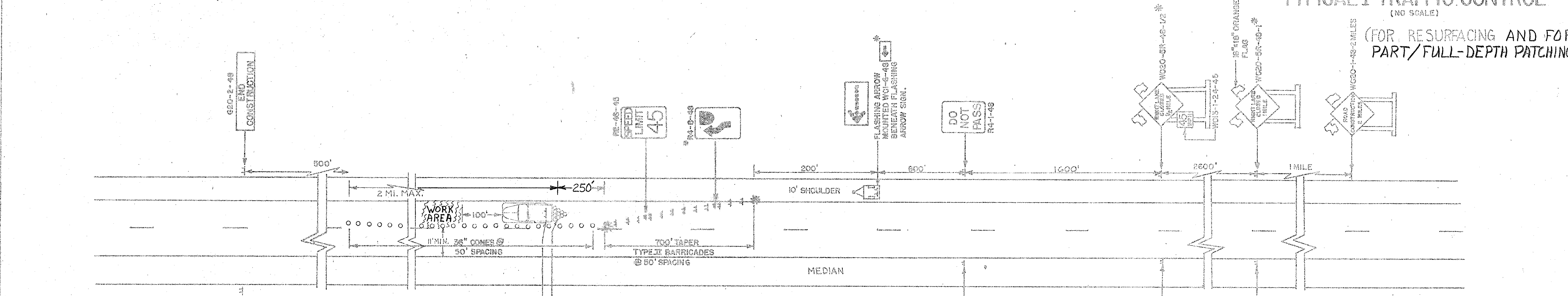
| FED. ROAD DIV. NO. | STATE | COUNTY | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|---------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHESTER | 11.530.1 | 1-85 | 29 | 60 |



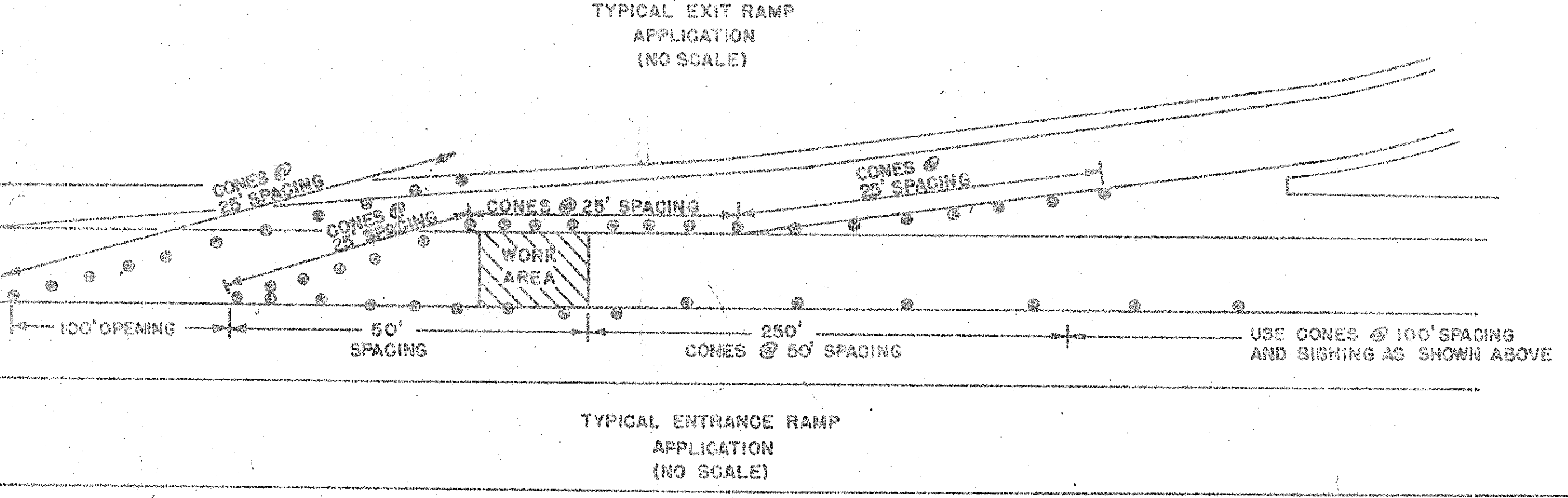
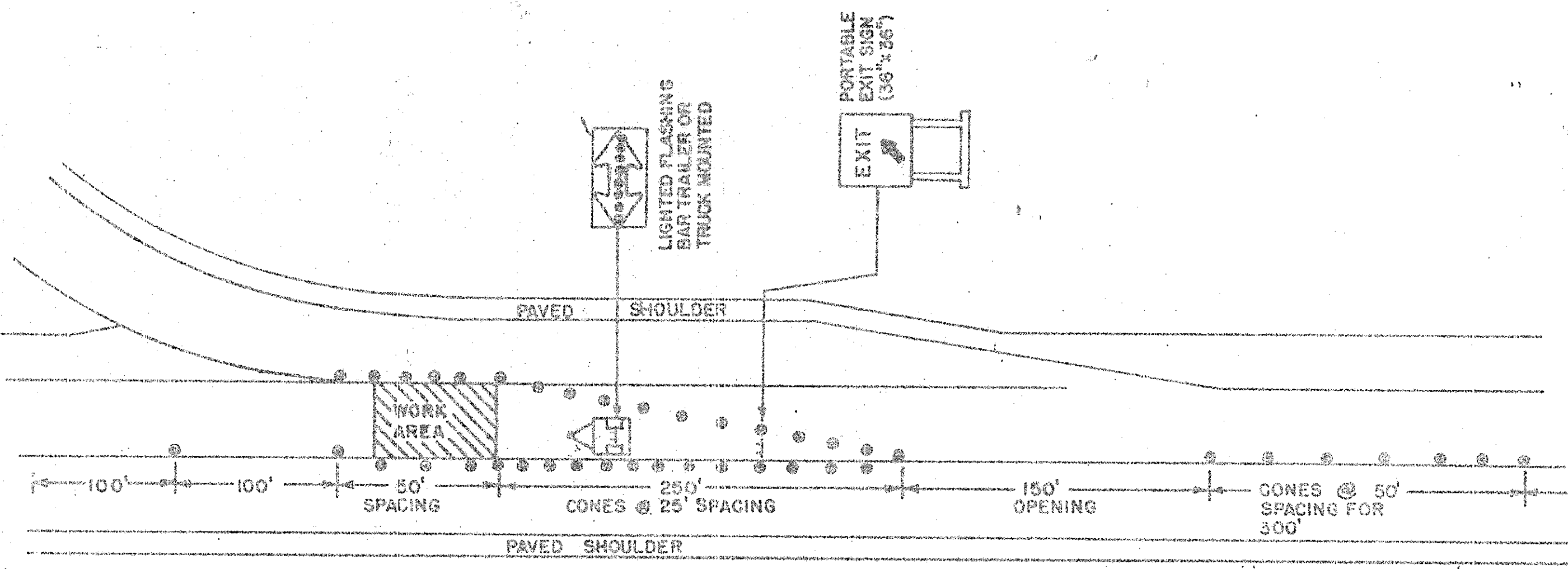
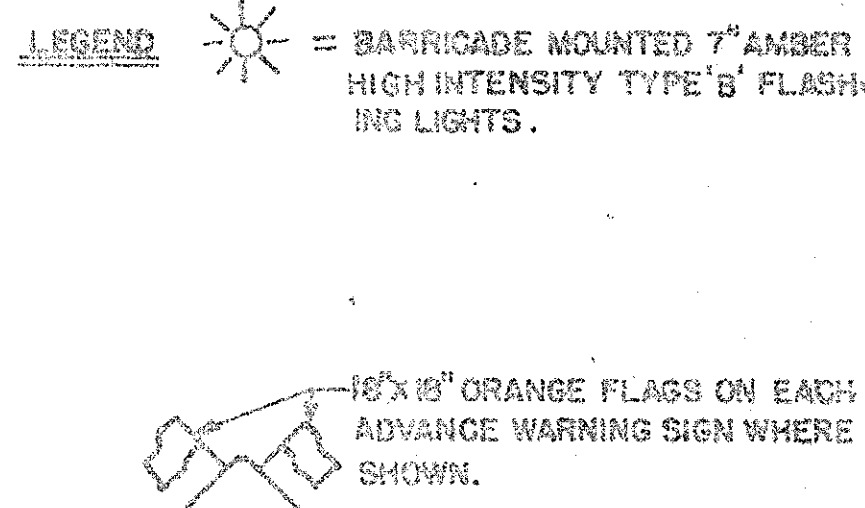
TYPICAL I TRAFFIC CONTROL

(NO SCALE)

(FOR RESURFACING AND FOR PART/FULL-DEPTH PATCHING)



- NOTE:**
1. TYPE II BARRICADES SHALL HAVE A MINIMUM WIDTH OF 4 FEET.
 2. CONES AND DRUMS ARE NOT REQUIRED TO BE REFLECTORIZED (DURING DAYLIGHT HOURS ONLY).
 3. NO LANES ARE TO BE CLOSED DURING HOURS OF DARKNESS.



REQUIREMENTS

- 1) ALL SIGN LOCATIONS ARE TO BE MEASURED FROM THE WORK AREA. WORK LIMITS FOR THE PROJECT WILL BE DETERMINED BY THE ENGINEER AND AS INDICATED IN THE CONTRACT.
- 2) ALL ADVANCE SIGNS ARE TO BE MOUNTED 2 FEET FROM THE EDGE OF PAVED SHOULDER TO EDGE OF SIGN AND 5 FEET FROM GROUND TO BOTTOM EDGE OF LOWEST SIGN. SPECIAL SIGN MOUNTING ASSEMBLIES MAY BE NECESSARY IN AREAS OF DOUBLE-FACED GUARDRAIL (SEE NOTE 4 FOR CLOSING LEFT LANE)
- 3) ALL SIGNS, BARRICADES, AND CONES ARE TO BE REFLECTORIZED USING TYPE II SHEETING.
- 4) THE CONTRACTOR SHALL PROVIDE TYPE 'B' (HIGH INTENSITY) 7 INCH DIAMETER AMBER FLASHING LIGHTS ON THE INDICATED TYPE II BARRICADES. THESE LIGHTS ARE TO BE AS SPECIFIED IN PART II OF THE SCMTGD, 1972 (REVISED 6-1-76).
- 5) SPACINGS INDICATED ARE FOR NORMAL CONDITIONS; ADJUSTMENTS MAY BE REQUIRED BECAUSE OF HORIZONTAL AND/OR VERTICAL ALIGNMENT OF OTHER SIGHT DISTANCE RESTRICTIONS.
- 6) DO NOT INSTALL G20-1-60 (ROAD CONSTRUCTION NEXT XX MILES) SIGNS.
- 7) ORANGE FLAGS (18 INCHES X 18 INCHES) ARE TO BE MOUNTED ON EACH WARNING SIGN AS INDICATED.
- 8) THE CONTRACTOR SHALL PERFORM HIS WORK IN SUCH A MANNER SO AS NOT TO ENDOUR ONTO THE ADJACENT TO THE CLOSED LANE.
- 9) CONES SHALL BE PLACED AND ADJUSTED ACCORDINGLY DURING WORKING HOURS.
- 10) IF WORK IS BEING DONE AT TWO DIFFERENT LOCATIONS AT THE SAME TIME, THE CONTRACTOR WILL NOT BE ALLOWED TO BLOCK ONE LANE AT ONE LOCATION AND A DIFFERENT LANE AT THE OTHER OPERATION. WORK AT THE TWO LOCATIONS IS TO BE DONE IN THE SAME LANE.
- 11) IF WORK IS BEING DONE AT TWO DIFFERENT LOCATIONS AT THE SAME TIME THE TWO LOCATIONS ARE TO BE A MINIMUM OF 2 MILES FROM THE END OF ONE LOCATION TO THE BEGINNING OF THE WORK AT THE OTHER LOCATION. A COMPLETE SET OF SIGNS WILL BE REQUIRED AT EACH LOCATION.
- 12) IF THE SIGNING IS NOT MAINTAINED ACCORDING TO THE SPECIAL PROVISIONS, SUPPLEMENTAL SPECIFICATIONS AND/OR THE PLANS, THE DEPARTMENT RESERVES THE RIGHT TO RESTRICT CONSTRUCTION OPERATIONS.

* LEFT LANE(S) CLOSURE

- 1) SIGNS ILLUSTRATED ARE USED FOR CLOSING THE RIGHT LANE.
- 2) WHEN LEFT LANE (S) IS (ARE) CLOSED USE THE FOLLOWING:
2-W020-51-48-1MILE
2-W020-51-48-1/2MILE
2-R4-7-48
- 3) BARRICADES ON LEFT OF TRAFFIC SHALL HAVE RIGHT SLOPING STRIPES.
- 4) SPECIAL SIGN SUPPORTS MAY BE NEEDED FOR CLOSURE OF LEFT (MEDIAN) LANE(S) IN THE AREA OF THE DOUBLE FACED GUARDRAIL AND SHALL BE PROVIDED BY THE CONTRACTOR.
- 5) THE FLASHING ARROW AND W01-6-48 SHALL POINT TO THE RIGHT.

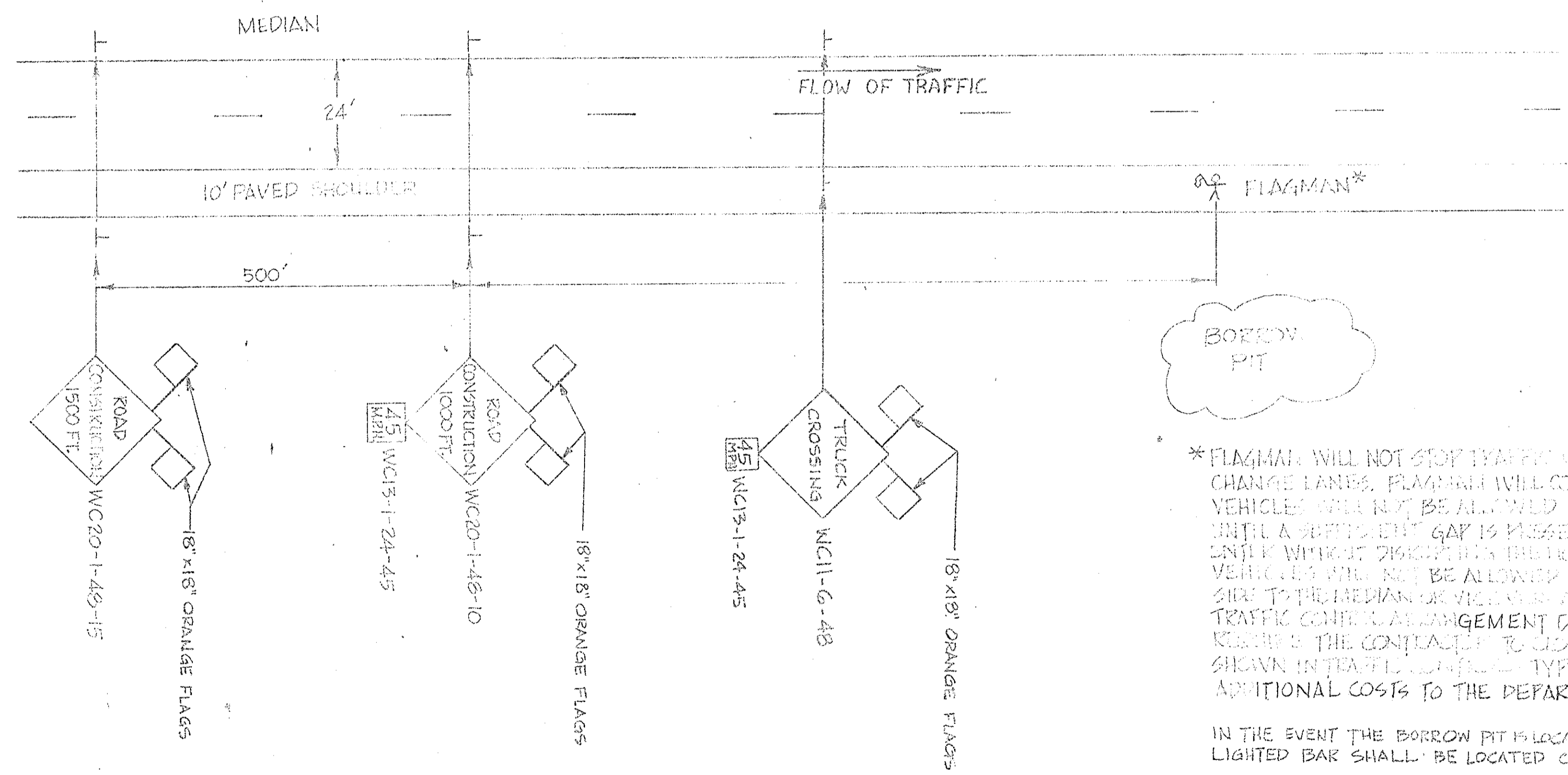
PORTABLE TRUCK MOUNTED CRASH CUSHION

- 1) PORTABLE CRASH CUSHION IS TO BE TRUCK MOUNTED AND DESIGNED SUCH THAT A 4500 L.B. VEHICLE TRAVELING AT 60MPH IMPACTS A STATIONARY TMA (TRUCK MOUNTED ATTENUATOR) EQUIPPED TRUCK WEIGHING APPROXIMATELY 20,000 LBS. AT LEAST FOUR FIFTHS OF THE VEHICLE'S IMPACT ENERGY SHALL BE DISSIPATED DURING THE IMPACT. NO MORE THAN ONE THIRD OF THE IMPACT ENERGY SHALL BE ABSORBED BY A MOTIONLESS TRUCK THROUGH PROJECTING IT FORWARD.
- 2) THE PORTABLE TRUCK MOUNTED CRASH CUSHION SHALL BE A 6 FOOT TRUCK MOUNTED ATTENUATOR MANUFACTURED BY ENERGY ABSORPTION SYSTEMS, INC. OF CHICAGO, ILLINOIS OR EQUAL.
- 3) THE PORTABLE TRUCK MOUNTED CRASH CUSHION SHALL BE LOCATED 100 FEET IN ADVANCE OF THE WORK AREA WHEN THE WORK AREA IS WITHIN THE THROUGH TRAVEL LANES.
- 4) THE PORTABLE TRUCK MOUNTED CRASH CUSHION SHALL BE USED ONLY DURING RESURFACING.

FLASHING ARROW PANEL

FLASHING ARROW PANEL SHALL BE 48" X 66" WITH A MINIMUM LEGIBILITY DISTANCE OF 1 MILE. THE PANEL FACE SHOULD BE NON-REFLECTIVE BLACK.

TYPICAL II TRAFFIC CONTROL
(FOR EXCAVATION WORK)



* FLAGMAN WILL NOT STOP TRAFFIC ON THE FREEWAY OR REQUIRE TRAFFIC TO CHANGE LANES. FLAGMAN WILL CONTROL CONTRACTOR VEHICLES. CONTRACTOR VEHICLES WILL NOT BE ALLOWED TO ENTER TRAVEL LANE 2 OF THE INTERCHANGING UNTIL A SUFFICIENT GAP IS PRESENT TO ALLOW COMING FROM TRAFFIC TO ENTER WITHOUT DISRUPTING THE NORMAL FLOW OF TRAFFIC. THE CONTRACTOR'S VEHICLES WILL NOT BE ALLOWED TO ENTER THE MAIN ROADWAY FROM THE LEFT SIDE TO THE MEDIAN OR VICE VERSA. IF IN THE EVENT OF THE BORROW PIT, THE TRAFFIC CONTROL ARRANGEMENT DOES NOT WORK PROPERLY, THE ENGINEER WILL REQUIRE THE CONTRACTOR TO CLOSE THE NIGHT LANE USING THE FLAGMAN AS SHOWN IN TYPICAL I. THIS SHALL BE DONE AT NO ADDITIONAL COSTS TO THE DEPARTMENT.

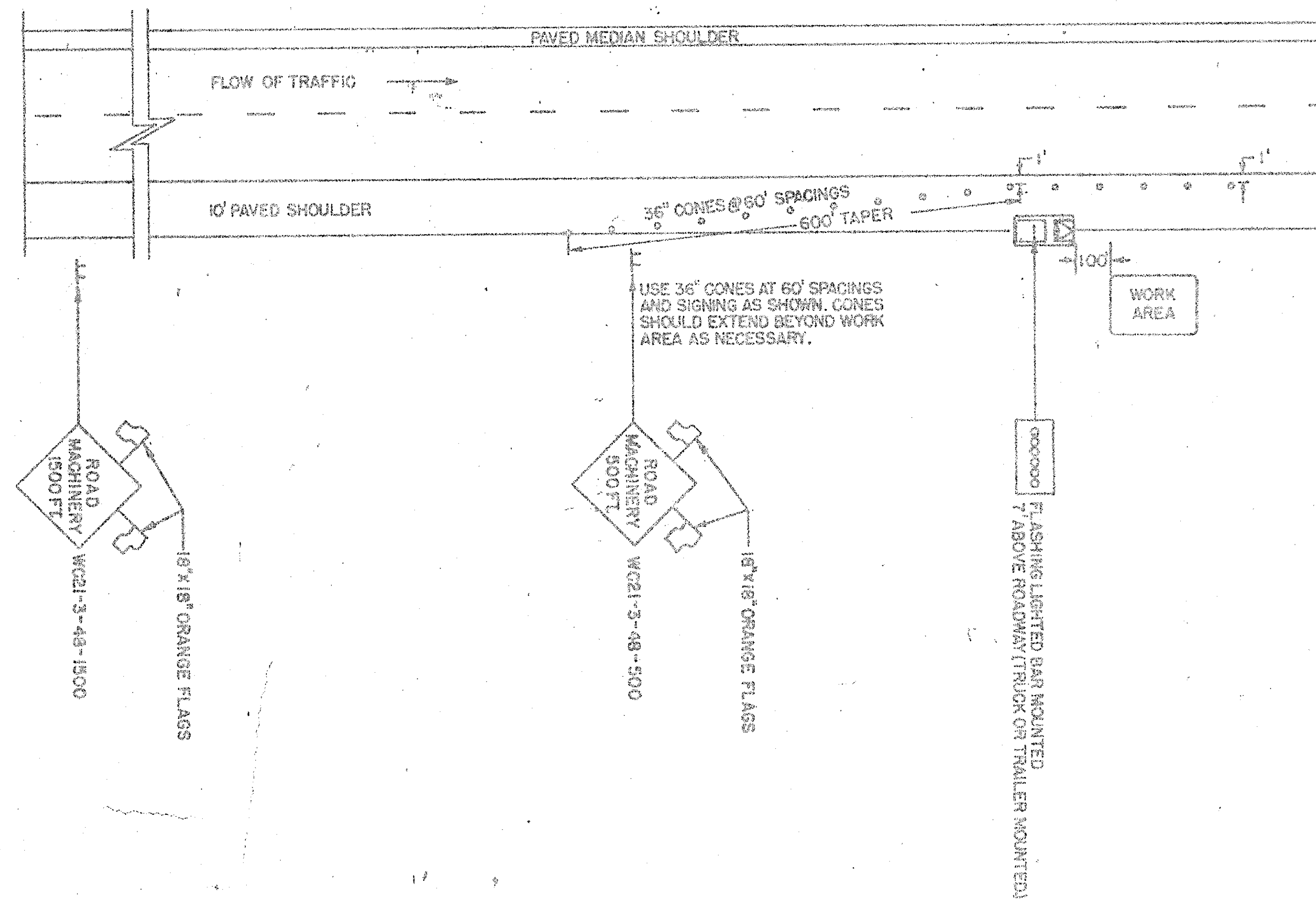
IN THE EVENT THE BORROW PIT IS LOCATED IN THE MEDIAN, THE FLAGMAN & FLASHING LIGHTED BAR SHALL BE LOCATED ON THE LEFT SIDE OF THE ROADWAY.

FURTHERMORE, TRUCKS ENTERING THE MEDIAN THAT EXIT IN THE OPPOSITE DIRECTION WILL REQUIRE TWO (2) SETS OF TRAFFIC CONTROL AS SHOWN AND DESCRIBED HEREON - ONE FOR EACH DIRECTION OF TRAVEL.

"REQUIREMENT", TYPICAL I TRAFFIC CONTROL APPLIES TO SIGNS ON THIS SHEET.

TYPICAL III TRAFFIC CONTROL FOR GUARD RAIL & CATCH BASIN

| PROJ. NO. | STATE | COUNTY | FILE NO. | PROJECT NO. | ROUTE | SHEET NO. | TOTAL SHEETS |
|-----------|-------|----------|----------|-------------|-------|-----------|--------------|
| 3 | S.C. | CHEROKEE | 11.530.1 | 12-85-3(66) | 1-85 | 32 | 60 |



REQUIREMENTS

1. ALL SIGN LOCATIONS ARE TO BE MEASURED FROM THE WORK AREA. WHEN WORK PROGRESSES TO A NEW LOCATION, ALL ADVANCE SIGNS ARE TO BE REPOSITIONED.
2. ALL CONSTRUCTION SIGNS ARE TO BE LEVELED AND MOUNTED 2 FEET FROM EDGE OF PAVED SHOULDER TO EDGE OF SIGN AND 5 FEET FROM GROUND TO BOTTOM OF SIGN.
3. ALL SIGNS ARE TO BE REFLECTORIZED USING TYPE II SHEETING.
4. IF THE SIGNING IS NOT MAINTAINED ACCORDING TO THIS SHEET, THE ENGINEER SHALL SUSPEND WORK UNTIL THE SIGNS ARE PROPERLY INSTALLED.
5. SPACINGS SHOWN ARE FOR NORMAL CONDITIONS, ADJUSTMENT MAY BE NECESSARY DUE TO HORIZONTAL AND/OR VERTICAL ALIGNMENT OR OTHER SIGHT DISTANCE RESTRICTIONS.
6. NO WORK IS TO BE PERFORMED BETWEEN THE HOURS OF SUNDOWN AND SUNRISE.
7. ORANGE FLAGS 18" X 18" ARE TO BE MOUNTED ON EACH ADVANCE WARNING SIGN.
8. IF WORK IS BEING DONE AT TWO DIFFERENT LOCATIONS, A COMPLETE SET OF SIGNS WILL BE REQUIRED AT EACH LOCATION.
9. TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE PLANS, THE SPECIFICATIONS, PART 'X' OF THE SCMUTCD (REVISED JUNE 1, 1976), THE ABOVE REQUIREMENTS AND AS FOLLOWS:

CASE I: A PORTION OF THE SHOULDER HAS TO BE OCCUPIED IN ORDER TO ACCOMPLISH WORK BEING PERFORMED -- SIGNING AND CONE PLACEMENT SHALL BE IN ACCORDANCE WITH THE ABOVE.

CASE II: WORK BEING PERFORMED CAN BE ACCOMPLISHED WITHOUT HAVING TO OCCUPY A PORTION OF THE SHOULDER -- ONLY ONE W021-3-48 SIGN WILL BE NEEDED AT 500' IN ADVANCE OF THE WORK AREA.

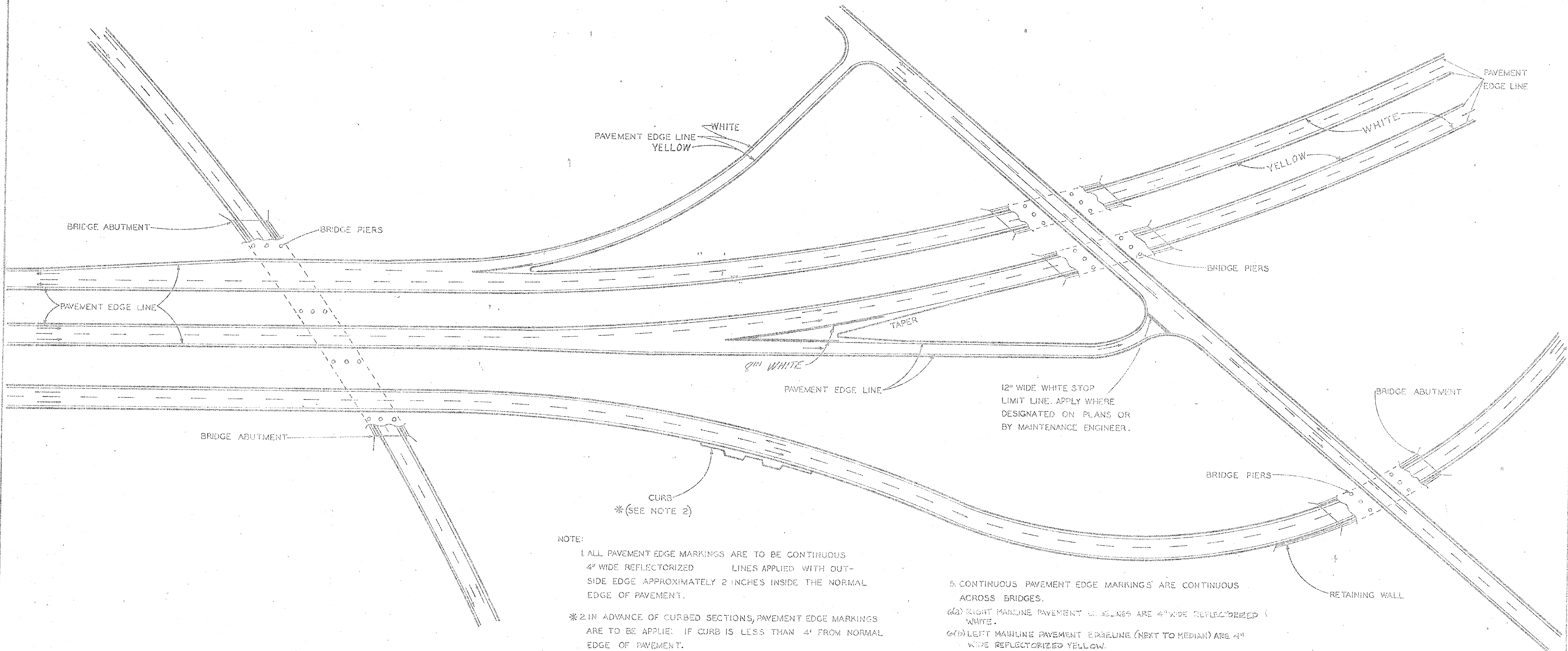
10. WORK IS TO BE ACCOMPLISHED WITHOUT CLOSING TRAFFIC LANES.

TYPICAL PAVEMENT MARKINGS

| | | | | | | | |
|---|----|----------|----------|---------|------|----|----|
| 3 | SC | CHEROKEE | 11.550.1 | 853(65) | 1-85 | 33 | 60 |
|---|----|----------|----------|---------|------|----|----|

S. C. DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION, COLUMBIA, S. C.

PAVEMENT EDGE MARKINGS AND STOP LIMIT LINES



NOTE:

1. ALL PAVEMENT EDGE MARKINGS ARE TO BE CONTINUOUS 4" WIDE REFLECTORIZED LINES APPLIED WITH OUTSIDE EDGE APPROXIMATELY 2 INCHES INSIDE THE NORMAL EDGE OF PAVEMENT.
- * 2. IN ADVANCE OF CURBED SECTIONS, PAVEMENT EDGE MARKINGS ARE TO BE APPLIED IF CURB IS LESS THAN 4' FROM NORMAL EDGE OF PAVEMENT.
3. APPLY PAVEMENT EDGE MARKINGS ON MEDIANS AND OUTSIDE EDGE OF TRAVELED WAY THROUGHOUT INTERSTATE MAINLINE.
4. APPLY PAVEMENT EDGE MARKINGS ON EACH SIDE OF RAMPS, EXCEPT WHERE CURBED. SEE CONCERNED INTERCHANGE OR REST AREA PLAN SHEET.

5. CONTINUOUS PAVEMENT EDGE MARKINGS ARE CONTINUOUS ACROSS BRIDGES.
 - (a) RIGHT MAINLINE PAVEMENT EDGELINES ARE 4" WIDE REFLECTORIZED WHITE.
 - (b) LEFT MAINLINE PAVEMENT EDGELINE (NEXT TO MEDIAN) ARE 4" WIDE REFLECTORIZED YELLOW.
6. PAVEMENT EDGE MARKINGS ARE NOT NORMALLY USED ON FRONTAGE ROADS OR CROSSOVERS WHICH DO NOT HAVE PAINTED CENTERLINES.

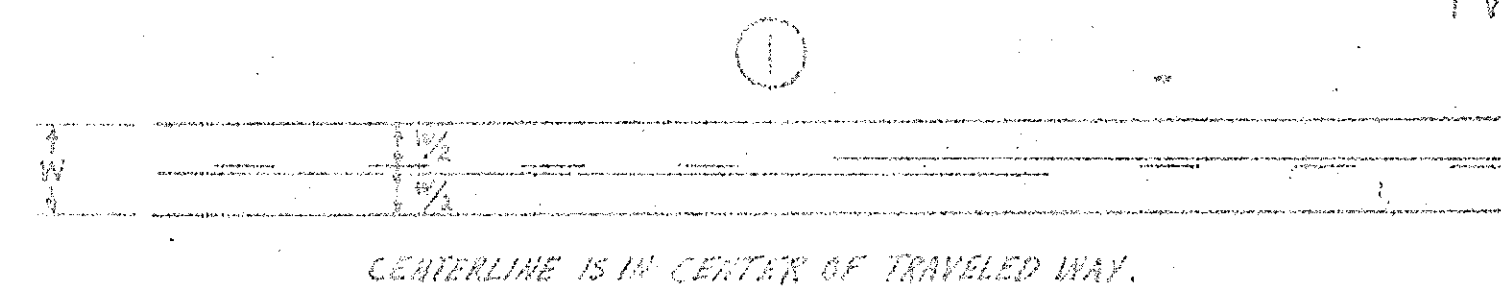
TYPICAL PAVEMENT MARKINGS

CENTER LINES, LANE LINES AND BARRIER LINES

| FED. ROAD DIST. NO. | STATE | COUNTY | FILE NO. | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|------------|----------|-------------|-----------|--------------|
| 3 | S. C. | CHESTERLEE | 11-630.1 | 12 | 34 | 60 |

ILLUSTRATION NUMBER IN CIRCLE

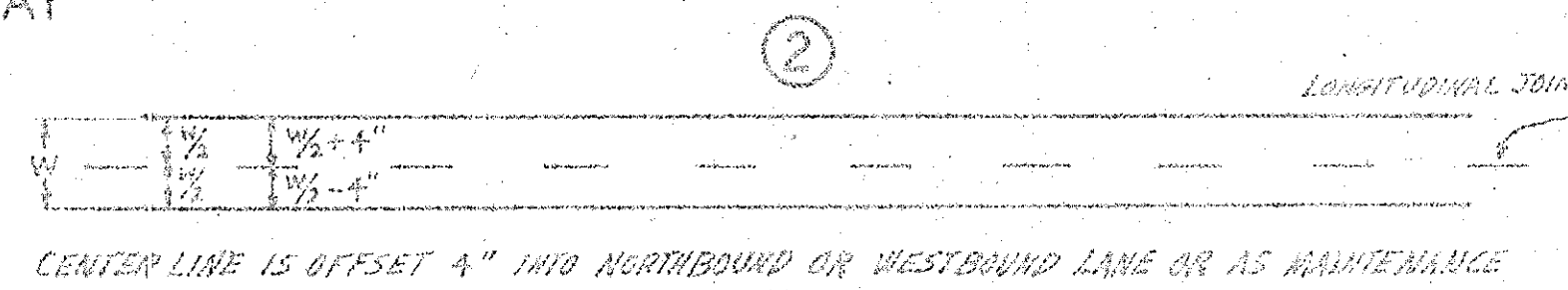
ASPHALT SURFACE



CENTERLINE IS IN CENTER OF TRAVELED WAY.

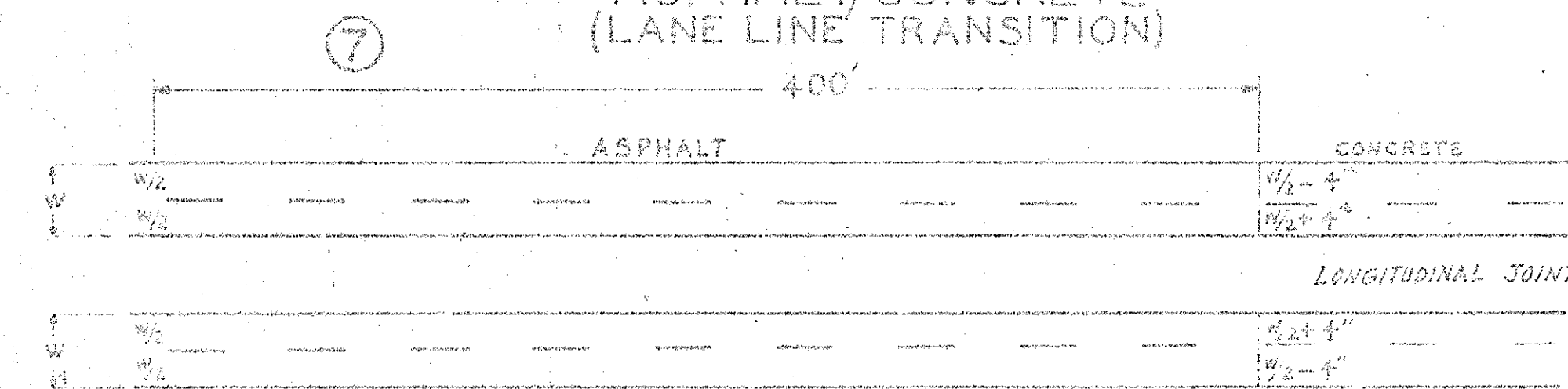
TWO LANE - TWO WAY

CONCRETE SURFACE



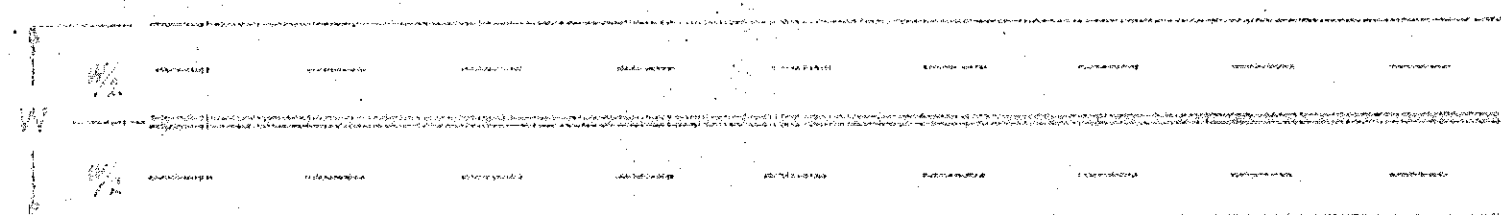
CENTER LINE IS OFFSET 4" INTO NORTHBOUND OR WESTBOUND LANE OR AS MAINTENANCE ENGINEER DESIRES. IF LONGITUDINAL JOINT IS NOT PRESENT IN CENTER OF TRAVELED WAY CENTERLINE WILL BE IN CENTER OF TRAVELED WAY.

ASPHALT/CONCRETE (LANE LINE TRANSITION)



LANE LINE IS OFFSET 4" TO INSIDE LANE ON CONCRETE AND IS CENTERED ON ASPHALT SURFACE. TRANSITION LANE LINE FOR 400' FROM CENTER TRAVELED WAY ON ASPHALT TO 4" OFFSET ON CONCRETE. TRANSITION TAKES PLACE ENTIRELY ON ASPHALT.

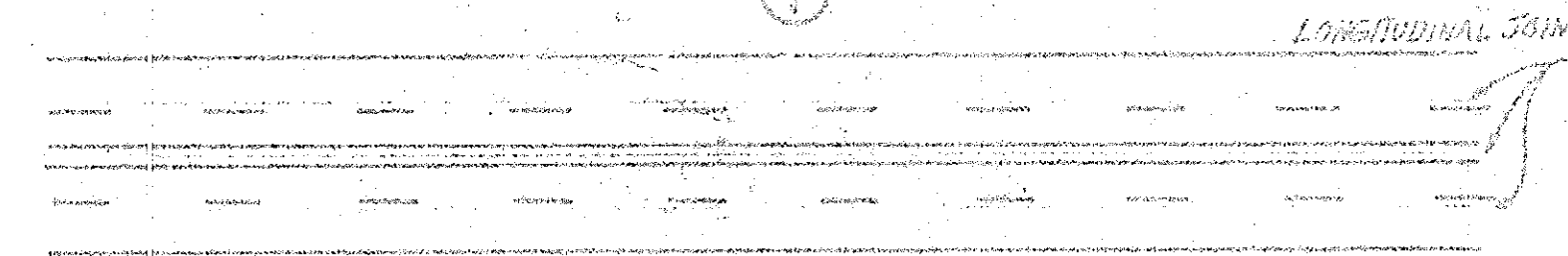
3



DOUBLE YELLOW CENTERLINE IN CENTER OF TRAVELED WAY. LANE WIDTHS DEPEND ON TOTAL ROAD WIDTH AND WILL BE SHOWN ON PLAN SHEETS.

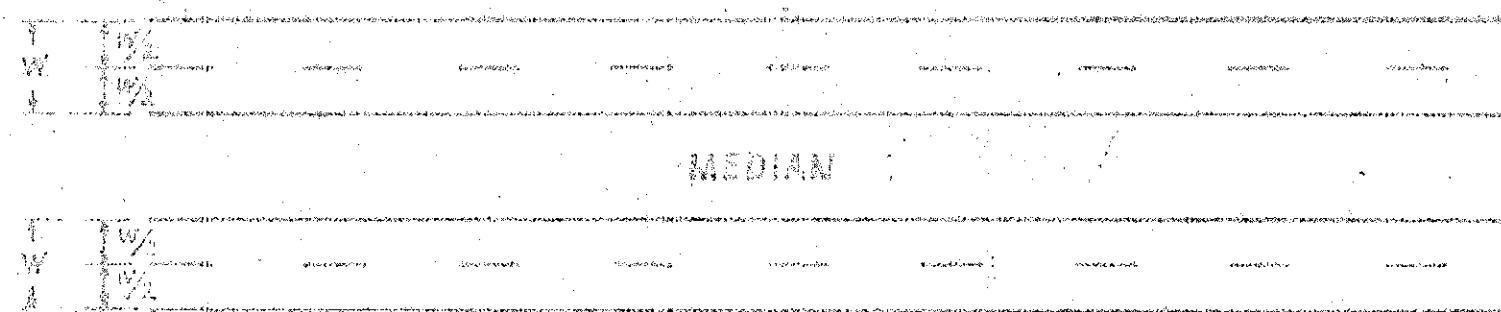
FOUR LANE - NOT DIVIDED

4



DOUBLE YELLOW CENTERLINE STRADDLES CENTER LONGITUDINAL JOINT. LANE LINES ARE OFFSET TO INSIDE LANE EACH DIRECTION. WIDTH OF LANES ARE SHOWN ON PLANS.

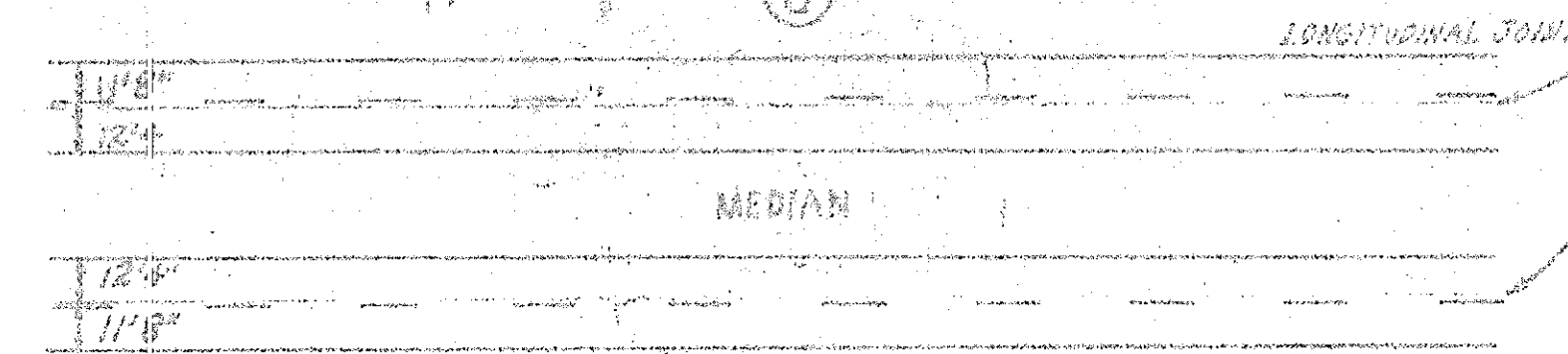
5



LANE LINES ARE IN CENTER OF TRAVELED WAY.

FOUR LANE - DIVIDED

6



LANE LINES ARE OFFSET TO OUTSIDE LANE EACH DIRECTION. WIDTH OF LANES ARE SHOWN ON PLANS EXCEPT INTERSTATE MAINLINE WHICH IS SHOWN ABOVE.

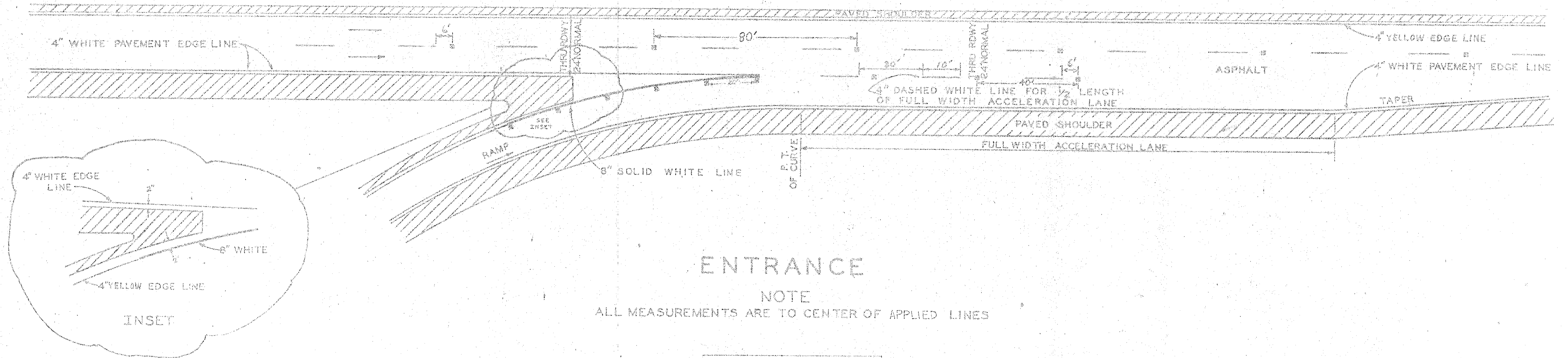
NOTES:

- SEE PAGES PART III MARKINGS IN THE SOUTH CAROLINA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (SCMUTCD) FOR DISCUSSION OF CENTER LINES, LANE LINES, AND BARRIER LINES.
- BARRIER LINES SHALL BE APPLIED ON TWO LANE TRAVELWAYS WHERE CENTER LINES ARE PAINTED, WHERE INDICATED IN THESE PLANS OR IN ACCORDANCE WITH WARRANTY IN SCMUTCD. SEE PAGE 2-107 OF SCMUTCD FOR METHOD OF LOCATING AND DETERMINING THE LIMITS OF NO-PASSING ZONES AT VERTICAL AND HORIZONTAL CURVES.
- CENTER LINES ON TWO LANE TWO-WAY URBAN STREETS ARE BROKEN YELLOW LINES.
- THE STANDARD DASHED CENTERLINE AND LANE LINE SHALL BE 11 FEET IN LENGTH WITH A 3 FOOT GAP. WHERE LOW SPEEDS ARE PREVALENT, THE LINE SEGMENTS AND GAPS MAY BE SHORTENED MAINTAINING A 1 TO 3 RATIO OF LENGTH OF STRIPE TO LENGTH OF GAP.
- ALL MEASUREMENTS ARE TO CENTER OF APPLIED LINES.

STANDARD PAVEMENT MARKINGS

FOR EXIT AND ENTRANCE RAMPS HAVING
PARALLEL TYPE I ACCELERATION AND DECELERATION LANES
S. C. DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION, COLUMBIA, S. C.

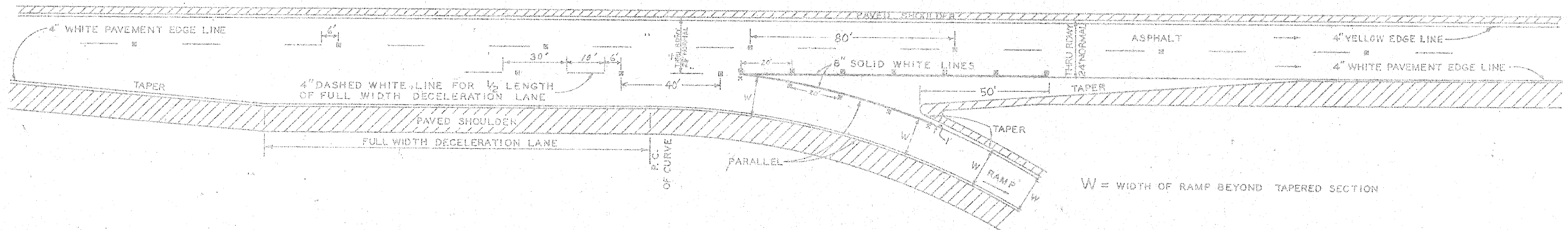
| FILE NO. | STATE | COUNTY | FILE NO. | PROJECT NO. | ROAD NO. | DATE | REVISED |
|----------|-------|----------|----------|-------------|----------|------|---------|
| 3 | S.C. | CHEROKEE | 11,530.1 | 12 | 85-3(69) | 1-85 | 35 60 |



ENTRANCE

NOTE
ALL MEASUREMENTS ARE TO CENTER OF APPLIED LINES

☒ - Clear Pavement Markings in Recessed Slots.



EXIT

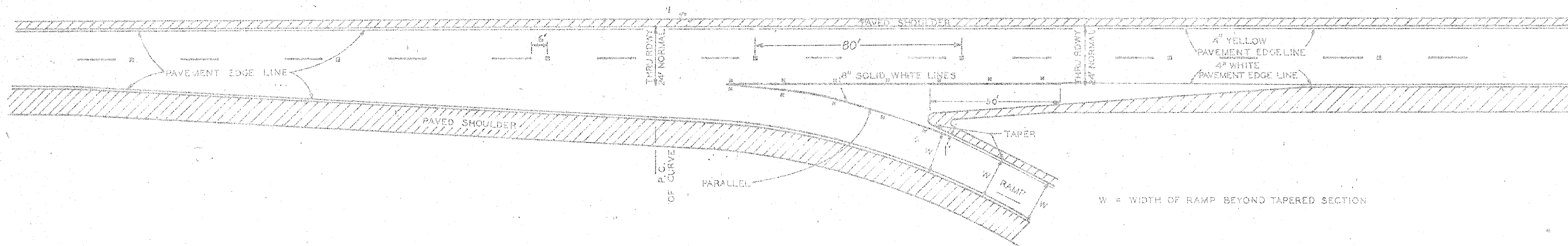
NOTE
ALL MEASUREMENTS ARE TO CENTER OF APPLIED LINES

W = WIDTH OF RAMP BEYOND TAPERED SECTION

STANDARD PAVEMENT MARKINGS

FOR EXIT AND ENTRANCE RAMP HAVING
 TAPERED (TYPE I) ACCELERATION AND DECELERATION LANES
 S.C. DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION, COLUMBIA, S. C.

| DIST. NO. | STATE | COUNTY | FILE NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|-----------|-------|----------|----------|-------------|-----------|-----------|--------------|
| 3 | S.C. | CHEROKEE | 11,530.1 | 1085-363 | 1-85 | 136 | 60 |



W = WIDTH OF RAMP BEYOND TAPERED SECTION

EXIT

NOTE

ALL MEASUREMENTS ARE TO CENTER OF APPLIED LINES

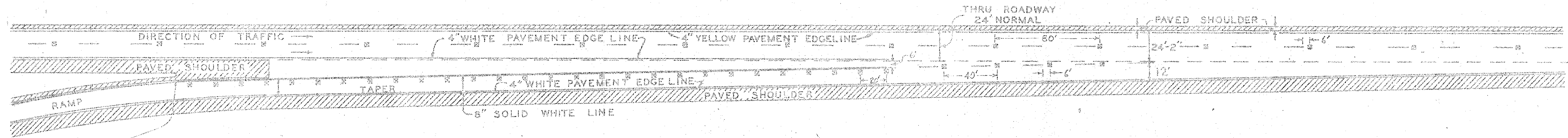
*Clear marked markers
 in recessed slots*

| FED. ROAD DIST. NO. | STATE | COUNTY | FILL. NO. | PROJECT NO. | ROAD M.P. | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------|-----------|-------------|-----------|-----------|--------------|
| | 22 | CHEROKEE | 11.530.1 | 18 | 85.2(3) | 1-85 | 37 |

STANDARD PAVEMENT MARKINGS

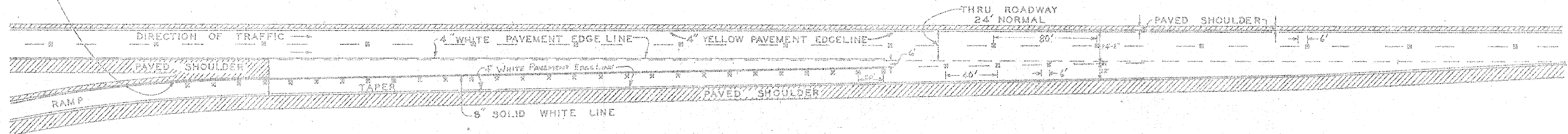
FOR ENTRANCE RAMP HAVING
TAPERED (TYPE III) ACCELERATION LANES

S. C. DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION, COLUMBIA, S. C.



TYPE III-A

END 4" SOLID YELLOW LINE AND
BEGIN 8" SOLID WHITE LINE OPPOSITE
POINT WHERE PAVED SHOULDERS JOIN



TYPE III-B

NOTE: ALL MEASUREMENTS ARE TO CENTER OF APPLIED LINES.

☒ - CLEAR PAVEMENT MARKERS IN
RECESSED SLOTS.

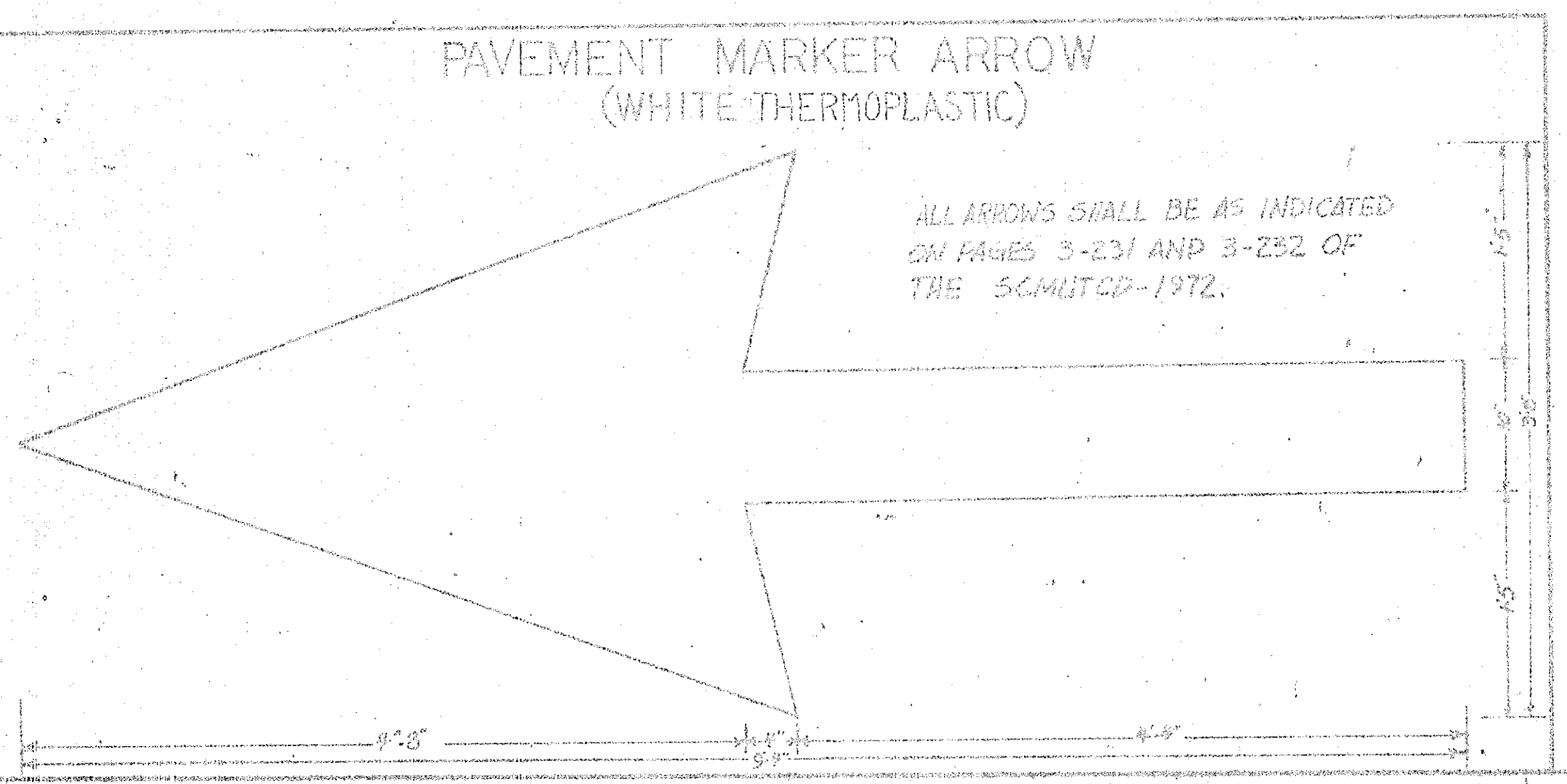
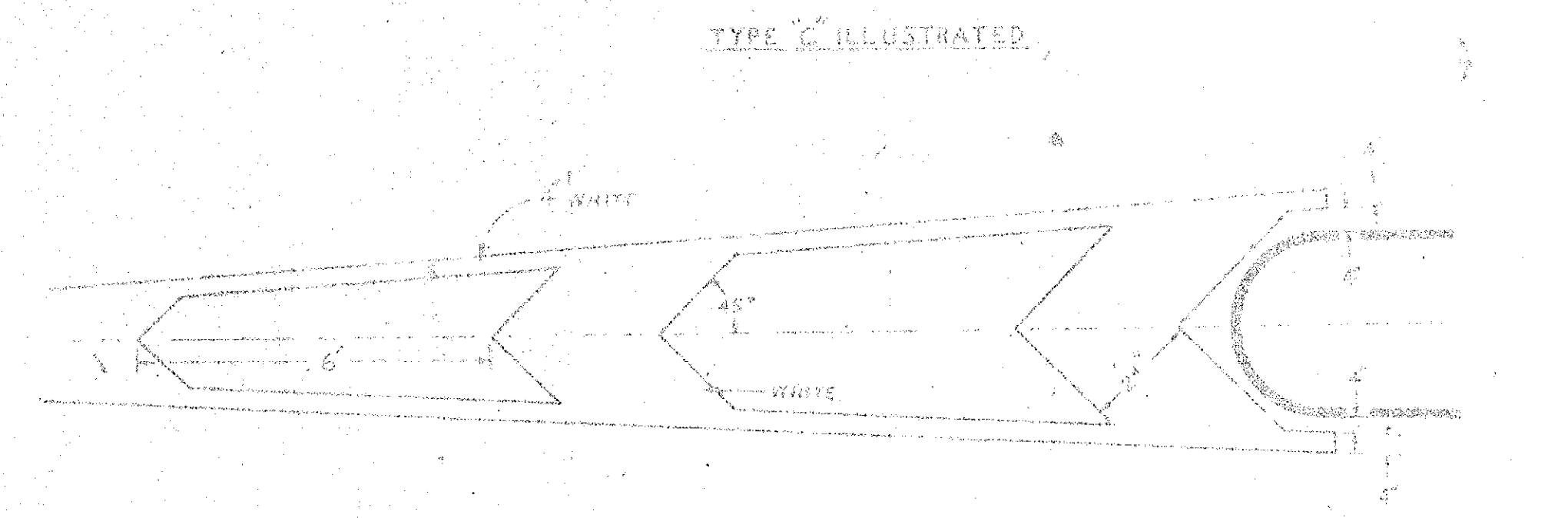
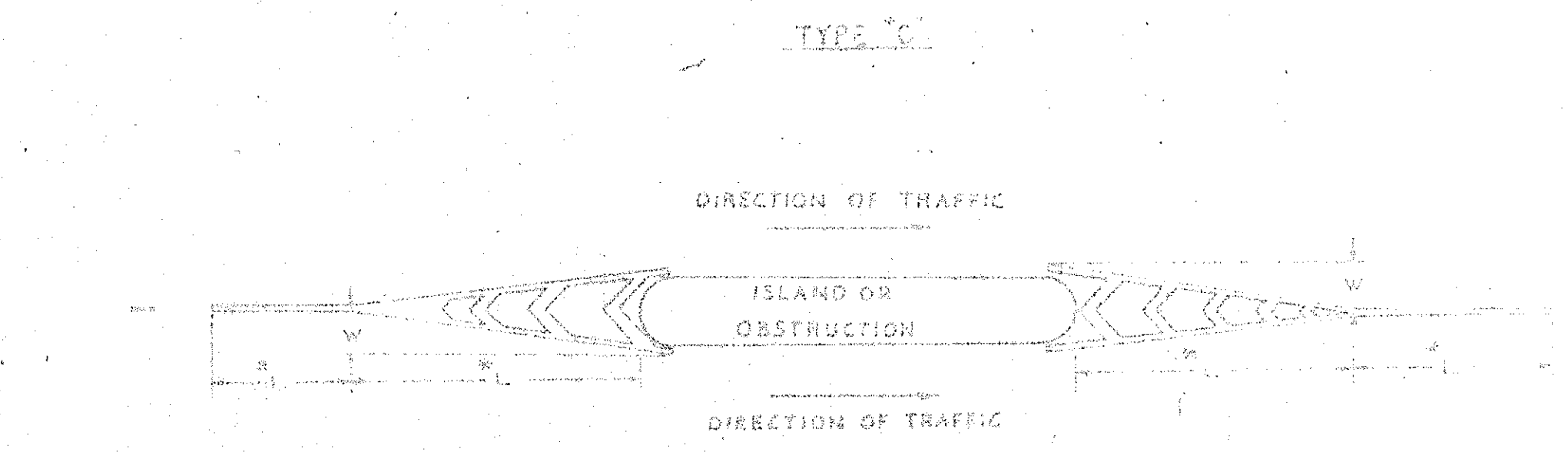
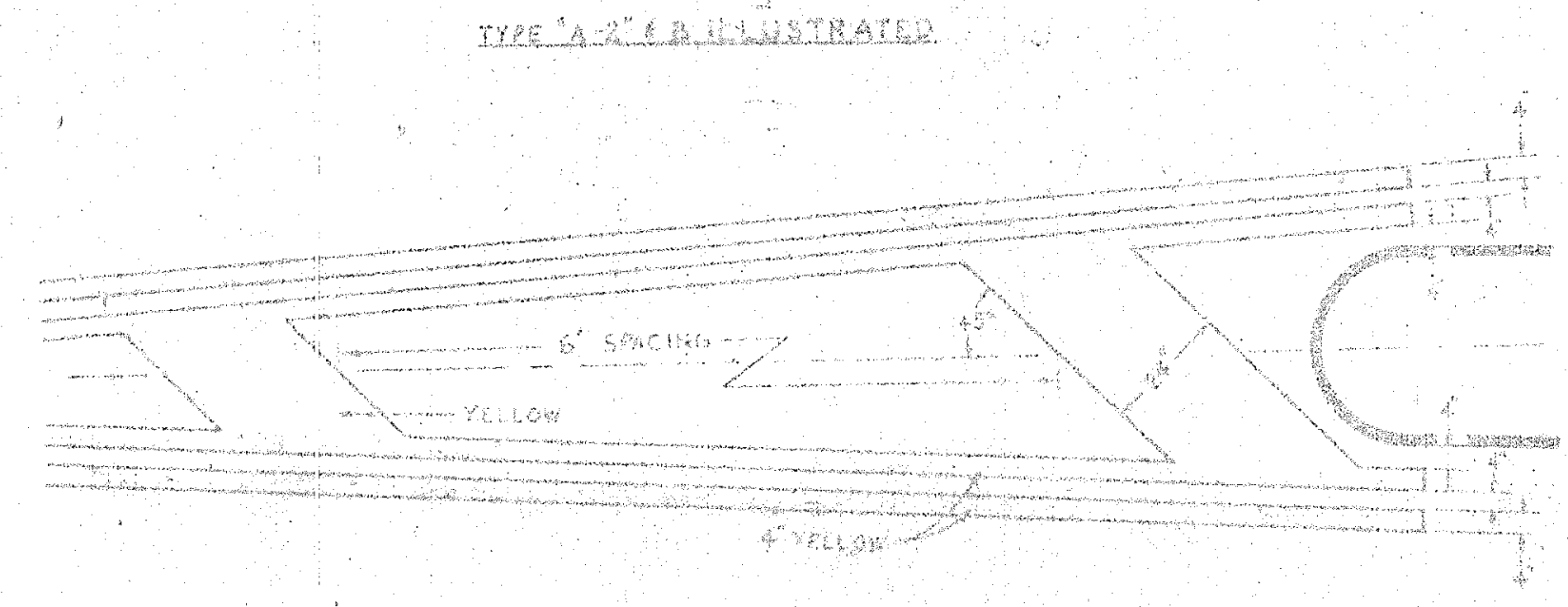
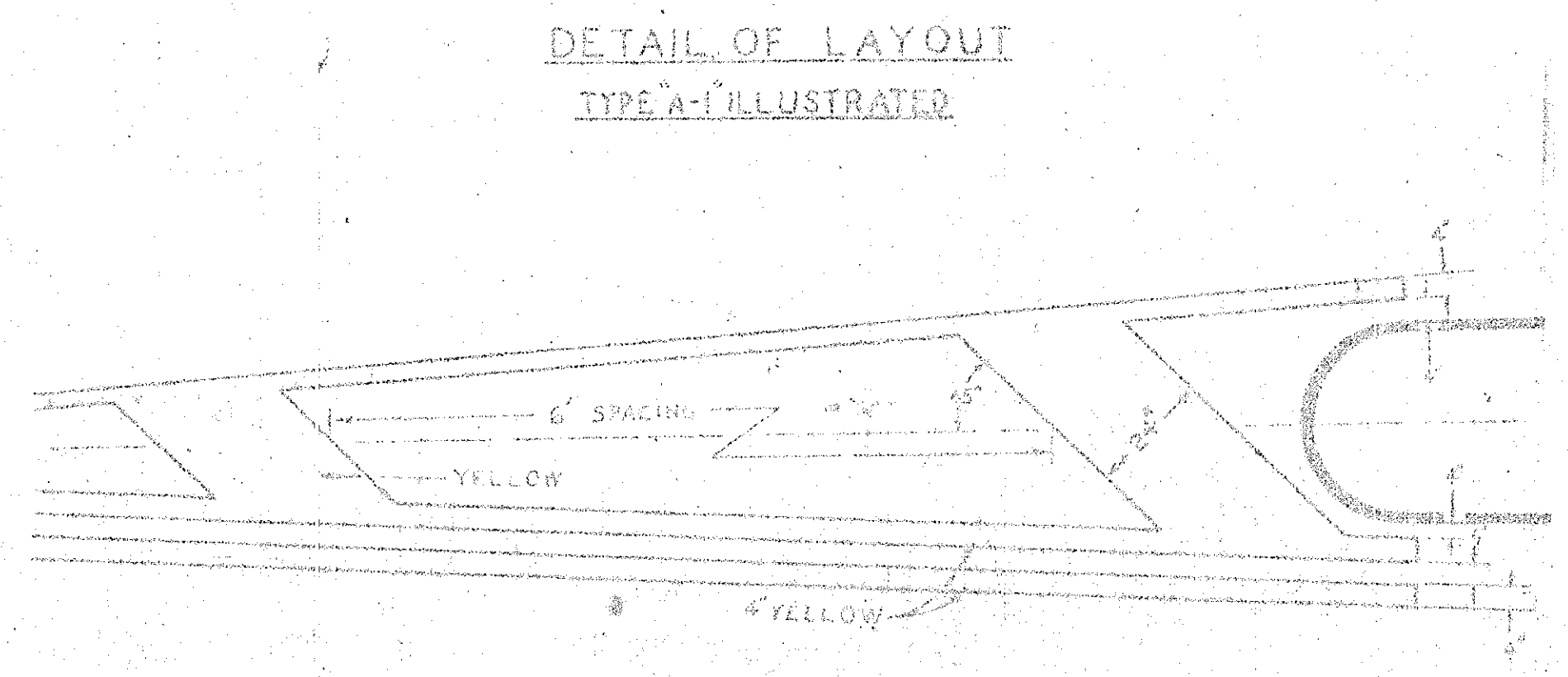
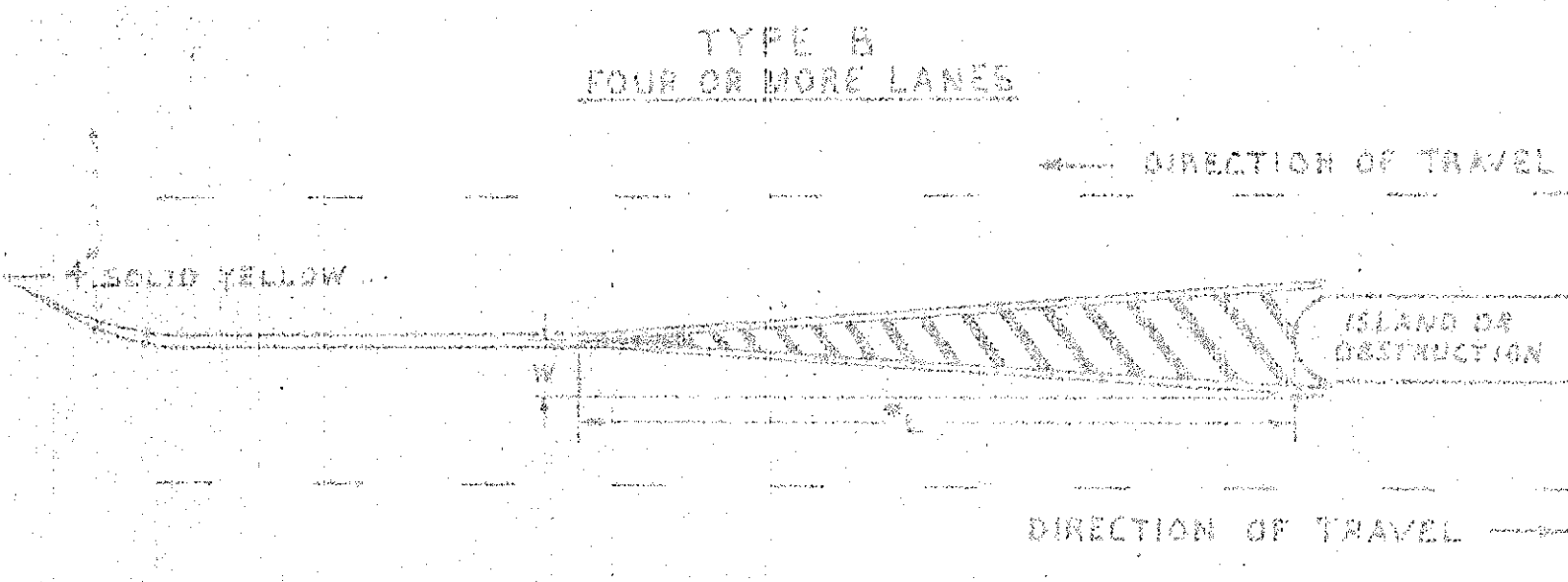
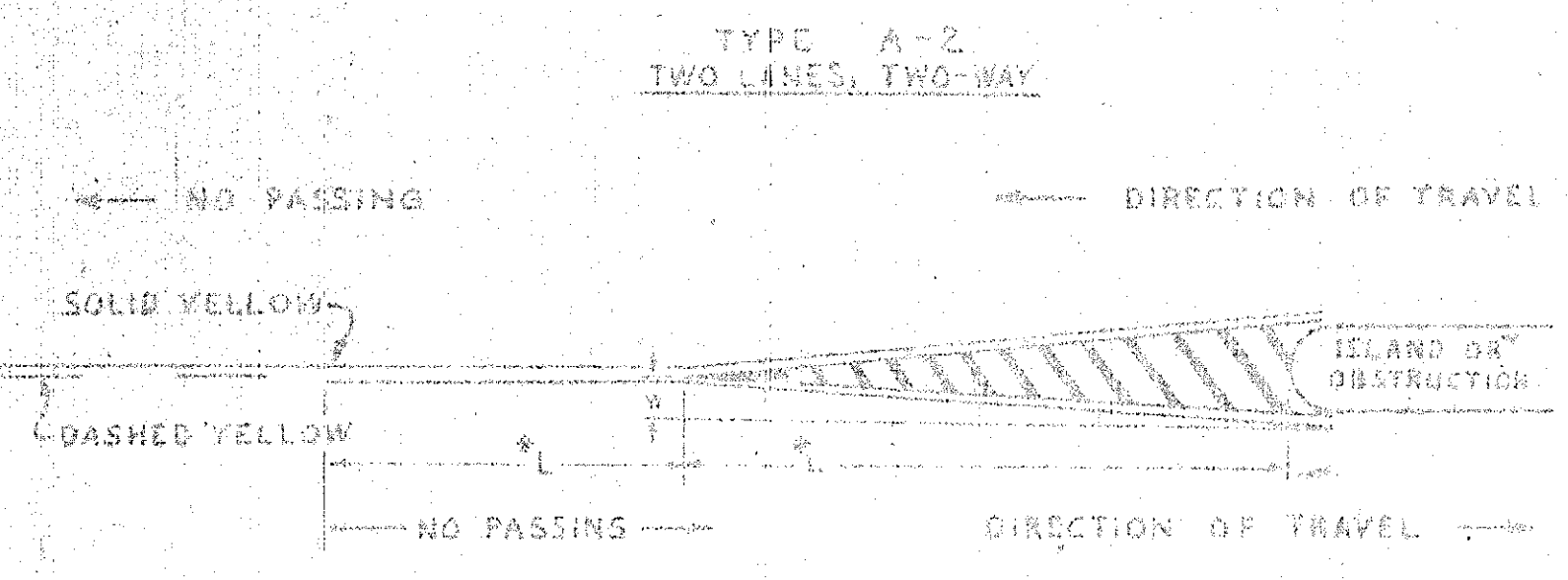
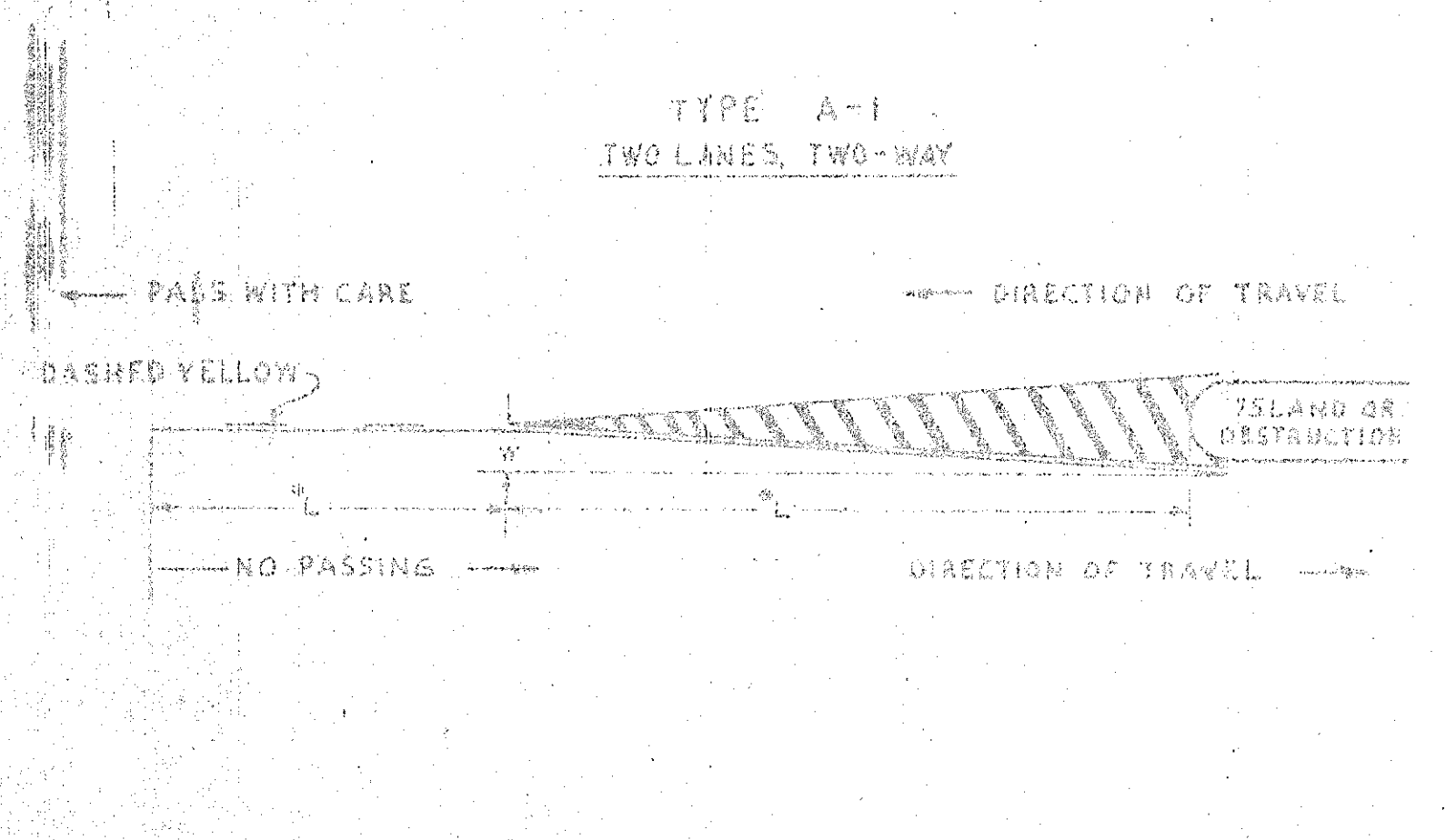
| SPD. NO. DIV. NO. | STATE | COUNTY | FILE NO. | PROJECT NO. | ROUTE No. | SHEET No. | TOTAL SHEETS |
|-------------------|-------|----------|----------|-------------|-----------|-----------|--------------|
| 3 | S.C. | CHEROKEE | 11.530.1 | 12-85-3(6) | 1-85 | 38 | 60 |

TYPICAL PAVEMENT MARKINGS

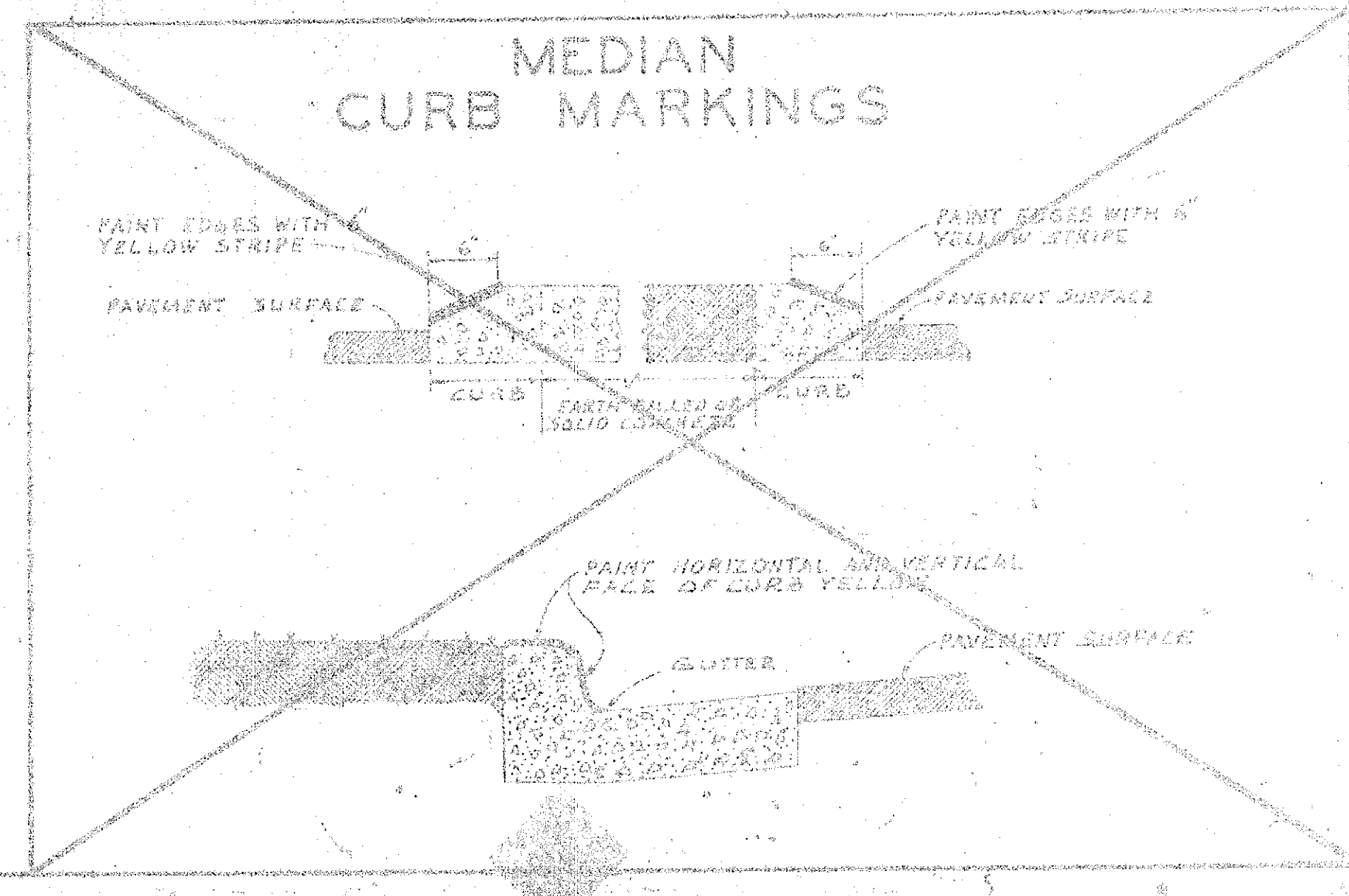
S.C. DEPT. OF HIGHWAYS AND PUBLIC TRANS., COLA., S.C.

APPROACH MARKINGS TO ISLANDS OR OBSTRUCTIONS

MEDIAN CURB MARKINGS • PAVEMENT MARKER ARROWS

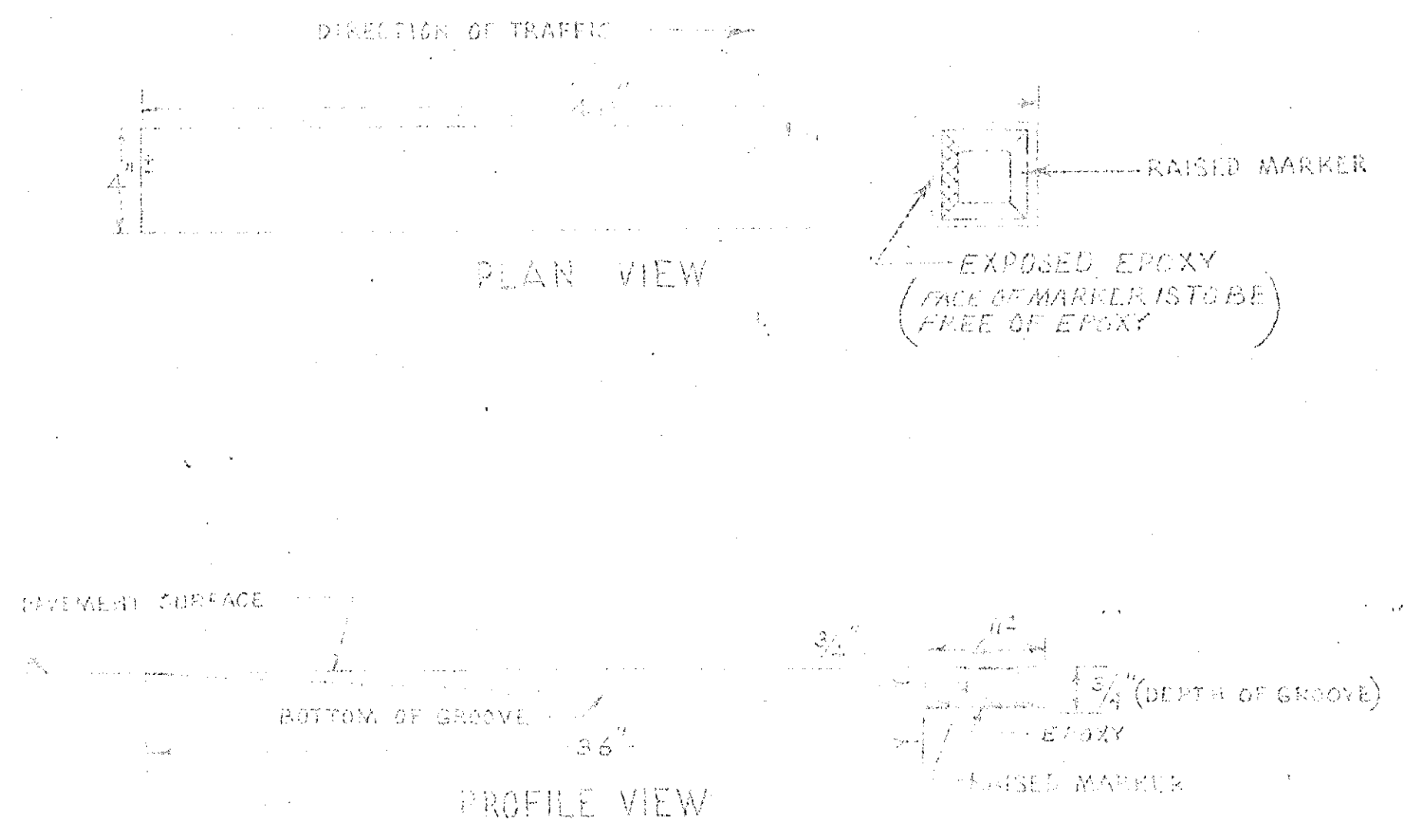


* L (FEET) = $S \cdot W$ WHERE S (MPH) IS THE SPEED OF MOST DRIVERS (USE 50TH PERCENTILE SPEED IF AVAILABLE) AND W IS THE OFF-SET. MINIMUM LENGTHS ARE 200 RURAL AND 100 URBAN. LENGTHS OF APPROACH MARKINGS ESTABLISHED BY THE FORMULA AND THE MINIMUMS ARE APPLICABLE WHERE PHYSICALLY POSSIBLE. GEOMETRIC ROADWAY DESIGN WILL OCCASIONALLY REQUIRE MARKINGS ADAPTABLE TO THE DESIGN WHICH MAY BE SHORTER THAN THE ESTABLISHED MINIMUM LENGTHS.

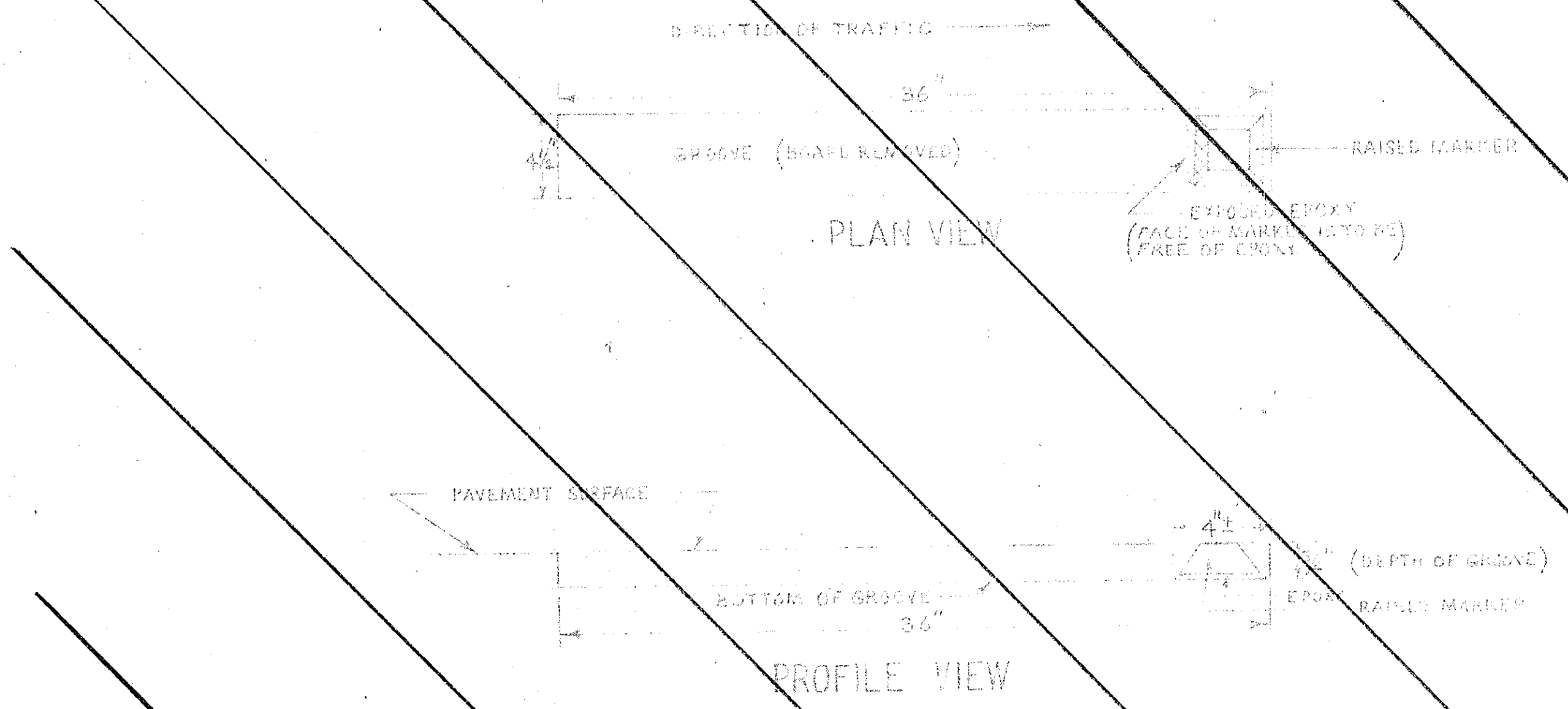


DETAIL OF RECESSED PAVEMENT MARKER SLOTS

GRINDING DETAIL



BOARD DETAIL

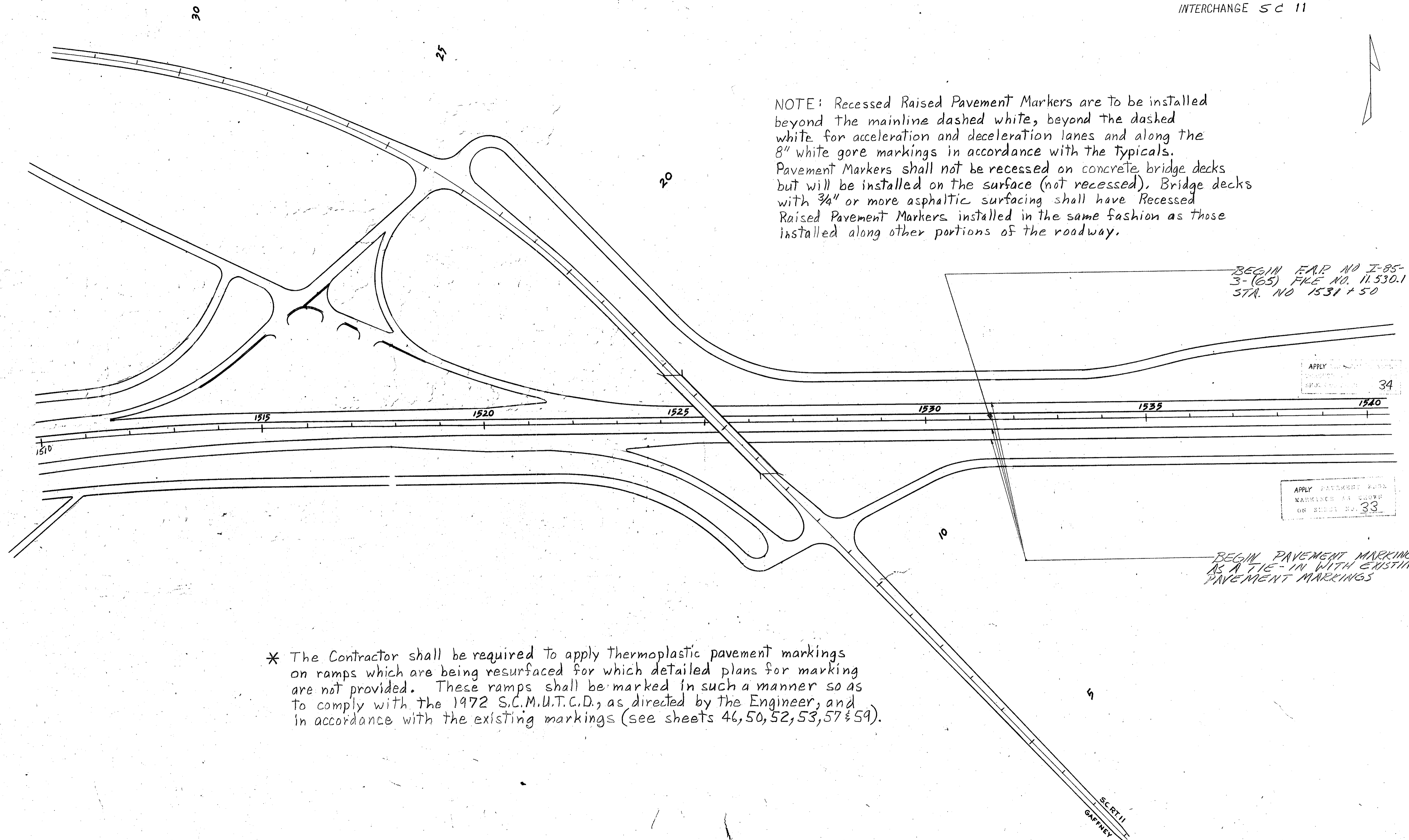


THE CONTRACTOR WILL BE REQUIRED TO GRIND A SLOT IN THE PAVEMENT 40 IN. ± LONG BY 4 IN. ± WIDE. THE SLOT SHALL TAPER CONTINUOUSLY FROM THE SURFACE OF THE PAVEMENT THROUGH A 36 IN. LENGTH REACHING A POINT 3/4 IN. IN DEPTH AT THE FACE OF THE MARKER (SIDE EXPOSED TO ONCOMING TRAFFIC). THE 3/4 IN. DEPTH SHOULD EXTEND UNTIL THE END OF THE SLOT FOR MARKER PLACEMENT. THE SLOT SHALL BE WIDE ENOUGH FOR THE MARKER TO FIT FREELY INTO SLOT AND DEEP ENOUGH TO ALLOW FOR EPOXY ADHESIVE. THE TOP OF MARKER SHOULD BE APPROX. LEVEL WITH TOP OF PAVEMENT. THIS OPERATION CAN BE ACCOMPLISHED WITH A NORTON OR CONCUT HUMP CUTTER POWERED BY A 65 H.P. GAS ENGINE, MODIFIED FOR A 4 IN. WIDE DIAMOND CUTTING HEAD OR ANY OTHER ACCEPTABLE MEANS. ANY DAMAGE TO THE PAVEMENT CAUSED BY THE GRINDING PROCESS SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE AND BY METHODS AND MATERIALS ACCEPTABLE TO THE ENGINEER. FOR DETAILS NOT SHOWN SEE THE SPECIAL PROVISIONS REGARDING RAISED PAVEMENT MARKERS.

1. THE DIMENSIONS FOR THE BOARDS ARE 1/2" x 4 1/2" x 3' "ACTUAL" DIMENSIONS.
2. THE BOARDS ARE TO BE PLACED IN THE OUTSIDE LANE WITH THE LEFT EDGE OF THE BOARDS OFFSET 1' FROM AND PARALLEL TO THE CENTER LINE.
3. THE DISTANCE BETWEEN BOARDS IS TO BE 50' FROM LEADING EDGE TO LEADING EDGE.
4. ALL BOARDS ARE TO BE PLACED IN THE BOUND AND UNBOUND LANES OF I- FROM STA. TO STA.
5. ALL BOARDS ARE TO BE PLACED SO THAT THEY WILL BE IN THE CENTER OF THE 30' GAP BETWEEN PAVEMENT MARKINGS. THE PAVEMENT MARKINGS WILL BE PLACED WITH 10' LINES AND 30' GAPS.
6. THE BOARDS ARE TO BE ATTACHED TO THE PAVEMENT JUST PRIOR TO THE PASSING OF THE PAVING MACHINE WHEN THE HOT PLANT MIX SLAB COURSE IS BEING APPLIED.
7. THE BOARDS WILL BE REMOVED AND THE RAISED REFLECTIVE PAVEMENT MARKERS INSTALLED IMMEDIATELY AFTER PASSING OF THE PAVING MACHINE AND BEFORE OPENING THE LANE TO TRAFFIC.
8. THE FINAL POSITION OF THE RAISED REFLECTIVE MARKER TOP IN THE RECESSED SLOT SHALL BE FLUSH WITH THE SURFACE OF THE PAVEMENT TRAVELWAY. THIS MAY BE ACHIEVED BY ADJUSTING THE AMOUNT OF EPOXY USED IN SECURING THE MARKER IN THE RECESSED SLOT. THIS ADJUSTMENT OF THE EPOXY SHALL NOT AFFECT THE OTHERWISE STABLE BONDING OF THE RAISED MARKER TO THE BOTTOM OF THE SLOT. THE ENTIRE BOTTOM SURFACE OF THE RAISED MARKER SHALL CONTACT THE EPOXY ADHESIVE.
9. THE 3/4" DEPTH REPRESENTS 1/2" THICKNESS OF THE BOARD PLUS A 1/4" THICKNESS OF HOT PLANT MIX SLAB COURSE APPLIED ON TOP OF THE BOARD. THE 1/2" THICKNESS OF HOT PLANT MIX IS REMOVED ALONG WITH THE BOARD.

| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 5 | S. C. | CHEROKEE | 11.530.1 | 12-85-3(6) | I-85 | 40 | 60 |

INTERCHANGE SC 11



NOTE: Recessed Raised Pavement Markers are to be installed beyond the mainline dashed white, beyond the dashed white for acceleration and deceleration lanes and along the 8" white gore markings in accordance with the Typical. Pavement Markers shall not be recessed on concrete bridge decks but will be installed on the surface (not recessed). Bridge decks with 3/4" or more asphaltic surfacing shall have Recessed Raised Pavement Markers installed in the same fashion as those installed along other portions of the roadway.

BEGIN F.A.P. NO I-85-3-(65) FILE NO. 11.530.1 STA. NO 1531 + 50

APPLY MARKINGS AS SHOWN ON SHEET NO. 34

APPLY PAVEMENT MARKINGS AS SHOWN ON SHEET NO. 33

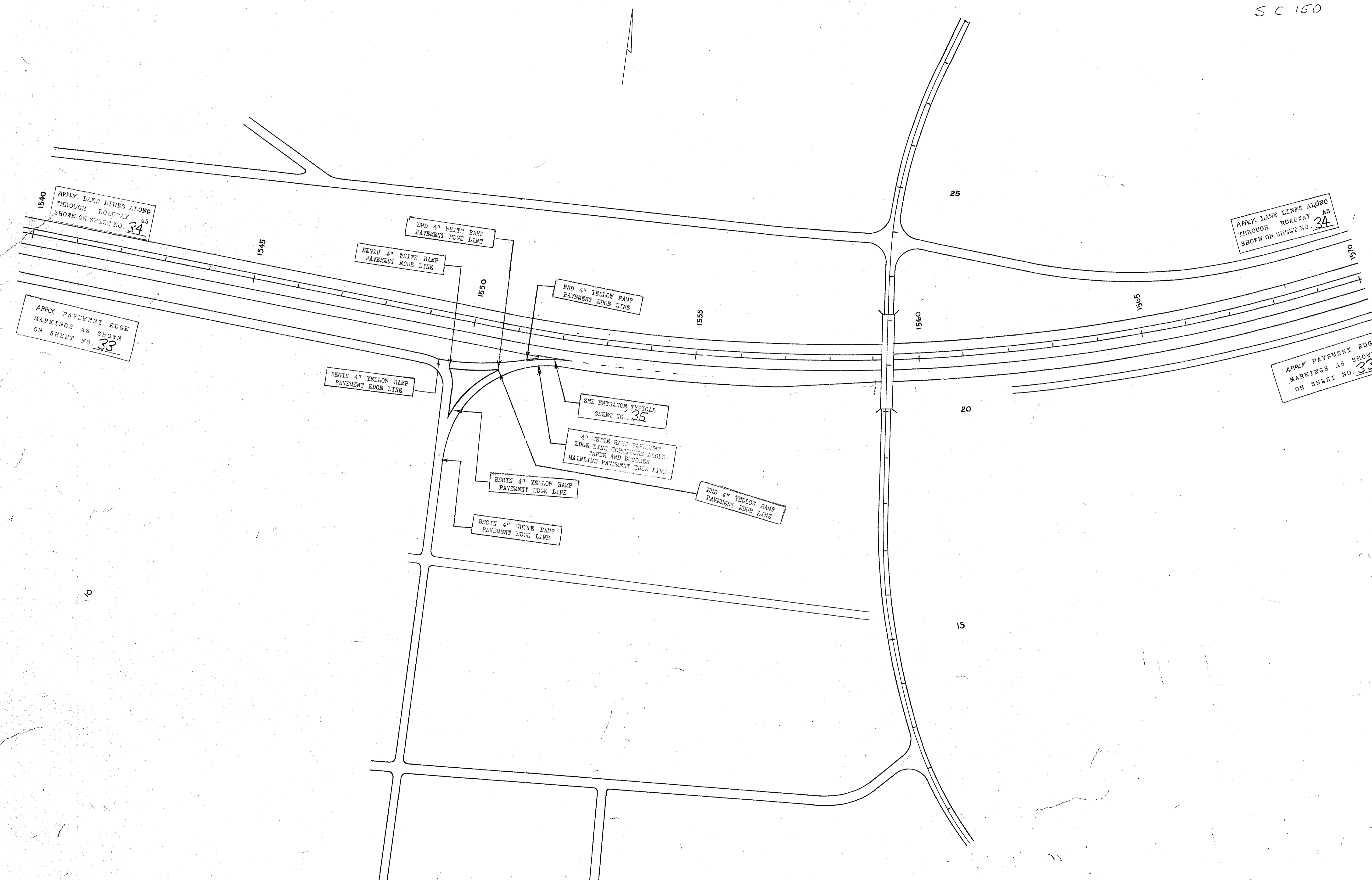
BEGIN PAVEMENT MARKINGS AS A TIE-IN WITH EXISTING PAVEMENT MARKINGS

* The Contractor shall be required to apply thermoplastic pavement markings on ramps which are being resurfaced for which detailed plans for marking are not provided. These ramps shall be marked in such a manner so as to comply with the 1972 S.C.M.U.T.C.D., as directed by the Engineer, and in accordance with the existing markings (see sheets 46, 50, 52, 53, 57 & 59).

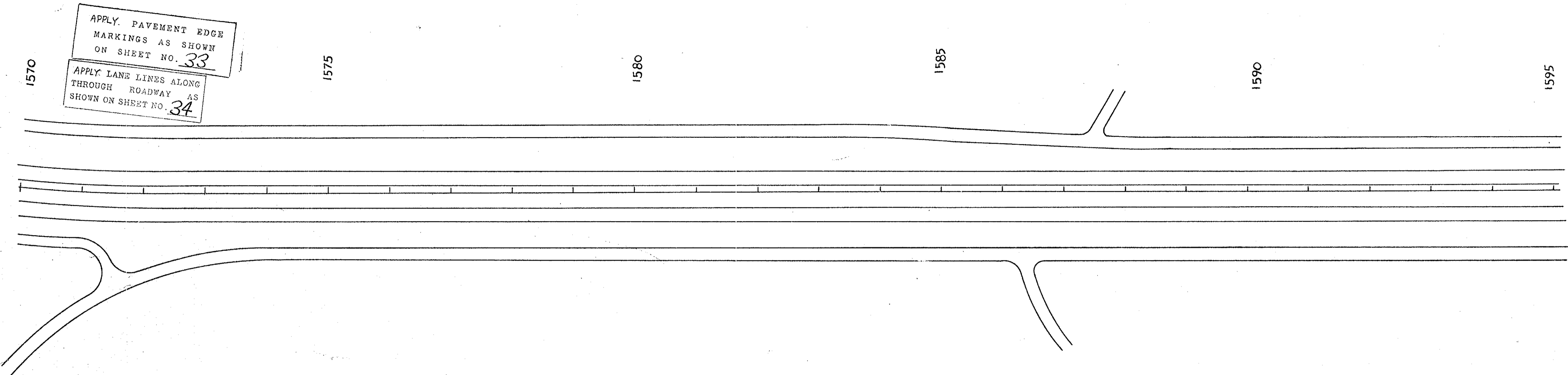
SECRET
GAFFNEY

| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11,580.1 | 12 85-3(4) | I-85 | 41 | 60 |

5 C 150



| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|----------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11,580.1 | 12 85-3(65) | I-85 | 42 | 60 |



APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

1570

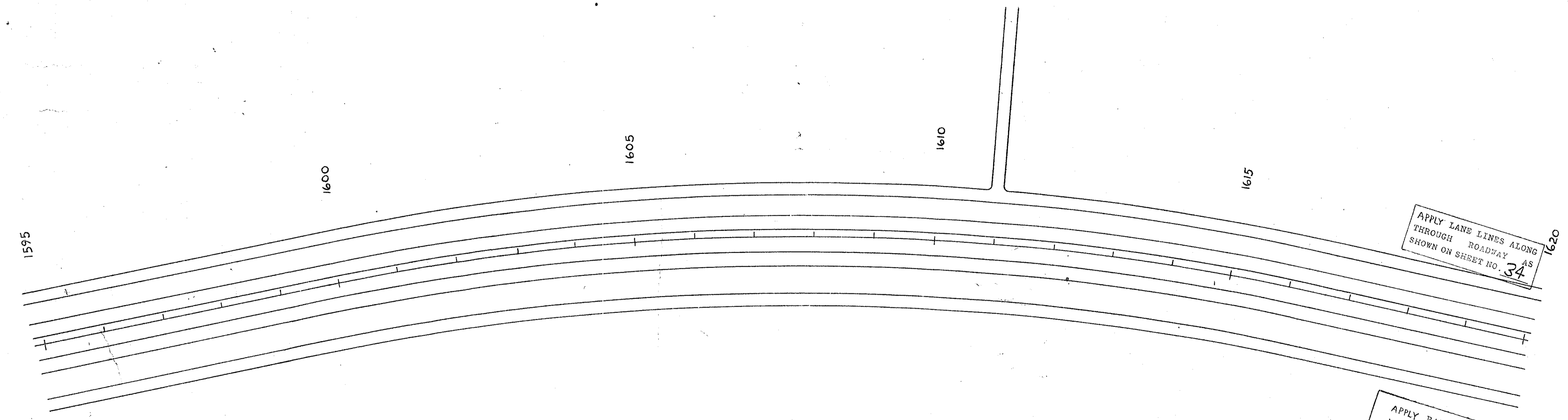
1575

1580

1585

1590

1595



APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

1595

1600

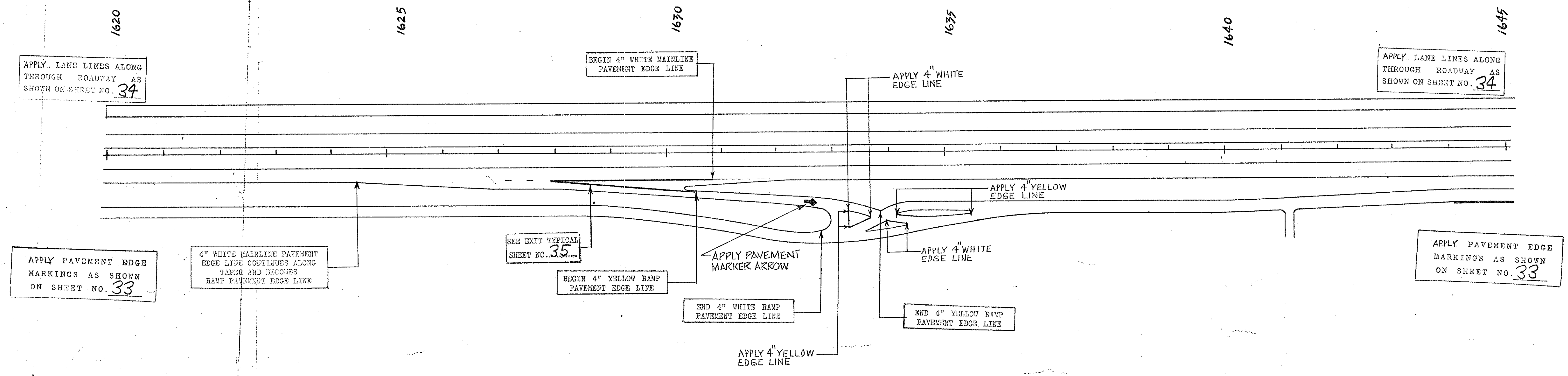
1605

1610

1615

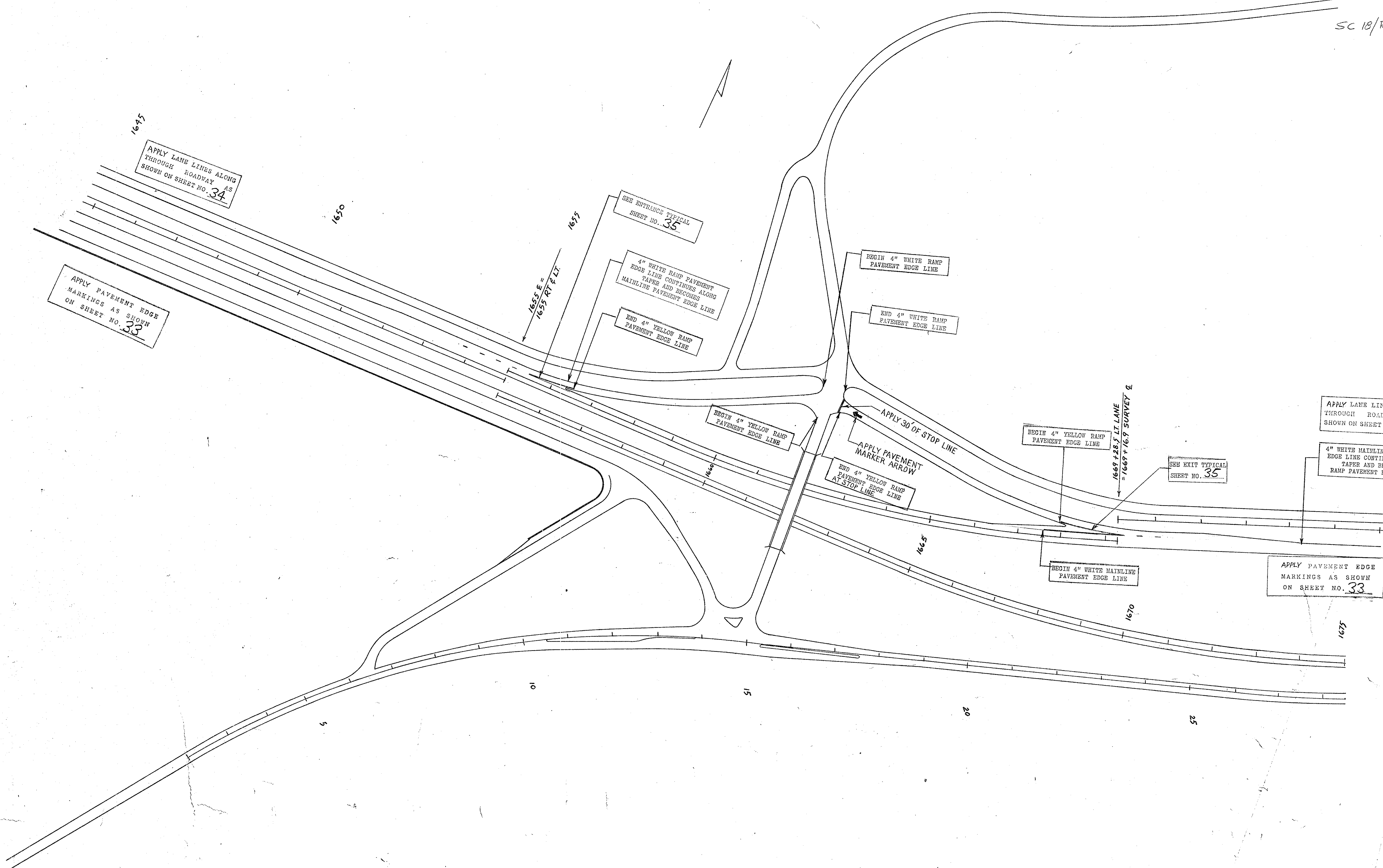
1620

| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | G. C. | CHEROKEE | 11,530.1 | 12-85.3(60) | I-85 | 43 | 60 |



| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11,530.1 | 85-3(6) | I-85 | 44 | 60 |

SC 18/ROAD 82



1645
APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

SEE ENTRANCE TYPICAL SHEET NO. 35

4" WHITE RAMP PAVEMENT EDGE LINE CONTINUES ALONG TAPER AND BECOMES MAINLINE PAVEMENT EDGE LINE

END 4" YELLOW RAMP PAVEMENT EDGE LINE

BEGIN 4" WHITE RAMP PAVEMENT EDGE LINE

END 4" WHITE RAMP PAVEMENT EDGE LINE

BEGIN 4" YELLOW RAMP PAVEMENT EDGE LINE

APPLY PAVEMENT MARKER ARROW
END 4" YELLOW RAMP PAVEMENT EDGE LINE AT STOP LINE

APPLY 30' OF STOP LINE

BEGIN 4" YELLOW RAMP PAVEMENT EDGE LINE

SEE EXIT TYPICAL SHEET NO. 35

APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

4" WHITE MAINLINE PAVEMENT EDGE LINE CONTINUES ALONG TAPER AND BECOMES RAMP PAVEMENT EDGE LINE

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

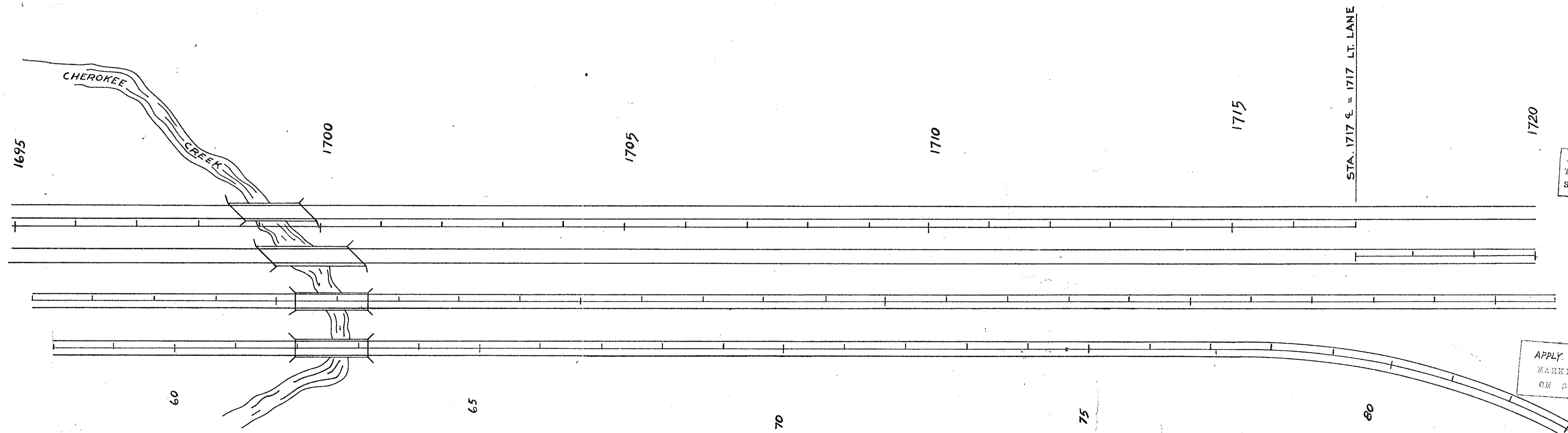
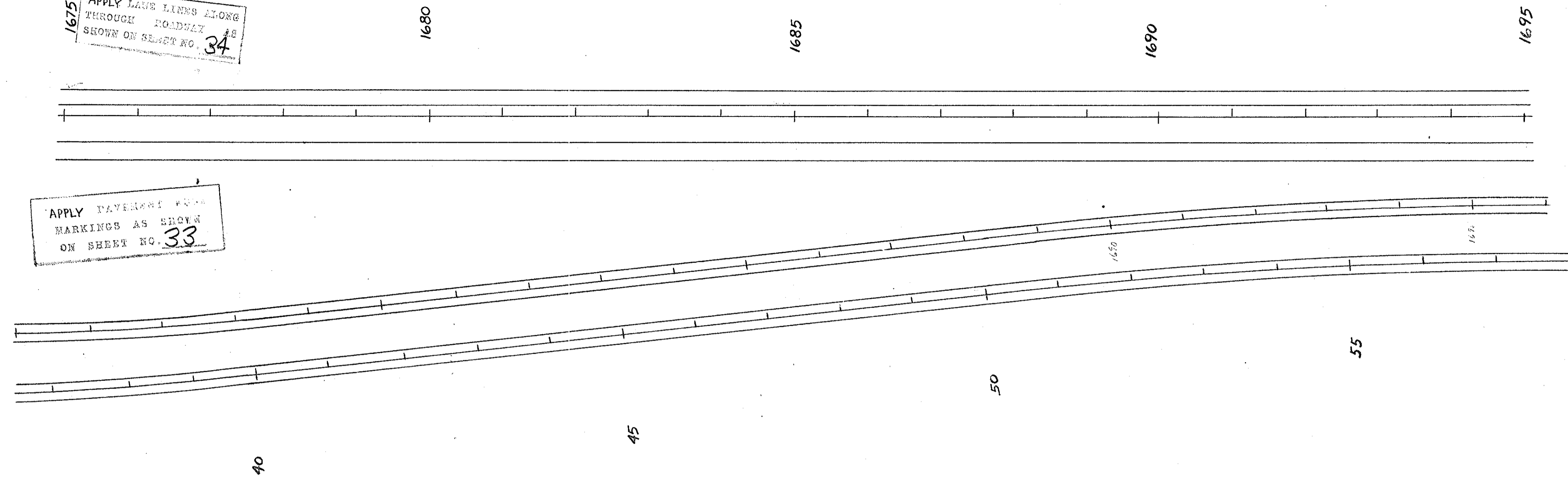
1669 + 28.5 LT. LANE = 1669 + 16.9 SURVEY ±

BEGIN 4" WHITE MAINLINE PAVEMENT EDGE LINE

| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | 12 85-3(6) | I-85 | 45 | 60 |

1675
 APPLY LANE LINES ALONG
 THROUGH ROADWAY AS
 SHOWN ON SHEET NO. 34

APPLY PAVEMENT EDGE
 MARKINGS AS SHOWN
 ON SHEET NO. 33



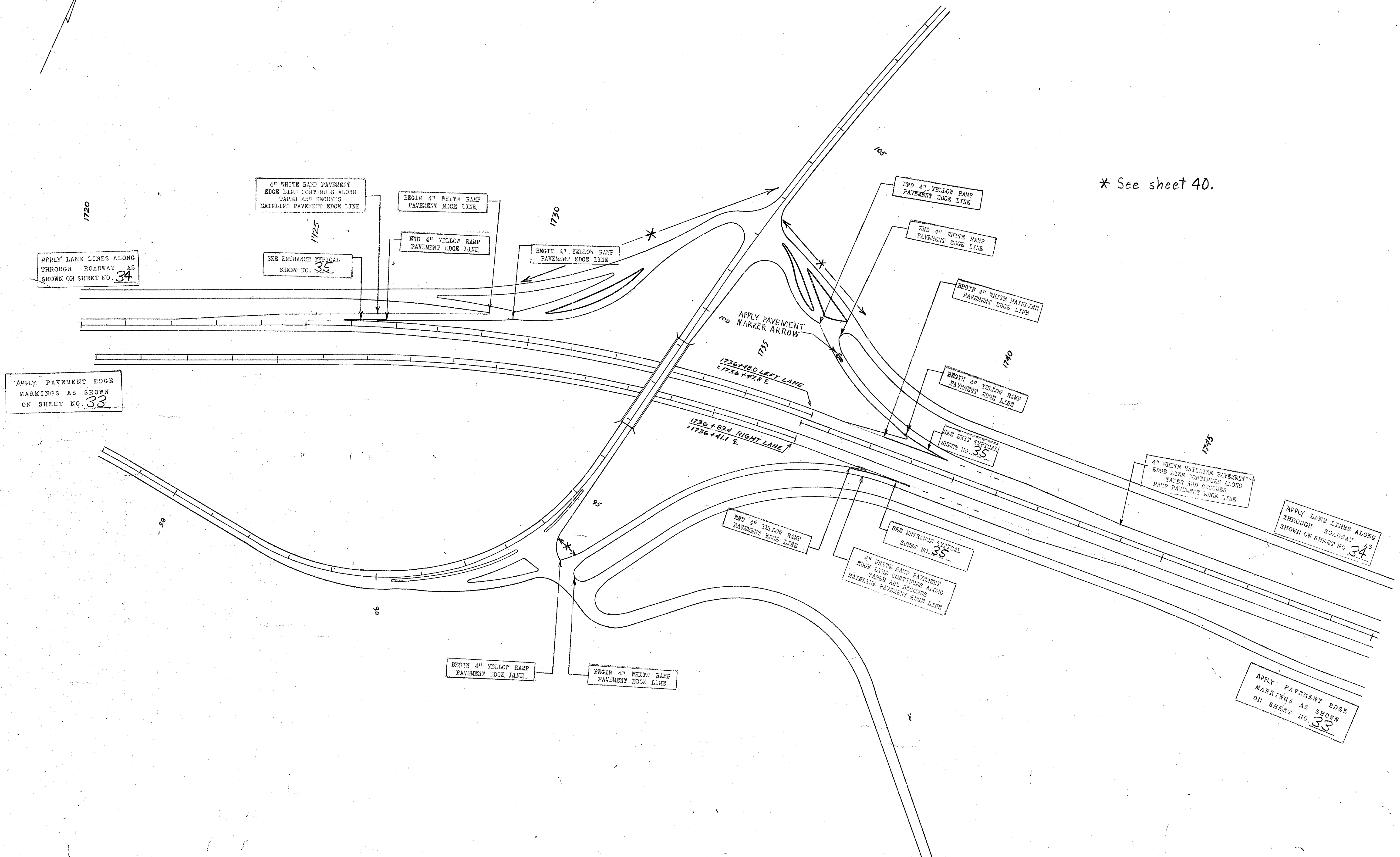
APPLY LANE LINES ALONG
 THROUGH ROADWAY AS
 SHOWN ON SHEET NO. 34

APPLY PAVEMENT EDGE
 MARKINGS AS SHOWN
 ON SHEET NO. 33

| FED. ROAD DIST. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S.C. | CHEROKEE | 11,530.1 | 12-85-3(68) | I-85 | 46 | 60 |

INTERCHANGE S.C. 18

* See sheet 40.



APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

4" WHITE RAMP PAVEMENT EDGE LINE CONTIGUES ALONG TAPER AND BECOMES MAINLINE PAVEMENT EDGE LINE

BEGIN 4" WHITE RAMP PAVEMENT EDGE LINE

END 4" YELLOW RAMP PAVEMENT EDGE LINE

BEGIN 4" YELLOW RAMP PAVEMENT EDGE LINE

END 4" YELLOW RAMP PAVEMENT EDGE LINE

END 4" WHITE RAMP PAVEMENT EDGE LINE

BEGIN 4" WHITE MAINLINE PAVEMENT EDGE LINE

BEGIN 4" YELLOW RAMP PAVEMENT EDGE LINE

SEE EXIT TYPICAL SHEET NO. 35

4" WHITE MAINLINE PAVEMENT EDGE LINE CONTIGUES ALONG TAPER AND BECOMES RAMP PAVEMENT EDGE LINE

APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

APPLY PAVEMENT MARKER ARROW

1736+40.0 LEFT LANE = 1736+77.8 E

1736+89.4 RIGHT LANE = 1736+111.2 E

SEE ENTRANCE TYPICAL SHEET NO. 35

4" WHITE RAMP PAVEMENT EDGE LINE CONTIGUES ALONG TAPER AND BECOMES MAINLINE PAVEMENT EDGE LINE

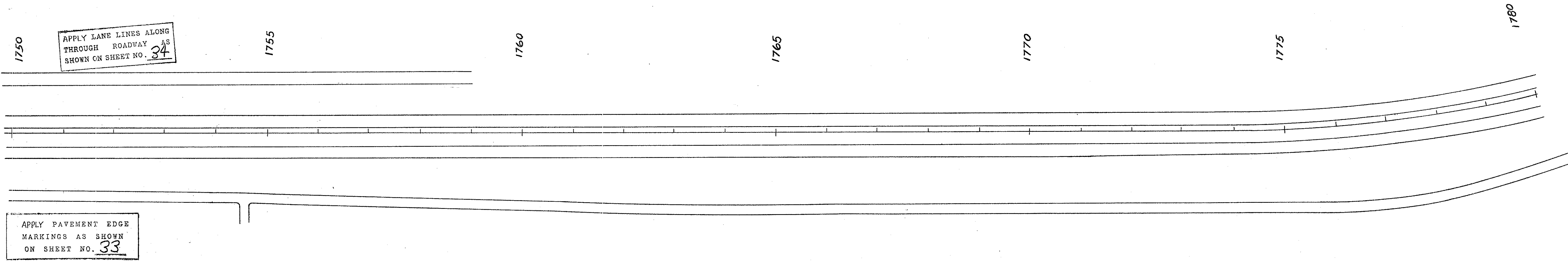
END 4" YELLOW RAMP PAVEMENT EDGE LINE

BEGIN 4" YELLOW RAMP PAVEMENT EDGE LINE

BEGIN 4" WHITE RAMP PAVEMENT EDGE LINE

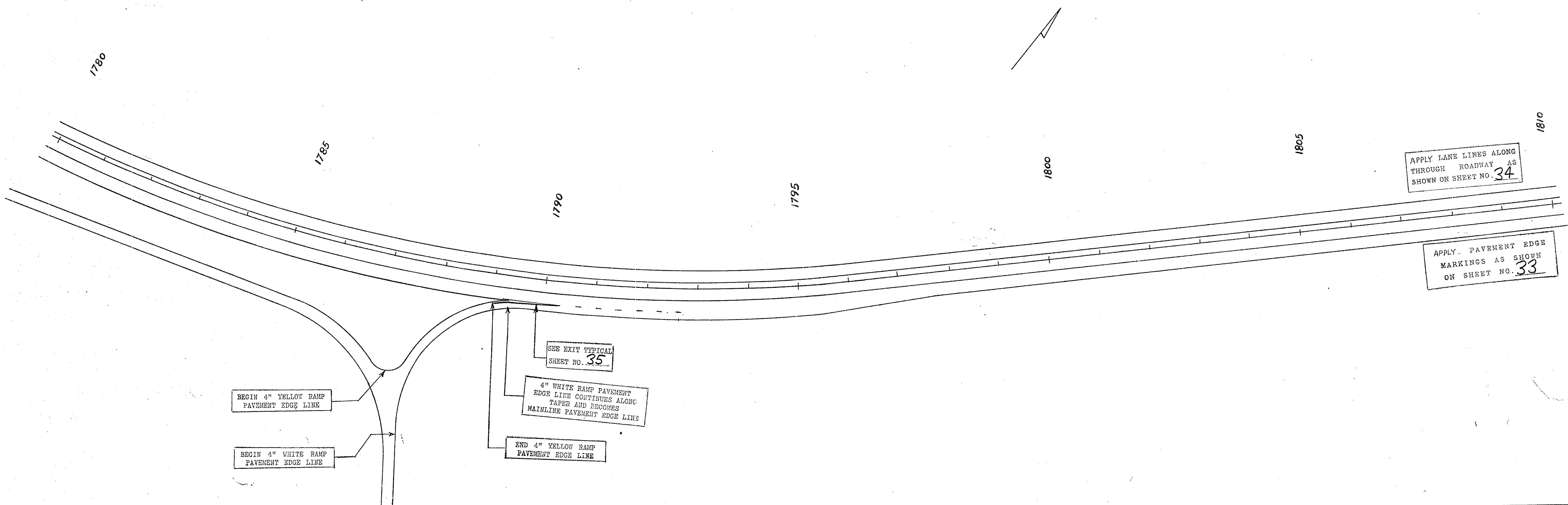
APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | 12 85.3(6) | I-85 | 47 | 60 |



APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33



APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

BEGIN 4" YELLOW RAMP PAVEMENT EDGE LINE

BEGIN 4" WHITE RAMP PAVEMENT EDGE LINE

SEE EXIT TYPICAL SHEET NO. 35

4" WHITE RAMP PAVEMENT EDGE LINE CONTINUES ALONG TAPER AND BECOMES MAINLINE PAVEMENT EDGE LINE

END 4" YELLOW RAMP PAVEMENT EDGE LINE

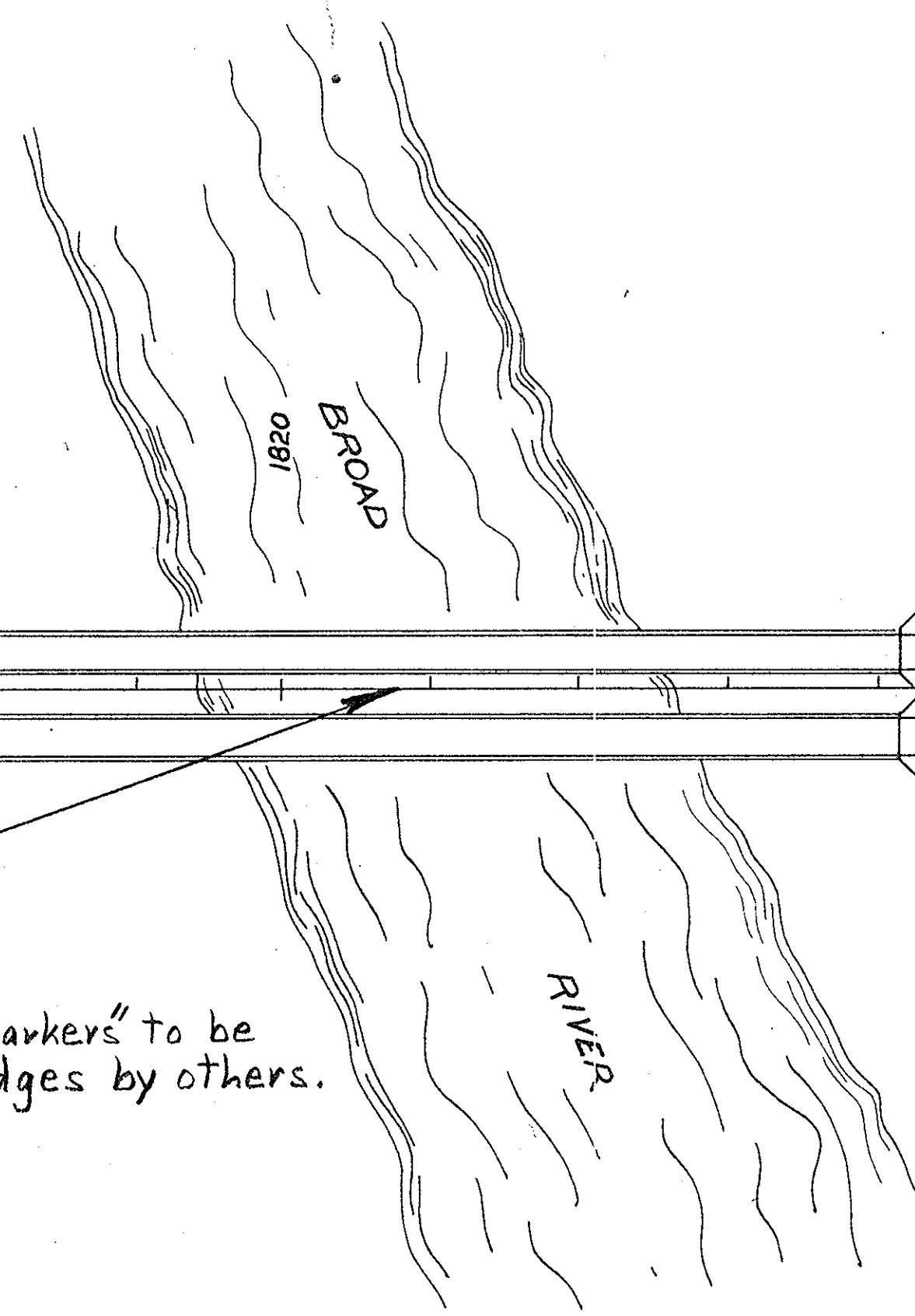
| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | 12.85.2(48) | I-85 | 48 | 60 |

1810 1815 1820 1825 1830 1835 1840

APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

NOTE: "Raised Markers" to be installed on bridges by others.



4" WHITE MAINLINE PAVEMENT EDGE LINE CONTINUES ALONG TAPER AND BECOMES RAMP PAVEMENT EDGE LINE

SEE EXIT TYPICAL SHEET NO. 36

BEGIN 4" YELLOW RAMP PAVEMENT EDGE LINE

BEGIN 4" WHITE MAINLINE PAVEMENT EDGE LINE

END 4" YELLOW RAMP PAVEMENT EDGE LINE

END 4" WHITE RAMP PAVEMENT EDGE LINE

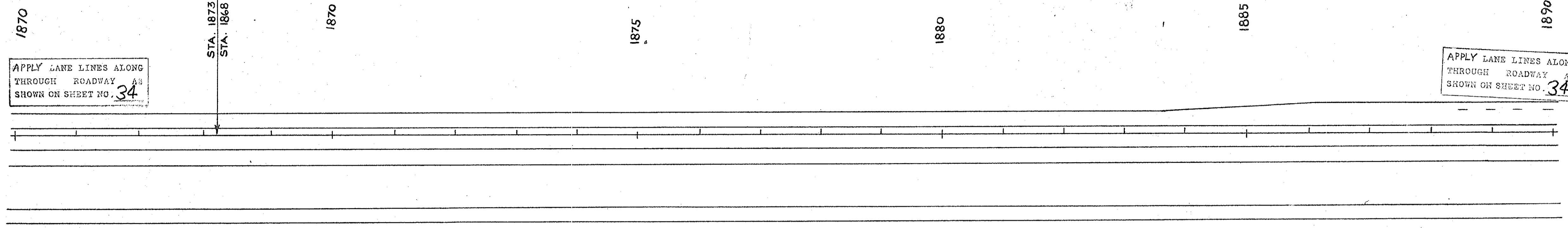
APPLY PAVEMENT MARKER ARROW

1840 1845 1850 1855 1860 1865 1870

APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

| FED. ROAD DIV. NO. | STATE | COUNTY | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|-------------|---------------------|-----------|--------------|
| 3 | S.C. | CHEROKEE | 11.530.1 | 12 85.3(65) I-85 | 49 | 60 |



APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

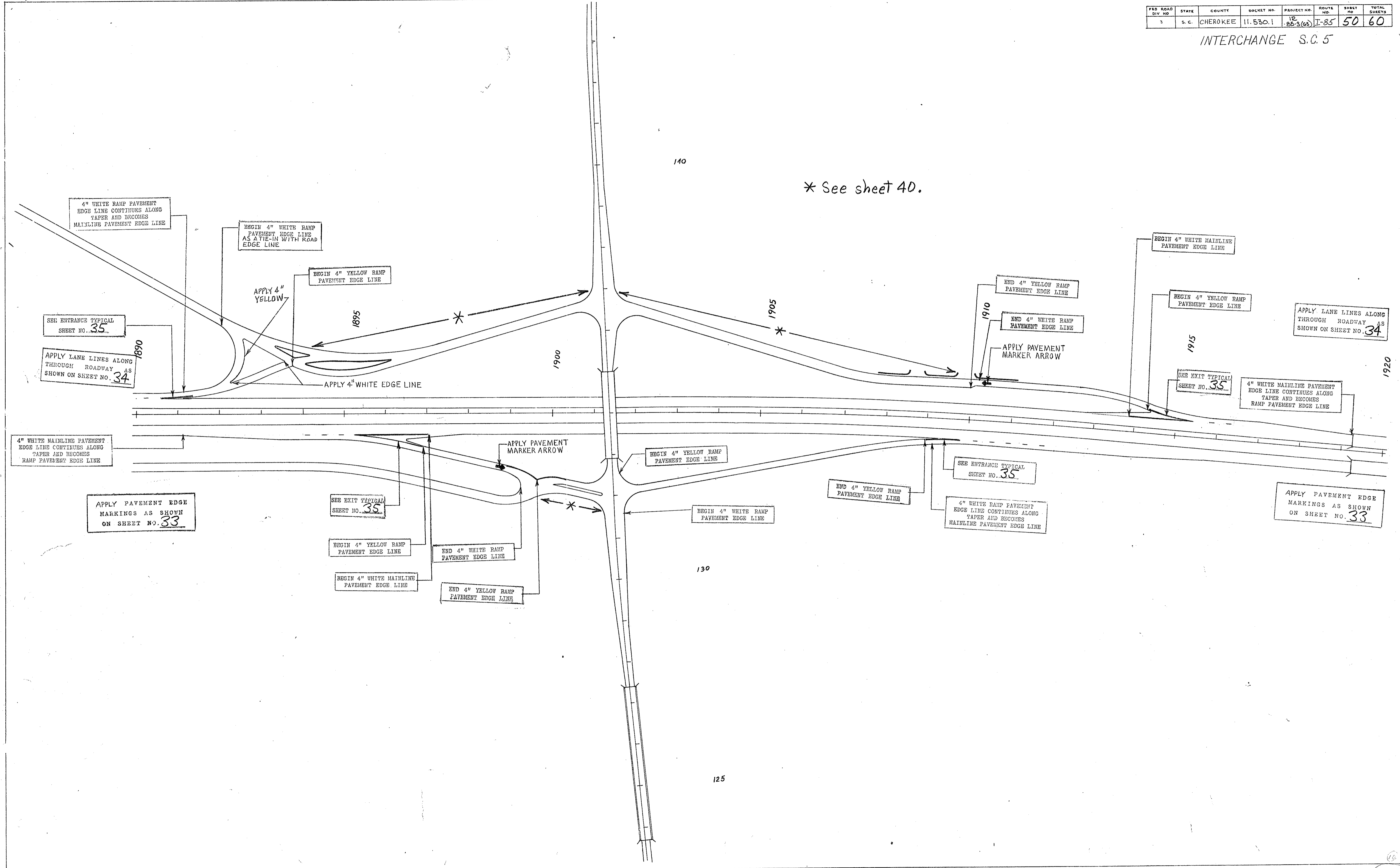
APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

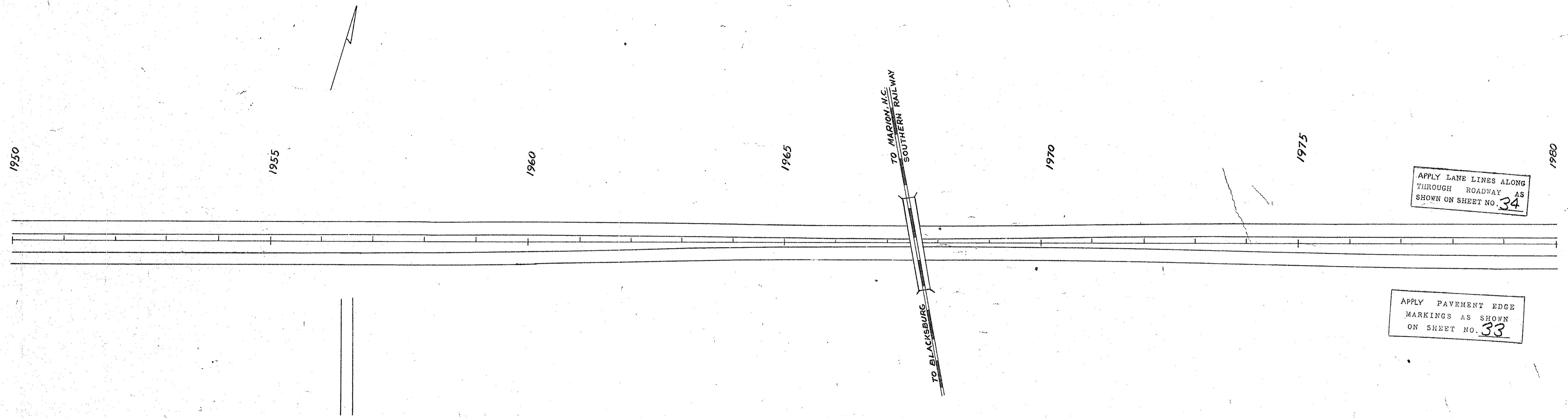
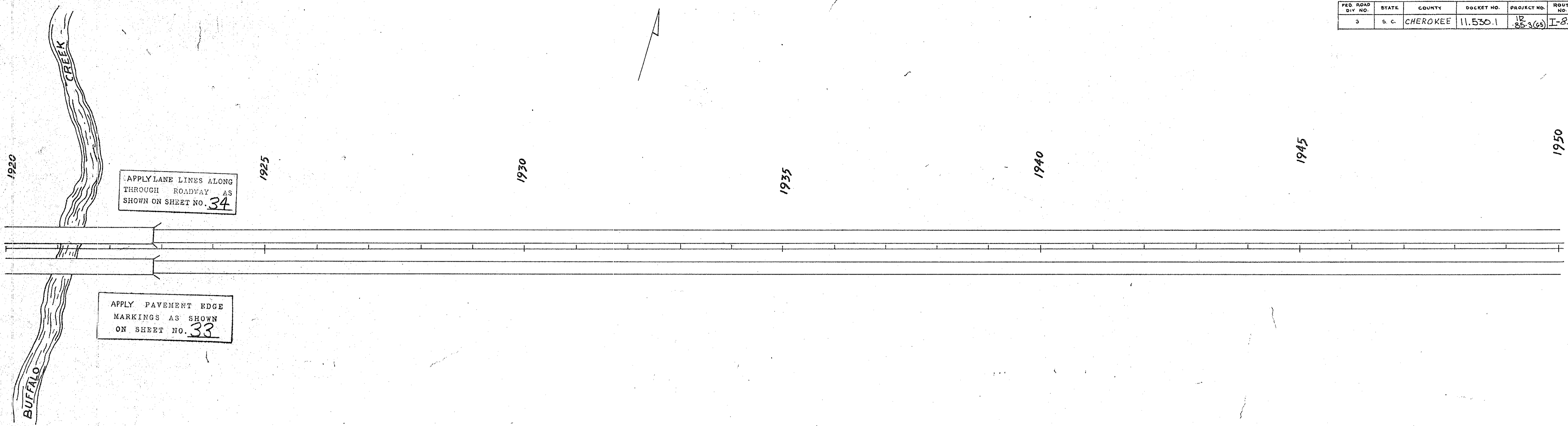
| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | 12-85-3(65) | I-85 | 50 | 60 |

INTERCHANGE S.C. 5

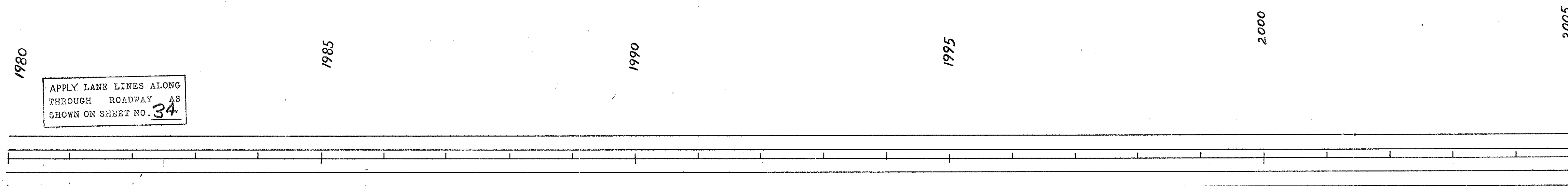
* See sheet 40.



| FED. ROAD DIST. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11,530.1 | 10-85-3(6) | I-85 | 51 | 60 |

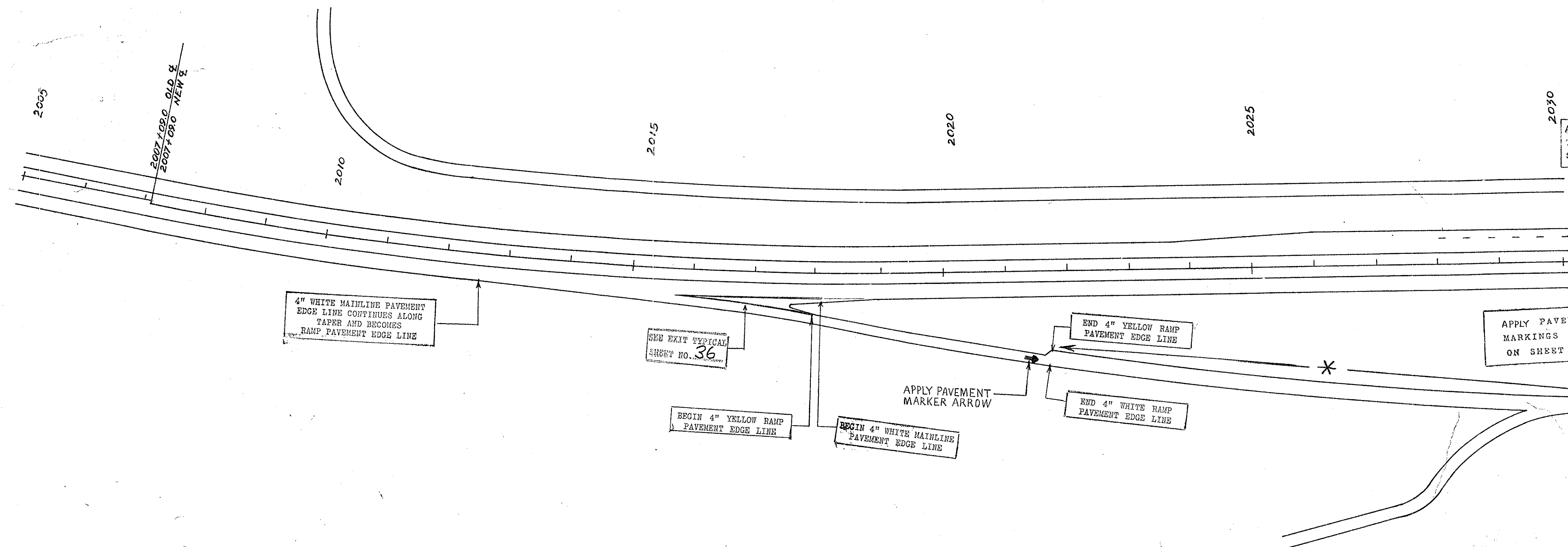


| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 2 | S. C. | CHEROKEE | 11.530.1 | 12 85-2(43) | I-85 | 52 | 60 |



APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33



APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

4" WHITE MAINLINE PAVEMENT EDGE LINE CONTINUES ALONG TAPER AND BECOMES RAMP PAVEMENT EDGE LINE

SEE EXIT TYPICAL SHEET NO. 36

BEGIN 4" YELLOW RAMP PAVEMENT EDGE LINE

BEGIN 4" WHITE MAINLINE PAVEMENT EDGE LINE

APPLY PAVEMENT MARKER ARROW

END 4" YELLOW RAMP PAVEMENT EDGE LINE

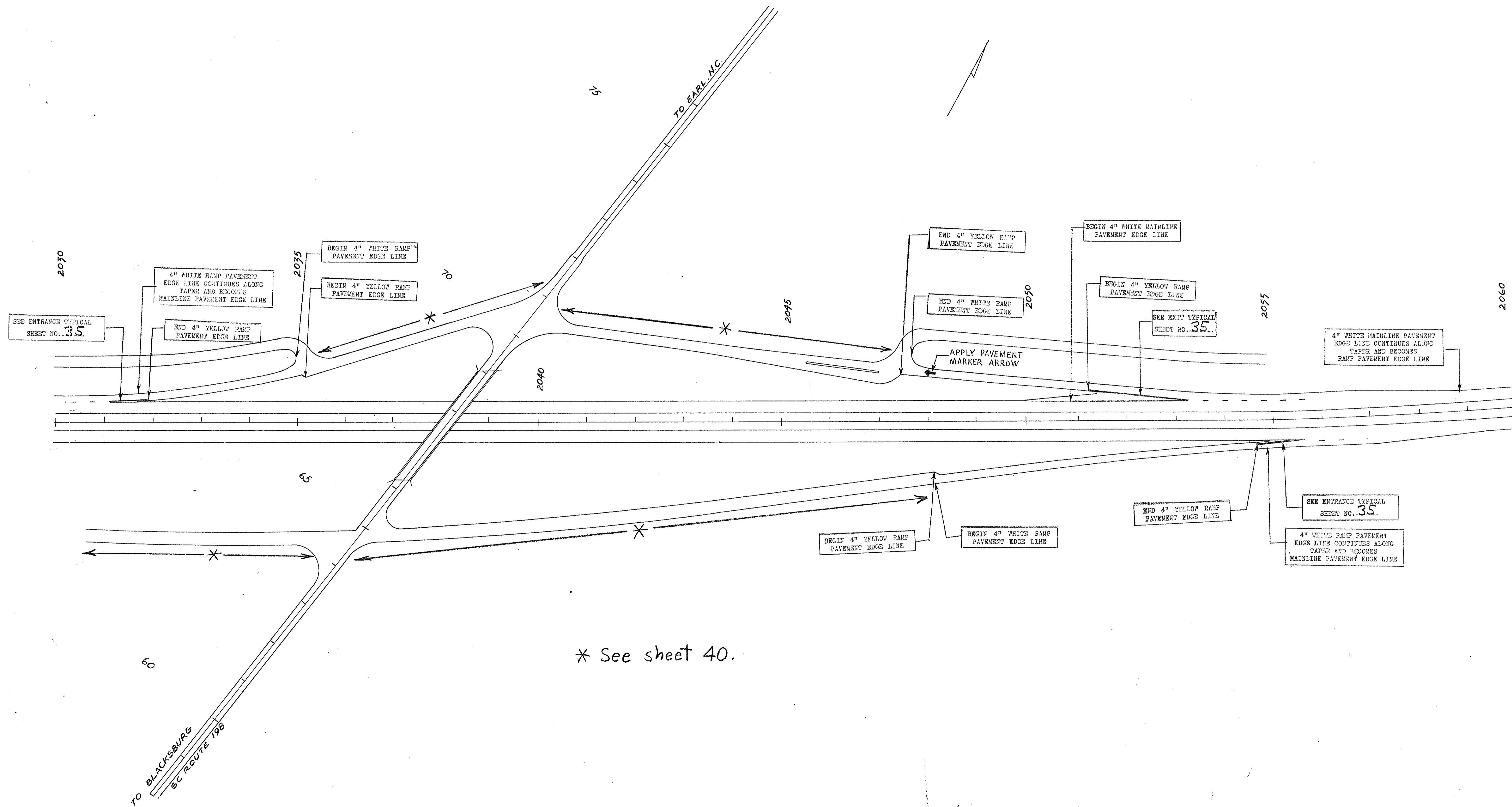
END 4" WHITE RAMP PAVEMENT EDGE LINE

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

* See sheet 40.

| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|----------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | 12 85.3(45) | I-85 | 53 | 60 |

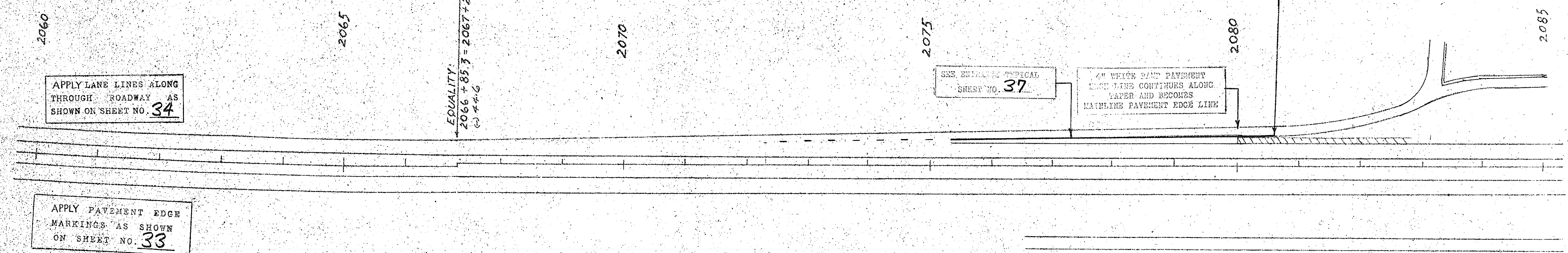
INTERCHANGE S.C. 198



* See sheet 40.

| FED. ROAD DIV. NO. | STATE | COUNTY | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | I-85 | 54 | 60 |

WELCOME CENTER ENTRANCE RAMP
(SEE SHEET No. 55)



APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

EQUALITY:
2066 + 85.3 = 2067 + 29.9
@ #46

SEE ENTRANCE TYPICAL SHEET NO. 37

4\"/>

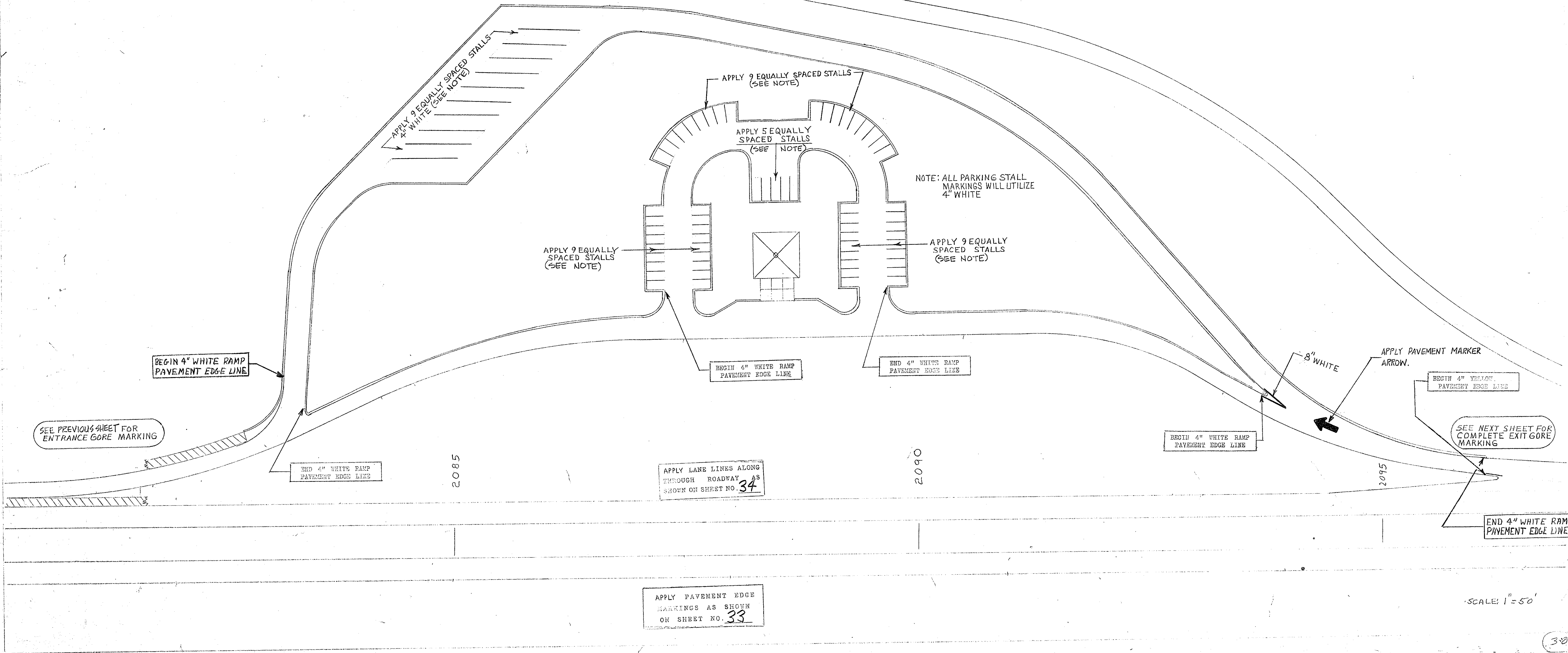
END 4\"/>

APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

| FED. ROAD DIV. NO. | STATE | COUNTY | FILE NO. | PROJ. NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|----------|-------------|-----------|-----------|--------------|
| 3 | S.C. | CHEROKEE | 11-530.1 | 12-85.3(68) | I-85 | 55 | 60 |

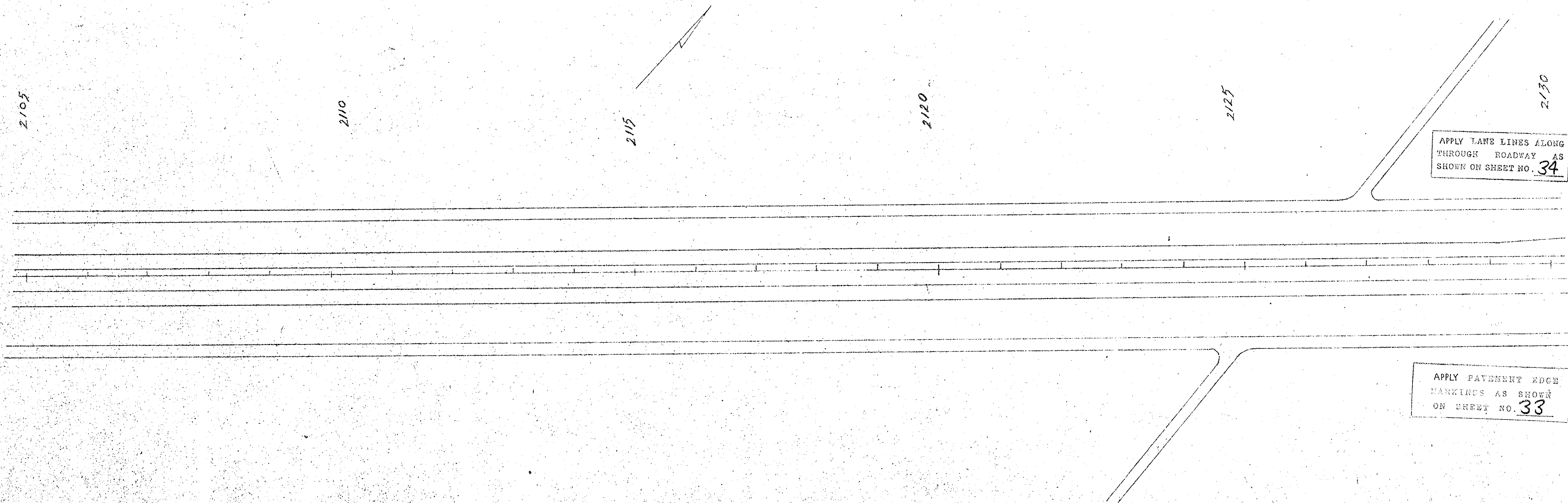
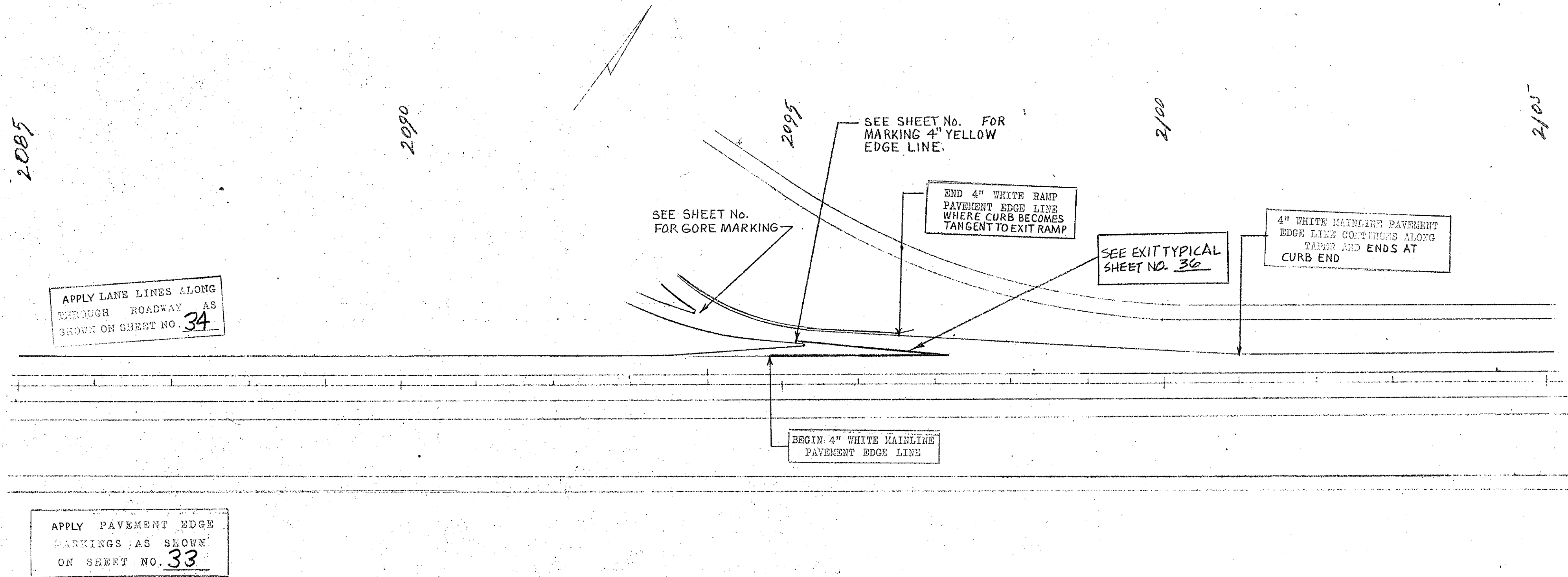
NOTE:
USE EXISTING PAINT MARKINGS AS GUIDE FOR APPLYING THERMOPLASTIC MARKINGS ON PARKING STALLS.



APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

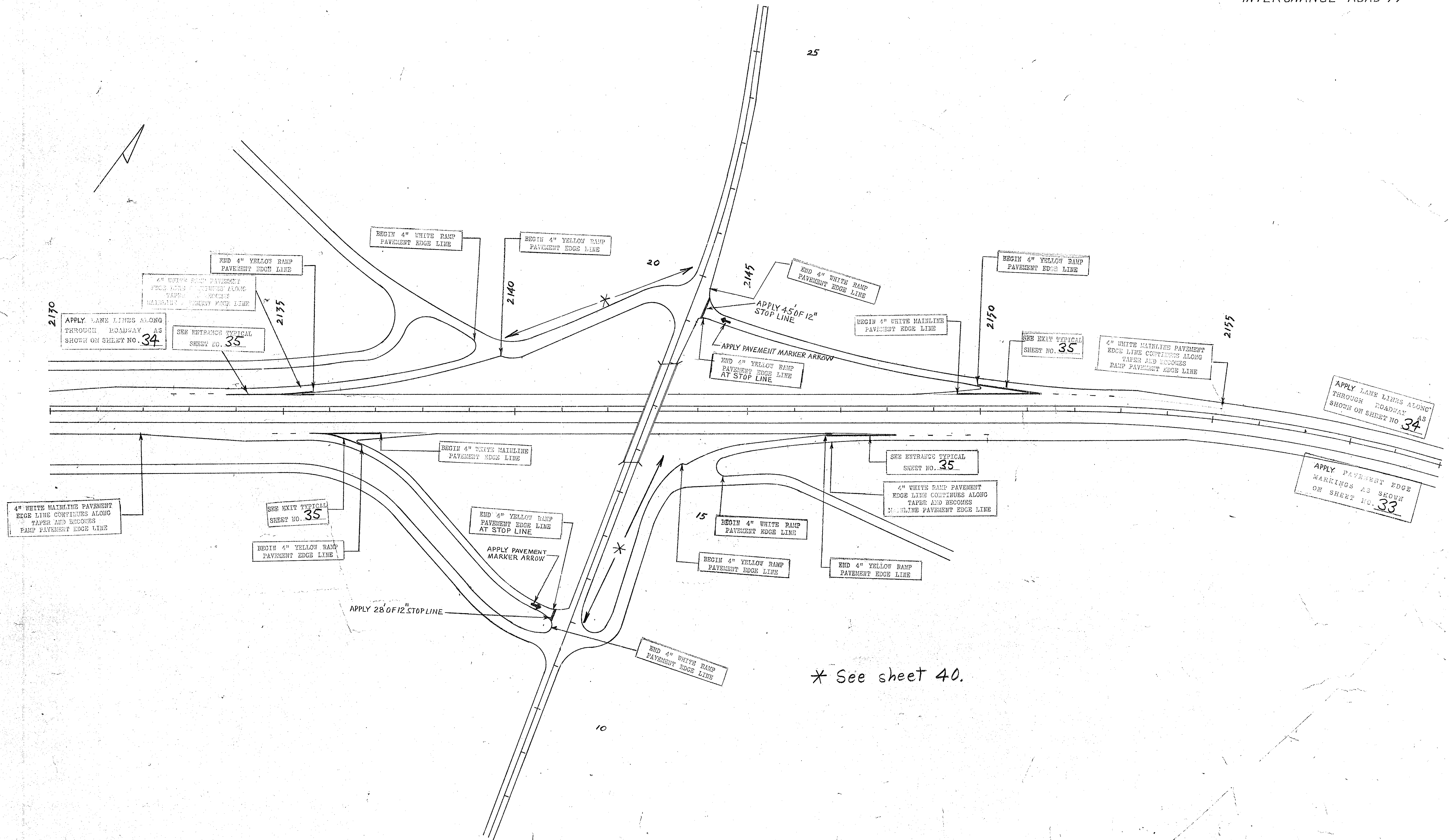
SCALE: 1" = 50'

| FED. ROAD DIST. NO. | STATE | COUNTY | DISTRICT NO. | PROJECT NO. | ROUTE | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------|--------------|-------------|-------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | IR 85.3(6) | I-85 | 56 | 60 |



| FED. ROAD DIST. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S.C. | CHEROKEE | 11,530-1 | 12 85-3(65) | I-85 | 57 | 60 |

INTERCHANGE ROAD 99

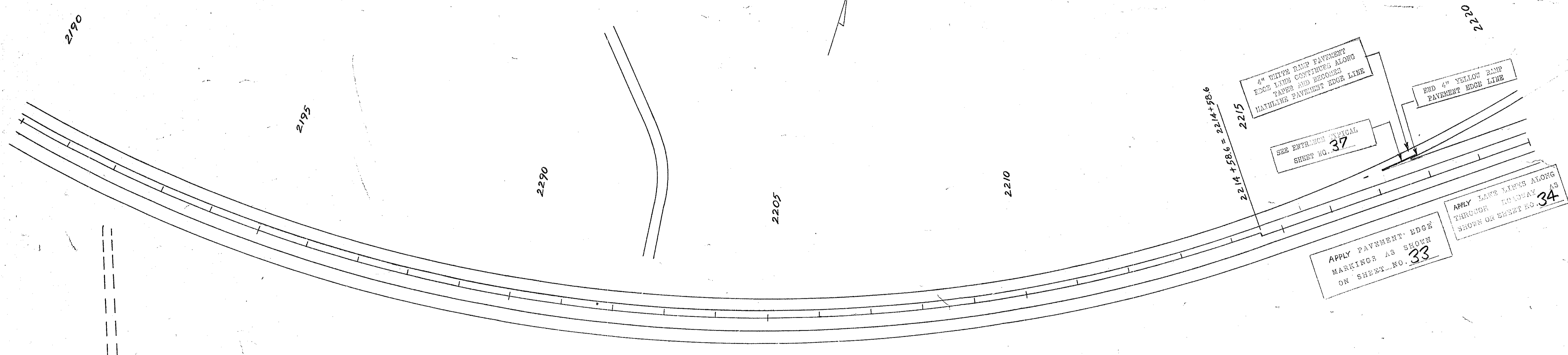
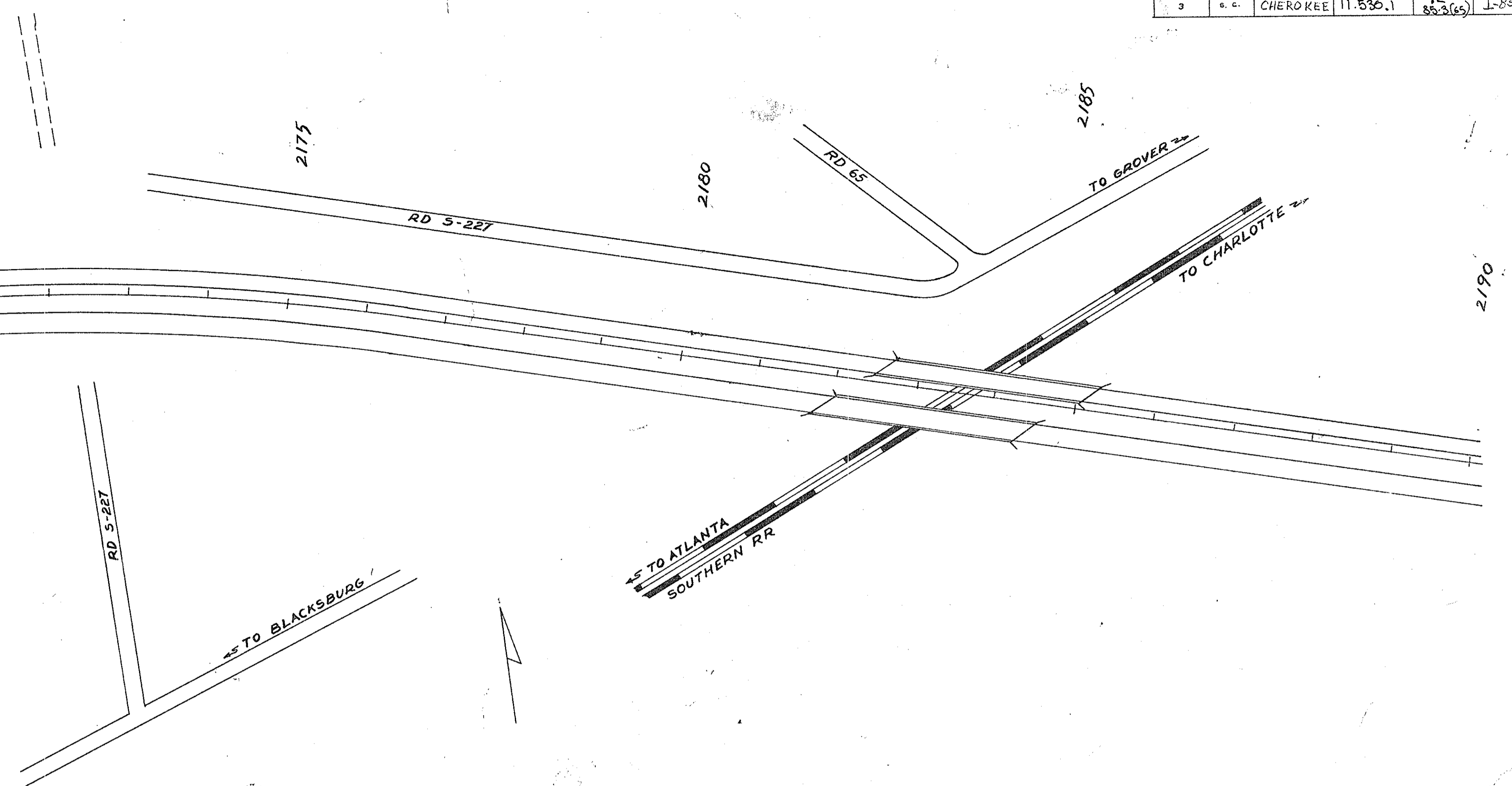


* See sheet 40.

| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|---------------|-----------|-----------|--------------|
| 3 | G. C. | CHEROKEE | 11-530.1 | 1E 88-3(6) | I-85 | 58 | 60 |

2160
 APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33



2214 + 58.6 = 2214 + 58.6

2215
 4" WHITE RAMP PAVEMENT EDGE LINE CONTINUES ALONG TAPER AND BECOMES ALONG MAINLINE PAVEMENT EDGE LINE

END 4" YELLOW RAMP PAVEMENT EDGE LINE

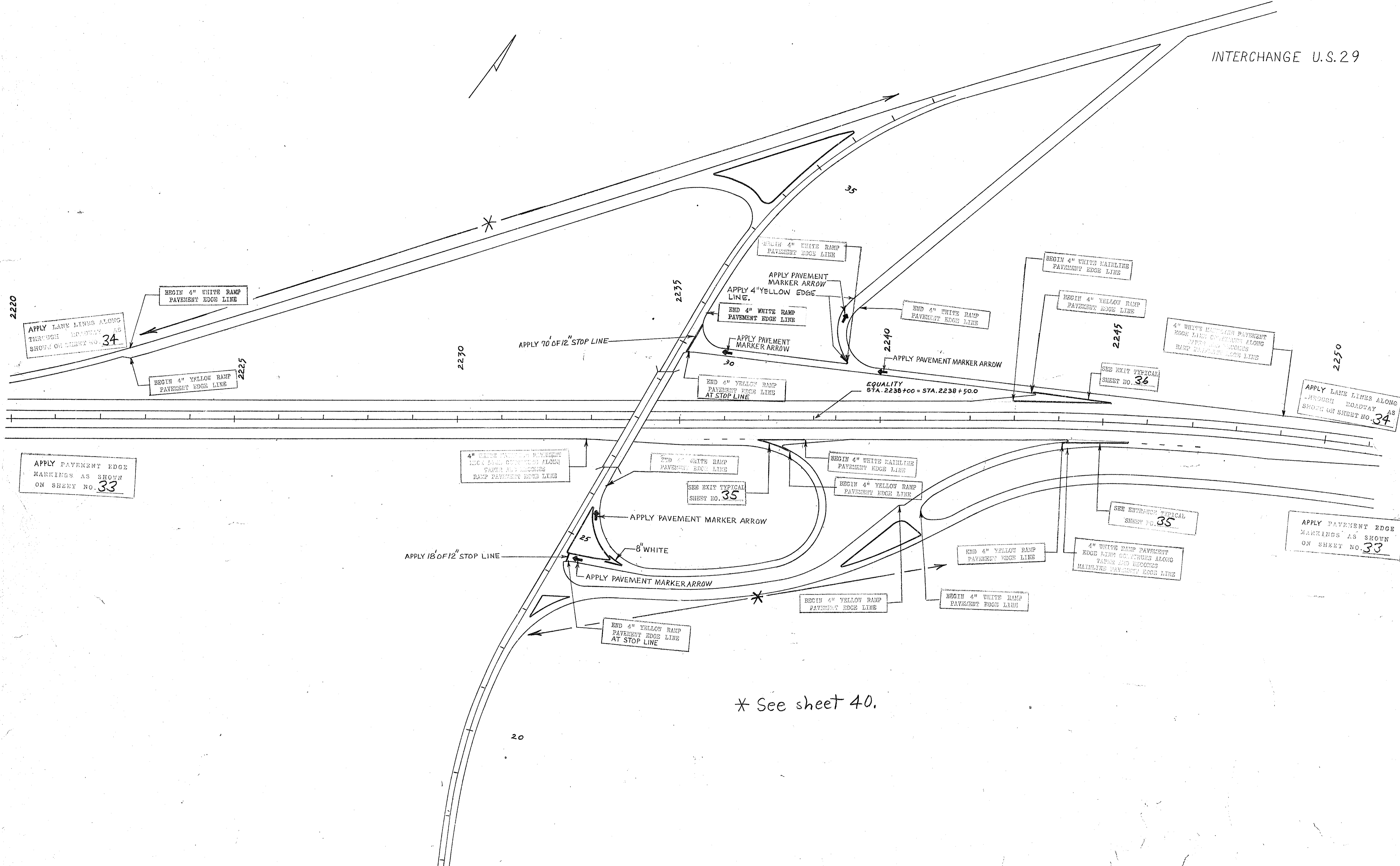
SEE ENTRANCE SIGNAL SHEET NO. 37

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

| FED. ROAD DIV. NO. | STATE | COUNTY | DOCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | 18.853 (45) | I-85 | 59 | 60 |

INTERCHANGE U.S. 29



APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

BEGIN 4" WHITE RAMP PAVEMENT EDGE LINE

BEGIN 4" YELLOW RAMP PAVEMENT EDGE LINE

APPLY 70' OF 12" STOP LINE

BEGIN 4" WHITE RAMP PAVEMENT EDGE LINE

APPLY PAVEMENT MARKER ARROW

APPLY 4" YELLOW EDGE LINE

END 4" WHITE RAMP PAVEMENT EDGE LINE

APPLY PAVEMENT MARKER ARROW

END 4" WHITE RAMP PAVEMENT EDGE LINE

APPLY PAVEMENT MARKER ARROW

BEGIN 4" WHITE MAINLINE PAVEMENT EDGE LINE

BEGIN 4" YELLOW RAMP PAVEMENT EDGE LINE

4" WHITE MAINLINE PAVEMENT EDGE LINE CONTIGUOUS ALONG RAMP MAINLINE EDGE LINE

SEE EXIT TYPICAL SHEET NO. 36

APPLY LANE LINES ALONG THROUGH ROADWAY AS SHOWN ON SHEET NO. 34

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

4" WHITE MAINLINE PAVEMENT EDGE LINE CONTIGUOUS ALONG RAMP PAVEMENT EDGE LINE

END 4" WHITE RAMP PAVEMENT EDGE LINE

SEE EXIT TYPICAL SHEET NO. 35

APPLY PAVEMENT MARKER ARROW

8" WHITE

APPLY PAVEMENT MARKER ARROW

APPLY 18' OF 12" STOP LINE

BEGIN 4" WHITE MAINLINE PAVEMENT EDGE LINE

BEGIN 4" YELLOW RAMP PAVEMENT EDGE LINE

SEE ENTRANCE TYPICAL SHEET NO. 35

4" WHITE RAMP PAVEMENT EDGE LINE CONTIGUOUS ALONG RAMP AND MAINLINE MAINLINE PAVEMENT EDGE LINE

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

END 4" YELLOW RAMP PAVEMENT EDGE LINE

BEGIN 4" WHITE RAMP PAVEMENT EDGE LINE

END 4" YELLOW RAMP PAVEMENT EDGE LINE AT STOP LINE

* See sheet 40.

| FED. ROAD DIV. NO. | STATE | COUNTY | BUCKET NO. | PROJECT NO. | ROUTE NO. | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|----------|------------|-------------|-----------|-----------|--------------|
| 3 | S. C. | CHEROKEE | 11.530.1 | 85-3(65) | I-85 | 60 | 60 |

2250

2255

2260

2265

APPLY LANE LINES ALONG THROUGH HIGHWAY AS SHOWN ON SHEET NO. 34

APPLY PAVEMENT EDGE MARKINGS AS SHOWN ON SHEET NO. 33

